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GENERAL COMPETITION RULES

This Yearbook is to be used
at all times in conjunction with the F.I.A. Yearbook of Automobile Sport.

Motorsport Ireland (MI) reserves the right
to add, delete or alter these rules, regulations and definitions.

Notice of any such amendments will be published in the official
Motor Sport Bulletin and/or on the MI website: www.motorsportireland.com
which will have the same force as the General Competition Rules

**IT IS ESSENTIAL THAT THESE RULES ARE READ IN CONJUNCTION WITH THE
REGULATIONS AND DEFINITIONS IN THE APPENDICES TO THIS YEARBOOK**

Effective 1st January 2012

This 2012 Edition supersedes all previous editions.

In the event of a conflict the following priority will apply:

International Events: FIA Yearbook, MI GCR's, MI Appendices

National Events: MI GCR's, MI Appendices, FIA Yearbook

RIAC / Motorsport Ireland

All references to RIAC in the
context of Motorsport will be taken to include the title "Motorsport Ireland" or MI

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Introduction to 2012 Yearbook

Hello and welcome to the 2012 Motorsport Ireland Yearbook. A lot of care and attention has been invested to ensure the contents of this publication are accurate and laid out in a user-friendly fashion. Thank you to all contributors that made this possible.

As a sport run in the main by volunteers I continue to marvel at the number of people that give freely of their time in the organisation of our sport. Having visited most clubs in the country during 2011 it is very evident that a great willingness exists to put on bigger and better events and the genuine camaraderie that exists is second to none. In addition to the club organisation at grass root level, all of our discipline committees work diligently for the betterment of their particular branch of the sport. It is a great testament to all of these volunteers that they are prepared to attend the number of meetings that are held all over the country and the experience they bring to the sport.

While it's very evident that a lot of genuine effort goes into organising our sport, the one criticism that comes across is poor communication. While all the work is being done we sometimes forget to keep the sport informed, but I would have to say that it would be nice to see clubs and competitors using the many modern ways of keeping up to date through use of the Motorsport Ireland website, MI bulletins and many other information sources. Our Competition Advisory Committee meetings also provide the sport with an ideal platform to voice concerns and gather information, and in the future I would see the role of the C.A.C. being enhanced through its improved use as a discussion forum for all aspects of our sport.

The young driver programmes continue to give all of us great pleasure in seeing the achievements of our licence holders at home and abroad. Our relationship with the Irish Sports Council remains central to the quest of celebrating the brilliant success in 2011 and planning for many more results in the future. Our plan is to continue to enhance these young driver programmes and explore the many good links that have been forged on the international front.

Have a safe and successful 2012.

Joe Corcoran
President
Motorsport Ireland



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MOTORSPORT IRELAND THE GENERAL COMPETITION RULES

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(Cross Reference to International Sporting code
Articles in FIA Yearbook)

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MOTORSPORT IRELAND GENERAL CODE OF CONDUCT

Motorsport Ireland expects you, and your associates, at all times to:

- Abide by Motorsport Ireland's regulations
- Respect the decisions of event officials
- Treat all competitors, marshals and officials equally with respect
- Maintain the highest standards of driving behaviour
- Conduct yourself in a proper manner at all times and always behave in the best interest of Irish motor sport
- Make every effort to minimise the impact of your activities on the environment around you

Abuse

Motorsport Ireland will not tolerate any form of abuse or aggression towards officials, marshals, spectators and competitors.

Motorsport Ireland and the National Tribunal will impose the most stringent sanction against any transgression.

Safety

You are reminded that participation in motor sport carries an inherent risk and the ultimate responsibility for safety lies with the individual. Motorsport Ireland urges you to make the maximum possible effort to ensure your own safety at all times.



GENERAL COMPETITION RULES

CHAPTER 1

GENERAL PRINCIPLES

1. International Control of Automobile Competitions.

The Fédération Internationale de l'Automobile, hereinafter referred to as the "F.I.A.", is the sole international authority entitled to make and enforce rules and regulations for the encouragement and control of automobile competitions (including records), and is the final international court of appeal for the settlement of disputes arising therefrom; it being acknowledged that the Fédération Internationale Motocycliste (F.I.M.) shall exercise the same powers in so far as vehicles with one, two or three wheels are concerned.

2. International Sporting Code.

That the above powers may be exercised in a fair and equitable manner, the F.I.A. has adopted an international Sporting Code, hereinafter called the "Code", which provides (among other things).

2.1. That each National Club (A.S.N., belonging to the F.I.A.) shall be presumed to accept and be bound by the Code.

2.2. Subject to such acceptance such National Club shall be recognised by the F.I.A. as the sole body having power to control automobile competitions in its own country.

2.3. A National Club shall have the right to delegate the whole or part of the powers granted by the Code to one or more Organisations in its country provided the consent of the F.I.A. is first obtained, and to revoke such delegation if it notifies the F.I.A.

2.4. A National Club may draw up its own national Competition Rules, but such Rules must be in conformity with the Code and approved by the F.I.A. Pending approval by the F.I.A. the Rules may be provisionally enforced forthwith.

3. National Control of Competitions.

The control of automobile competitions in the Republic of Ireland shall be administered, in accordance with the Code, by Motorsport Ireland (MI) as laid down in General Competition Rule 11.2.

4. The General Competition Rules (GCR's).

So that the above powers may be exercised in a fair and equitable manner, Motorsport Ireland (MI) has drawn up these Rules, to be known as "The General Competition Rules" and hereinafter referred to as "these Rules", which are in conformity with the Code.

5. Application of these Rules.

These Rules shall govern all competitions (as hereinafter defined) in which an automobile (as hereinafter defined) having more than three wheels

may take part, organised in the territory of Motorsport Ireland (MI).

Insofar as they do not conflict with these Rules any MI regulations, other supplementary regulations and official instructions applicable to a competition shall be deemed to form part of these Rules.

CHAPTER 2

NOMENCLATURE AND DEFINITIONS

6. Phraseology.

The following nomenclature, definitions and abbreviations shall be adopted in these Rules, in the appendices thereto, in all Supplementary Regulations and for general use. In these Rules words imparting a singular number include the plural and vice versa. Words imparting the masculine gender include the feminine gender.

7. F.I.A. (Fédération Internationale de l'Automobile).

The international federation of "Recognised Automobile Clubs".

8. Grand Prix.

The words Grand Prix may only be applied to competitions which combine all the essential qualities of an event of first importance and whose organisers have obtained special permission for the use of the title through this A.S.N. The words Grand Prix, accompanied by the name of the country, are reserved exclusively for the use of the A.S.N. of that country.

9. The Code (Code Sportif International).

FIA International Sporting Code.

10. "Recognised Automobile Club" or A.S.N.

National Sporting Authority of automobile competitions, recognised as such by the F.I.A.

11. Terms

11.1. R.I.A.C. (Royal Irish Automobile Club).

The recognised Automobile Club (A.S.N.) of the Republic of Ireland.

11.2. Motorsport Ireland (MI).

Trading name of the Irish Motorsport Federation Ltd (IMF), a wholly owned subsidiary of the RIAC. Under the powers of Article 16.3 of the FIA Statutes and in accordance with GCR 2.3. the FIA has agreed to the delegation of the Sporting Power from the RIAC to Motorsport Ireland in application of Article 5 of the Statutes.

11.3. MI.

The official abbreviation of Motorsport Ireland.

11.4. Chief Executive Officer (CEO).

The executive nominated by the Board of the Irish Motorsport Federation Ltd to be in charge of motorsport administration.



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11.5. Motorsport Department.

The administrative department of Motorsport Ireland under the direction of the Chief Executive.

12. Motor Sport Commission (MSC).

12.1. The Body delegated by the IMF to exercise control of Motor Sport in the Republic of Ireland.

12.2. President of the Motorsport Commission.

The member of the Motorsport Commission elected by the Commission to serve as its President.

13. Motorsport Ireland acknowledges the following organisations.

13.1. **Affiliated Club.** A club whose principal object is the organisation of competitions under the rules of Motorsport Ireland and which, for that purpose shall have entered into an affiliation agreement with MI which is subject to annual renewal.

13.1.1. In order to become affiliated a Club must apply to MI and enter into an agreement with MI to comply with its regulations and to pay the fees from time to time required. These fees may include

13.1.1.1. An Initiation fee, paid once only in the first year of affiliation, on first becoming an Affiliated Club.

13.1.1.2. An annual fee payable on the first of January of each following year.

13.1.2. While each application for affiliation is dealt with on its merits, the Motorsport Commission of MI will be guided by the following principles:

- It is desirable that a club have at least 50 paid up members at the time of application
- That the geographical position of the Applicant Club, or other special considerations are such that the needs of its members cannot be served by any existing Affiliated Club
- That the Applicant Club has obtained the consent of 75 percent of all the existing Affiliated Clubs. (Where 75 percent results in a fractional number, this fraction shall be ignored). This consent shall be expressed on a specific form supplied by MI, signed by the Chairman and Secretary of the Affiliated Club and ratified by a meeting of the Directors or Committee of that Club, as appropriate.
- Clubs aspiring to affiliation must, normally, be an Approved Organisation for a period of two years before making an application for affiliation.

13.1.3. In addition matters may be referred to the Motorsport Commission as provided in the Affiliation Agreement entered into by the Affiliated Clubs. Any organisation or individual penalised following a hearing by the Motorsport Commission may make an Appeal to the National Tribunal of Appeal subject to the conditions set out in Chapter 16 Article7.

13.2. Approved Organisation.

This is a body, while not entitled to organise events under these rules, whose principal objectives are to support and promote Motorsport either nationally or internationally and who provide goods and/or

services to Motorsport Ireland or any of its affiliated clubs.

- On application for Approval, such organisation must provide evidence of its size, scope and quality, the policy of the organisation and the strategy as to how the policy is implemented.
- The organisation and its policy should not conflict with any affiliated club.
- The Motorsport Commission shall decide on the acceptability of any application.
- Any such Approved organisation shall pay an annual registration fee to Motorsport Ireland and shall be required to request Approval on an annual basis.

13.3. Recognised Organisation.

This is an organisation which may provide goods, services or information for the benefit of Motorsport but whose primary objectives may not be the support or promotion of Motorsport but is considered by Motorsport Ireland to be in the general interest of the Sport.

13.4. Any First Time application for Club Affiliation or Approved Organisation status shall be published in the Motorsport Bulletin and any existing Affiliated Club shall be entitled to lodge an objection to the granting of status within one month. The Motorsport Commission shall take such objection into account when making its decision.

15. Automobile.

A land vehicle propelled by its own means, running on at least four wheels not in a line, which must always be in contact with the ground and of which at least two must affect the steering and at least two the propulsion.

16. Class.

A category of automobiles classified according to type (e.g. saloons, sports cars, racing cars, etc.) and/or cylinder capacity of their engines.

17. Competition.

Any Championship, Series, Cup, Trophy, Challenge, Race, Test attempt at Record, Trial, Rally or other event in which an automobile takes part and which has a competitive nature by publication of results. Competitions are "international" or "national" as hereinafter defined.

18. International Competition.

A competition is international when it is entered on the F.I.A. International Calendar and is open to competitors and drivers of various nationalities.

19. National Competition.

A competition is National when it is open only to competitors and drivers holding an appropriate licence (Rule No. 108) issued by the A.S.N. under whose organising permit such competition is held. A National Competition held under MI permit must be entered on the annual MI Fixture List. It has, however been agreed between the M.S.A. and MI that their respective licence holders shall



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be accepted in National Competitions in each other's territories, subject to any visa required by the Organisers, and provided that the type of licence held is appropriate to the event. (See also Appendix 1).

Entry Forms may require a visa from MI for International events. Entrants may normally obtain a visa either by post or personal call before submitting their entry form to the Organisers, but where an entrant contemplates entry to a series of such events, the MI will consider issuing a block of visas in advance, provided, that the Driver's name and licence number is given for each event.

20. Restricted Competition.

An International or National Competition is "restricted" when the competitors or drivers who take part in the competition have to comply with particular conditions other than those in Rule Nos. 19 and 21 of these Rules; for instance:

20.1. Competition by invitation.

20.2. Competition confined to automobiles of a particular make.

21. Closed Competition.

A competition is called "closed" when it is confined solely to members of the Club or Clubs organising the competition who are not the holders of the licence of any A.S.N. other than MI. MI may allow several Clubs to join in the organisation of a Closed Competition.

22. Rally.

22.1. A Rally may be either a rally of a sporting character or a Touring Assembly.

22.2. Broadly, there are two categories of Sporting Rallies:

22.2.1. Rallies with Special Stages.

The Stages being run over closed forestry roads, private roads or public roads closed to the public. The system of marking for these rallies is based on the total of the times taken for the competitor to cover the total number of Special Stages, having regard for a pre-determined target-time for individual stages which may not exceed an average speed of 110 k.p.h. loose surface or 130 k.p.h. tarmac. Timekeeping between one stage control and the next is carried out to a set average of not more than 50 k.p.h. and penalties may be incurred by the competitor for being either late or early.

22.2.2. Navigation Trials.

As the name suggest, competitors are required to follow a route set out on a route card either by map references or by tulip diagrams. The permitted average speed required, must not exceed 50 k.p.h. with penalties for being either late or early, and additional penalties for either leaving or entering a time point or Control in a direction other than specified in the route card.

22.2.3. Competitors averaging in excess of 65 k.p.h. between time controls on roads open to the public, in any Rally, Trial or other event will be subject to penalties up to and including exclusion.

22.2.4. Penalties as described in this Rule No. 22 are not to be treated as disciplinary penalties as defined in Rule No. 142

22.3. A Touring Assembly is an event organised with the primary object of assembling tourists at a point settled beforehand. In a Touring Assembly a prescribed route may have to be followed and each participant may have to report at pre-arranged points but no limits of speed are laid down. The programme of a Touring Assembly may include additional observed tests, (but nothing in the nature of a special stage or speed event). No prize money may be allocated in respect of Touring Assemblies. Notwithstanding anything in these Rules, A Touring Assembly shall be exempt from the general requirements thereof provided that:

22.3.1. The consent in writing of MI and its approval of the regulations for the Touring Assembly have first been obtained and

22.3.2. where the route runs through the territory of an A.S.N. other than MI Rule No. 81 of these Rules must be complied with.

22.3.3. A Touring Assembly is exempt from entry on the International Calendar even though it may be international in character.

23. Meeting.

An assembly of competitors and officials which include one or more competitions other than an individual attempt at record.

24. Race.

An event held on a closed circuit between two or more vehicles running at the same time, on the same course, in which speed or distance covered in a given time is the determining factor.

25. Speed and Other Events.

A meeting at which vehicles run individually (even though two or more individual runs may be taking place simultaneously) and in which the relative performance of the competitors are assessed by timing them over a given distance.

25.1. Rallysprint.

An event held in private grounds for rally type cars only. Cars must start at intervals as laid down in the approved Supplementary Regulations for the event, and both driver and co-driver may be carried. Course must not be a closed circuit and must not exceed 8 kilometres in length.

25.2. Rallycross.

A speed event which takes place on a combination of sealed and unsealed surface as part of a permanent circuit. Track requirements and safety precautions will be as specified by MI on application.



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25.3. Autocross.

A speed event on grass or loose surface (GS), (LS).

25.4. Sporting Trial.

A trial in which the marking during the event is mainly based on the ability to climb hills or traverse observed sections without cessation of forward motion for more than periods of 5 seconds at a time.

25.5. Test Trial.

A competition based on the fastest time taken in a series of Driving Tests (see 25.7.) and run over a pre-set route on the public highway.

25.6. Autotest.

As Test Trial above EXCEPT that a route is not followed and a series of driving tests takes place within a defined area of private ground.

25.7. Driving Test.

That part of a competition in which marking is based on a competitor's performance in manoeuvring or adjusting the control or speed of his car, or maintaining the forward motion of his car in adverse conditions, or a combination of these requirements allied with the time taken to perform the test.

25.8. In either competition under 25.5. or 25.6. competitors will not be required to travel in a forward or in reverse direction for a distance of more than 200 metres on each occasion in any one driving test.

26. Organising Permit.

The documentary authority to organise and hold a competition, granted solely by MI (Rule Nos. 55 and 56).

27. Organisers.

Any person or body proposing to hold, holding, or organising a competition under these rules (Rule No. 52).

28. Supplementary Regulations (SR's).

Regulations supplementary to these Rules, drawn up by the organisers of a competition or competitions and approved by MI with the object of laying down the details of such competition or competitions.

29. Record.

The best result obtained under the prescribed conditions.

30. Local Record.

A record established on a permanent or temporary track approved by MI whatever the nationality of the competitor.

31. National Record.

A record recognised as such by MI in accordance with the FIA International Sporting Code No.35, regardless of the nationality of the competitor.

32. Holder of Record.

32.1. If the record is one established in the course of an individual attempt, the holder is the person to

whom permission to make the attempt was granted and who made formal application for such permission.

32.2. If the record is one established in the course of a meeting the holder is the competitor in whose name entry was made of the automobile with which the performance was achieved.

33. The Start.

The start is the moment when the order to start is given to a competitor or competitors (see also Chapter 6).

34. Control Line.

A control line is a line by reference to which an automobile is timed or its performance in a competition is determined.

35. Starting Line.

A starting line is the first control line on a course (Rule No. 90).

36. Finishing Line.

A finishing line is the last control line on a course.

36.1 Parc Ferme

Parc Ferme is the place to which a competitor is obliged to bring his car(s) as set out in the Supplementary Regulations. In the parc ferme area only authorised officials may enter. No operations, checking, tuning or repair is allowed unless agreed by the authorised officials.

37. Handicap.

The method laid down in the Supplementary Regulations of a competition for the purpose of equalising the chances of the competitors.

38. Competitor.

A person whose entry is accepted for a competition or who competes in any competition whatsoever, whether as Entrant, Driver or Passenger.

39. Driver.

A person nominated as the driver of an automobile in any competition whatsoever.

40. Passenger.

A person other than the driver, who is 16 years of age or over, conveyed in an automobile during a competition and weighing, with personal equipment, not less than 60 Kgs. (Note: This definition includes navigators).

41. Competition Licence.

This licence is a certificate of registration issued under the Code by an A.S.N. to any person wishing to compete in competitions held under the Code. The holder of a licence is presumed to know the Code, and must observe it.

42. Licence Number.

The number on a competition licence.



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43. Competitors' Register.

The list maintained by an A.S.N. of persons to whom it has issued either an entrant's or driver's licence. The Register must show the respective licence number and the category of licence issued specifically stating whether National or International.

44. Nationality.

The nationality of an entrant or driver for the purpose of these Rules shall, after the grant of a licence, be deemed to be that of the A.S.N. which last issued a Competition Licence.

45. Track Licence.

The written approval of a track by the F.I.A. (International Track Licence) or by MI (National Track Licence). (For 'Fees' see GCR No. 88)

CHAPTER 3

COMPETITIONS - GENERAL CONDITIONS

46. Organisation of Competitions.

Competitions may only be organised in the territory of MI by:

- 46.1. MI.
- 46.2. Automobile Clubs affiliated to MI.
- 46.3. Other organisers approved by MI.

47. Official Documents.

For every competition the organisers are responsible for drawing up and publishing Supplementary Regulations and a Programme. If any condition contained in the Supplementary Regulations is contrary to these Rules, it shall be null and void.

48. Announcement to be made on all Official Documents.

All Supplementary Regulations, Programmes and entry forms relating to any competition shall bear in a conspicuous manner the following announcement:

"Held under the General competition Rules of MI (incorporating the provisions of the International Sporting Code of the F.I.A.)"

49. Acquaintance with and Submission to Rules.

Every person, or group of persons, organising a competition or taking part therein:

- 49.1. Shall be deemed to be acquainted with the Code and these Rules.
- 49.2. Shall undertake to submit himself without reserve to the consequences resulting from these Rules.
- 49.3. If a vehicle is found not to comply with the technical regulations, it shall be no defence to claim that no performance advantage was obtained.

50. Unauthorised Competitions.

Any proposed competition not organised in conformity with the Code and these Rules is prohibited, and if such a competition is included in a meeting for which a permit has been granted that permit shall be null and void. Any person taking part in such an unauthorised competition shall be subject to the provisions of Rule No. 140.

51. Postponement, Abandonment or Cancellation of a Competition.

A meeting or a competition forming part of a meeting shall not be postponed, abandoned or cancelled unless:

- 51.1. Provision for so doing is made in the Supplementary Regulations, or
- 51.2. The Stewards of the Meeting have taken action under Rule 129.11. In the event of cancellation or postponement for more than 24 hours, entry fee shall be refunded.

CHAPTER 4

COMPETITION ORGANISATION

52. Organisers.

Competitions shall normally be organised by a Club or Clubs affiliated to MI (Rule No. 13.1.) or in exceptional circumstances by some other body approved by MI and in either case shall be known as "The Organisers".

53. Registration of Clubs.

No Club shall organise a Competition unless it has been affiliated to MI (Rule No. 13.1.) and has paid affiliation fees for the current year.

54. Recognition of National and Local Authorities.

A competition may be held either on the road or on the track or on both but no permit shall be granted by MI or Competition be held unless the Organisers shall have first obtained such official permission as may be necessary from the competent governmental or local authority. Competitions taking place on the public highway must conform to the traffic laws of the country in which they take place. Any breach of such laws by a competitor or driver during a competition, shall be a breach of these Rules and may be dealt with accordingly.

55. Necessary Permission and Approval.

No Competition (Rule No.17) shall be held unless MI has signified its approval by granting an organising permit or has waived the necessity of an organising permit. MI may attach conditions to the grant of a permit or decline the grant or may withdraw a permit.

56. Application for an Organising Permit.

56.1. Every application for an Organising Permit shall be made out on the official form and shall be accompanied by the appropriate fee



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(Rule No. 57) together with a draft of the proposed Supplementary Regulations and entry form. The application must be lodged at least 28 days before the date of the proposed competition, except in the case of Stage Rallies where application must be made a minimum of 12 weeks before the competition. The application must state:

56.1.1. The name and address of the applicant.

56.1.2. The body or person on whose behalf the application is made and the official position held by the applicant.

56.1.3. The nature of the competition for which the permit is required.

56.1.4 The date and place of the proposed competition.

56.1.5. In the case of an application for a permit to hold a race meeting or speed event on a track, circuit or course, which is not already approved by MI, an application for approval must be made to MI at least 12 weeks before the proposed competition, and this application must be accompanied by a plan to a scale of not less than 6 inches to a mile. This plan shall give details of the course and indicate the proposed position of spectators, and measures to be employed to ensure their safety.

56.1.6. In the case of permanent race circuits compliance with Rule No. 87.

56.1.7. The name and relevant current valid licence number of the Clerk of the Course (Stage Rallies, Car and Kart Races and Hillclimb /Sprints /Rallycross).

56.2. MI reserves the right to withdraw a Permit or add to, or modify the conditions attached thereto, even after the permit has been granted.

57. Permit Fees.

The fees payable to MI on the grant of an organising permit is laid down in the schedule of fees to these Rules or as stated on current permit application form or Yearbook.

57.1. Late Applications.

Late applications for event permits will be automatically refused, or a fine imposed on the organisers not greater than an amount equivalent to that of the permit fee for an individual event of similar type. Incorrect or incomplete applications may be refused or returned for re-submission as may any application from any Organiser who has failed to discharge any requirement of, or any indebtedness to MI.

58. Waiver of necessity for Permit.

MI may waive the necessity for organising permits for individual specific events of the kind listed below. The organisation of competition under such a waiver shall be subject to such conditions as MI shall from time to time announce, and the waiver may be revoked at any time either generally or in respect of a particular Club or proposed competition without a reason being given.

58.1. Touring Rallies or Assemblies as noted in Rule No. 22.3. in which there is no individual timing except for the purpose of preventing competitors from traveling at inappropriate speed.

58.2. Veteran and Vintage events organised in accordance with regulations laid down from time to time by MI.

58.3. Fuel Economy Runs.

58.4. Other events such as Concours d'Elegance processions for charitable historic or road safety purposes for which MI may see fit to grant a waiver.

58.5. Gymkhana.

Meeting of automobiles and drivers not taking place on public roads and not requiring a competition licence. Such events require a waiver of permit.

58.6. Application for waiver of permit must be submitted at least 28 days in advance of the event.

59. Main Information to be included in Supplementary Regulations (SR.).

The supplementary Regulations shall contain the following information where appropriate:

59.1. The name and nature of the proposed competition(s).

59.2. The name and address of the Organisers.

59.3. A statement in accordance with No. 48 of these Rules.

59.4. The place and date of the meeting

59.5. A full description of the proposed competition including length and direction of course, fuel and the categories of automobile eligible.

59.6. The dates, times and nature of starts with details of handicapping (if any).

59.7. A reminder of such of these Rules as may be particularly applicable to the competition, and also in regard to licences, signals, protection against fire.

59.8. The manner in which results will be determined and prizes awarded.

59.9. A detailed list of prizes allocated to each competition.

59.10. The names of the Stewards of the Meeting and of the Clerk of the Course.

59.11. Provision regarding postponement, abandonment or cancellation.

59.12. Any additional information required on entry forms.



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59.13. The dates of opening and closing of entries and how and where they shall be made.

59.14. The amount of the entry fee.

59.15. Conditions in which entries may be refused.

59.16. All such other requirements and information as shall from time to time be specified in the MI Yearbook and Motor Sport Bulletins.

60. Alteration of Supplementary Regulations.

No alteration shall be made to the Supplementary Regulations after the commencement of the period for receiving entries unless:

60.1. MI and all competitors already entered agree to the alteration or

60.2. The Stewards of the Meeting so decide for reasons of safety or force majeure.

61. Entry.

An entry made and accepted in accordance with these Rules and any relevant Supplementary Regulations is a contract between a Competitor and the Organisers by which the Competitor agrees to take part in the competition for which he has entered unless prevented by force majeure, and the Organisers agree to comply with the conditions of entry provided that the Competitor has made every effort to take part in the competition. A breach of such contract may be treated as breach of these Rules.

62. Dispute regarding Entry.

If any dispute shall arise otherwise than by way of protest between a competitor or driver and Organiser regarding an entry or nomination to drive which has already been accepted, such dispute shall be referred to MI subject to the right of appeal (if any) provided by these Rules.

63. Opening of Entries.

An organising permit having been granted or the proposed competition being one of a kind in respect of which the necessity for an organising permit has been waived, the organisers may invite, receive and accept entries from eligible competitors. Every entry form or invitation to enter sent out by the Organisers shall be accompanied by a print of the relevant supplementary regulations. Every entrant shall complete and sign an entry form and submit it to the Organisers by the appropriate date together with the entry fee (if any). An entry form which is incomplete in any material particular may be accepted by the Organisers conditionally upon a completed entry form being received by a specified date. An entry not accompanied by the entry fee (if any) laid down by the supplementary regulations or without specifying Competition licence number of driver or entrant shall be null and void. An entry for a Competition organised outside the territory of MI by a competitor whose licence was issued by MI shall only be made with the prior approval of MI unless covered by the provisions of Rule No. 17.

64. Ineligible Entries.

Except with the written consent of MI, no Organisers shall accept an entry from any person in whom they have a financial interest.

65. Contents of Entry Form.

Entry forms shall contain the following:

65.1. Spaces for the full true names and addresses of entrant, driver (s) and passenger(s) and (if appropriate to the competition) the licence number of the entrant and driver(s). Supplementary regulations may permit the nomination of drivers and passengers to be deferred, but no driver or passenger shall compete unless he has first signed the appropriate indemnity declaration(s) and/or undertaking(s) as required by Rule No. 66.

65.2. An entry may be submitted by an agent on behalf of the entrant but in that case the entrant shall himself sign and submit an entry form as soon as possible, and in any case before the commencement of the competition.

65.3. The following statement: "if an entrant is under 18 years of age this form must be countersigned by the appropriate parent or guardian".

65.4. Forms of indemnity, declaration and undertaking as appropriate in accordance with Rule 66 and spaces for the appropriate signatures. If so desired, the forms to be signed by drivers and passengers (but not those to be signed by the entrant) may be on a separate sheet accompanying the entry form.

65.5. A statement in accordance with Rule No. 48 of these Rules.

65.6. Any other information required by the supplementary regulations to be stated on the form.

66. Indemnities, Declarations and Undertakings by Entrants, Drivers and Passengers.

66.1. Every entrant, driver and passenger before taking part in any competition shall sign an agreement in the following form:

"I have read the supplementary regulations issued for this event and agree to be bound by them and by the General Competition Rules and Regulations of MI including the guidelines and regulations contained in Motorsport Ireland's Code of Conduct for Childrens Sport. In consideration of the acceptance of this entry or of my being permitted to take part in this event I agree to save harmless and keep indemnified the (Organising Club), Irish Automobile Club Ltd. t/a The Royal Irish Automobile Club, Irish Motorsport Federation Ltd. t/a Motorsport Ireland and their respective officials, servants, representatives and agents from and against all actions, claims, costs, expenses and demands in respect of death, injury, loss of or damage



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to the person or property of myself, my driver(s), passenger(s) or mechanic(s) (as the case may be) howsoever caused arising out of or in connection with this entry or my taking part in this event and notwithstanding that the same may have been contributed to or occasioned by the negligence of the said bodies, their officials, servants, representatives or agents. Furthermore, in respect of any parts of this event on ground where Third Party Insurance is not required by law, this Agreement shall in addition to the parties named above extend to all and any other competitor(s) and their servants and agents and to all actions, claims, costs, expenses and demands in respect of loss of or damage to the person or property of myself, my driver(s), passenger(s) or mechanic(s). My age is (if applicable state over 18 years).

66.2. Every entrant shall sign the following declarations:

66.2.1. In the case of Races (which term include Speed Events)

"I declare that to the best of my belief the driver(s), passenger(s) possess the standard of competence necessary for an event of the type to which this entry relates and that the car entered is suitable and roadworthy for the event having regard to the course and the speeds which will be reached".

66.2.2. In the case of any event taking place wholly or partly on the public highway:

"I declare that the use of the car hereby entered is covered by Insurance as required by the Road Traffic Act, which is valid for such part of this event as shall take place on roads as defined in the Act".

66.3. Every driver taking part in a Speed Event (Rule No. 25) shall sign a declaration in the following form:

"I understand that should I at the time of this event be suffering from any disability whether permanent, temporary or otherwise which is likely to affect prejudicially my normal control of my automobile, I may not take part unless I have declared such disability to MI, who have, following such declaration issued a licence which permits me to do so". Rule 108.3).

66.4. Every driver taking part in a Race or Speed Event, or Special Stage Rally, shall sign an undertaking in the following form:

"I undertake that at the time of the event to which this entry relates I shall be in possession of a current certificate of medical fitness".

In the case of MI Licence Holders only certificates on the official MI or FIA Medical Forms will be accepted.

66.5. Any indemnity and/or declaration as prescribed by sub-paragraphs 66.1 and 66.2 above which is signed by a person under the age of 18 years shall be countersigned by that person's parent or guardian, whose full names and address shall be given. Furthermore, the parents and/or guardians of persons under 18 years of age shall grant permission to MI and the Irish Sports Council to carry out tests in accordance with the Irish Anti-Doping Rules (Rule No 139) in the following form:

"I/We hereby grant permission to MI and the Irish Sports Council to carry out tests as set out in Rule No 139 of the GCRs in accordance with the Irish Anti-Doping Rules."

67. Closing of Entries.

The date specified in the Supplementary Regulations for the closing of entries shall, in the case of International Meetings, be not less than seven days, and in the case of other Competitions, not less than three days before the date of the meeting.

For the purposes of "One Event" Licences only, entries may be accepted on the day.

68. Entry by Facsimile/Electronic.

Notwithstanding the provisions of Rule Nos. 63 and 66 of these Rules, an entry may be made by electronic means provided it is received before the time laid down for the closing of entries. Any entry fee must be dispatched in time to meet the closing date for the receipt of payment. All entries must bear the original signatures required before the start of any event. All events must allow a competitor to complete a manual entry form if so requested.

69. Entry Containing a False or Incorrect Statement.

An entry which contains a false or incorrect statement shall be null and void and the entrant may be deemed guilty of a breach of these Rules and the entry fee may be forfeited.

70. Refusal of Entry.

70.1. If an entry for an International Competition is refused, notification of such refusal shall be sent to the entrant at the address given on the entry form so as to arrive in ordinary course of delivery within eight days after the receipt of the entry or at least five days before the Meeting, stating the grounds for such a refusal.

70.2. An entry for a competition held under these Rules, other than an International Competition, may be refused in accordance with the Supplementary Regulations relating to such competition, and/or the relevant Appendices of this Yearbook.

71. Conditional Acceptance of Entry.

Supplementary Regulations may provide that entries may be accepted under certain clearly stated conditions. For instance, where the number of



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starters is limited, an entry may be accepted conditionally upon a vacancy occurring among entrants already accepted. Conditional acceptance shall be notified to the entrant in writing despatched not later than the day following the date for the closing of entries.

72. Acceptance of Entries.

If the number of entries received exceeds the maximum number of competitors laid down in the Supplementary Regulations, those to be accepted shall be selected in the manner specified by the Supplementary Regulations. If no manner is specified, they shall be selected either by ballot or by the order of their receipt as the Organisers shall decide.

73. Acknowledgement of Entries.

Unless the Supplementary Regulations prescribe a shorter time, Organisers shall acknowledge Entries within seven days of receipt. Except as otherwise provided by these Rules, organisers shall at the same time inform Entrants whether their Entry is accepted, refused, or a decision deferred.

74. Unauthorised Entries.

The Organisers shall not enter on the programme or publish as entered the name of any competitor or driver in respect of whom they have not accepted or conditionally accepted an entry or nomination. A competitor whose entry is conditionally accepted shall be shown as such.

75. Nomination of Reserves.

Should any competitor be eliminated in accordance with Rule No.72 of these Rules he may be accepted as a reserve.

76. Multiple Entries of an Automobile.

An automobile shall not be entered more than once in the same competition. MI may depart from this rule by authorising two entries of the same automobile in a competition. In such a case the responsibility of the entry shall be shared jointly by both entrants and the automobile shall only compete once with the same driver.

77. Change of Automobile.

Subject to anything to the contrary contained in the Supplementary Regulations after the publication of the programme the Stewards of the Meeting may authorise the substitution in an event of an automobile different from that specified on the entry form provided that the Supplementary Regulations are complied with in all other respects.

78. Insurance.

MI will arrange insurances in respect of legal liability for all events authorised by the issue of a permit. The organisers shall pay all premiums due in respect of such insurance or any other insurance required as a condition of permit or waiver of permit.

79. Publication of Results.

The Organisers shall inform each competitor of the provisional results of a competition within 21 days of the completion of the competition or such further time as MI shall allow.

80. Payment of Starting and Prize Money and Presentation of Awards.

The Organisers shall distribute all starting and prize money within 21 days after the final results of a competition shall have been arrived at, or such further period as MI may allow. Any awards shall be presented within a like period unless the Supplementary Regulations specify a particular date or occasion for presentation.

CHAPTER 5

COMPETITION COURSES

81. International Courses.

When the course of a competition traverses the territory of several countries the Organisers must first obtain through their own A.S.N. the consent of every A.S.N. having authority in any of such countries. Each A.S.N. whose territory is traversed will exercise control of such a competition whilst it is taking place within the territory except that the announcement of the results of the competition will be confirmed by the A.S.N. to whom the organisers are answerable.

82. Road Courses.

The selection of any courses for a competition shall be subject to the approval of MI with the request for such approval there shall be submitted an itinerary giving the distance to be covered.

83. International Track Licence.

Application may be made by MI to the F.I.A. for an International Track Licence for a permanent or temporary track. The F.I.A. may license a track for a meeting or for a series of meetings, or if the track is a permanent one, until the 31st December next ensuing. The F.I.A. may, after consultation with MI refuse to grant or may withdraw an international track licence in respect of a track within the territory of MI. The reasons for any such refusal or withdrawal shall be stated.

84. National Track Licence.

MI may grant a national track licence to a track for a Meeting or for a series of Meetings or, if the track is a permanent one, a licence which shall be valid for a period of not more than three years expiring on 31st December of the specified year. MI may refuse to grant or may withdraw a national track licence as it may think fit. A track in respect of which only a national track licence is current may not be used for attempts at International Class or World's Records. Such track licence shall only be valid in respect of Competitions organised in accordance with GCR 55.



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85. Information to be given on Track Licences.

A track licence granted by the F.I.A. will state the length of the track and whether it is approved for attempts at international and world's records. A track licence granted by MI will state the length of the track and whether it is approved for attempt at national records. The track licence will also contain the Special Track Rules, which all concerned are expected to know and are required to obey.

86. Display of Track Licence.

A track licence shall, so long as it is in force, be displayed in a prominent position at the track.

87. Conditions to be fulfilled for both Permanent and temporary tracks.

Permanent and temporary tracks shall comply with the conditions and requirements laid down in Appendix "O" to the Code (FIA) in respect to FIA Championship Events.

88. Track Licence Fees.

The scale of fees payable on the grant of a track licence is laid down in the Schedule of Fees of these Rules.

CHAPTER 6

STARTS, HEATS AND TIMING

89. Starting.

There are two methods of starting.

89.1. The flying start.

89.2. The standing start.

90. Starting Line.

90.1. In all record attempts and in competitions with a flying start, the starting line is the line on the crossing of which the timing commences (Rule No. 35.)

90.2. In competitions with a standing start, the starting line is the line in relation to which the position of each automobile (and if appropriate, each driver) is fixed prior to the start (Rule No. 33).

91. Starting Positions.

The Supplementary Regulations shall prescribe the relative positions of all automobiles prior to the start and the method by which such positions are to be determined.

92. Flying Start.

A flying start occurs when the automobile(s) are moving at the moment when the timing commences. To achieve a flying start, the competitors may be led by a starter in a pilot automobile until the starting signal is given. This method of achieving a flying start may be called a "Rolling Start".

93. Standing Start.

A standing start occurs when the automobile(s) are stationary at the start. The following conditions shall, as appropriate apply to a standing start.

93.1. For a record attempt the automobile must be stationary with that part which operates the timing apparatus behind but within 10 centimetres of the starting line. The engine of the vehicle shall be running before the start.

93.2. For all other competitions the Supplementary Regulations shall stipulate whether, before the start, the engine shall be running or stationary.

93.3. Automobiles starting singly or in line abreast shall be placed as follows:

93.3.1. If the timing is recorded by automatic timing apparatus, as laid down in 93.1. above.

93.3.2. If the timing is done by means of a watch or other timing apparatus not triggered automatically, the vehicle or vehicles shall be placed before the start with the front wheels touching the ground on the designated line or in accordance with the event Supplementary Regulations.

93.4. The Supplementary Regulations may prescribe that the drivers shall at the start be at some specified distance from their automobiles.

94. Timing.

94.1. In the case of a standing start the timing shall commence:

94.1.1. If the timing apparatus is automatic when it is operated or

94.1.2. If the timing is not automatic at the start.

94.2. In the case of a flying start when the leading automobile crosses the starting line.

94.3. In circuit competitions the completion of the first and subsequent laps shall be timed when the automobile crosses the control line in front of the Timekeepers' station unless the Supplementary Regulations otherwise provide.

95. Crossing a Control Line.

The timing of an automobile crossing a control line shall be taken at the moment when the centre of the front wheel(s) pass over that line, or where the timing apparatus is automatic in operation at the moment when it is operated.

96. Starter's Orders.

96.1. Drivers and vehicles ready for the start at Race Meetings, come under the orders of the Starter from the moment the one minute board is shown until the starting signal is given. All competing cars on the grid when the one minute signal is given or in the event of a formation lap, when shown the GREEN flag will be deemed to have come under starters orders. Any car having come under Starters Orders and then not being able to take its place on the final grid may start when ready from its pit, but only after the remaining cars have left the final grid and when directed to do so by the Pit Lane Marshal or Starter.



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96.2. In the case of a permanent circuit where all pits are behind the Starting Line or a continuation of it the provisions of Rule No 90 of the Code on "Start from the Pits". Starting line after the pits would then apply, i.e. the driver will be considered as having completed one lap the second time he passes the line.

97. Non starter.

Any driver not coming under the Starters Orders shall be deemed a non-starter. Any driver will be considered as having started if under Starter's orders at the start.

98. False Start.

A false start occurs when before the start a driver under Starter's Orders moves forward from his prescribed position.

99. Penalty for False Start.

99.1. A false start occurs when before the appropriate signal is given a competitor moves forward from the prescribed position.

99.2. The penalty for a false start shall be defined in the Appendix appropriate to the event, or in the Supplementary Regulations.

99.3. Any such penalty shall be notified to the competitor's team as soon as practicable or as described in the Supplementary Regulations.

99.4. If laid down in the Supplementary Regulations a further penalty may be imposed (e.g. a stop/go or drive through) within the limits of the Supplementary Regulations.

100. Heats.

A competition may be started in heats, the composition of which must be determined by the Organisers and published in the programme. The composition of heats may be modified or heats consolidated by, but only by, the Stewards of the Meeting.

101. Dead Heats.

In the case of a dead-heat the competitors concerned shall either share the prize allotted to their places in the results, or, if all such competitors agree the Stewards of the Meeting may authorise a re-run by such competitors only, and may impose conditions therefore, in no case whatsoever shall the whole competition be re-run.

CHAPTER 7

COMPETITORS AND DRIVERS

102. Registration of Competitors (Entrants) and Drivers.

Any person who wishes to qualify as a competitor or driver as defined in Rule No. 38, shall make a formal application for a competition licence to MI in accordance with Rule No. 41.

Before the commencement of a meeting, all competitors and drivers must produce a current valid competition licence to the organisers and in default the driver must not be permitted to start. Any competitor or driver found not to have a current valid licence will be liable for the fixed penalties set out in Rule No. 139.10. MI reserve the right to impose additional penalties where it is deemed appropriate in accordance with the procedures in Chapter 16 Article 9. Entries in the first place must not be accepted by organisers unless the competition licence numbers of competitors, drivers, co-drivers, navigators, etc. are given in the spaces provided in the entry forms. Only persons who possess a current Commercial Entrants Licence may enter another driver for a competition, except as laid down in Rule No 108.2.3. This will not affect a competitors right to be his or her own entrant.

Individuals seeking a Commercial Entrants licence in a name or title other than the applicant's name, must provide proof of permanent residence within the Republic of Ireland by including a current valid Certificate of Incorporation within the meaning of the Companies Acts 1963 to 2009.

Notwithstanding anything in these Rules, MI may in writing waive the necessity for entrants or drivers to hold a licence under these Rules in order to be eligible to take part in a particular competition, if it is primarily an event of a social character or an event open only to persons who have not recently regularly taken part in competitions.

103. Grant of Competition Licence.

103.1. A licence may be either an entrants, navigator or a driver's licence.

103.2. MI shall be entitled to grant licences to:

103.2.1. Citizens of the Republic of Ireland

103.2.2. A national of any other country represented on the FIA after receiving authority from the A.S.N. of that country so to do.

103.2.3. A national of a country not represented on the F.I.A. provided the F.I.A. is first informed and makes no objection of the grant of such licence.

103.3. In accepting a licence the holder shall be deemed to have contracted with MI to be bound by these Rules and in particular the obligations set out in Rule 49.

104. Restrictions on the Holding of Licences.

104.1. No person shall apply for or hold a current competitor's and/or driver's licence from more than one A.S.N.

104.2. No person shall apply for or hold more than one current entrants and driver's licence from MI.

104.3. No person shall obtain licences from more than two ASN's during any one calendar year.

104.4. Any incorrect or false statement in connection with an application for or upgrading of a licence



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(including medical details) may be considered a breach of Rule No. 139.3.

105. Refusal of Licence.

MI may refuse to issue a licence to an applicant who does not meet the criteria applicable to the licence applied for. The reasons for any such refusal shall be stated.

106. Period of Validity of Licence.

Licences shall be valid from the date of issue to the end of the current calendar year, unless previously surrendered, or withdrawn.

107. Licence Fee.

On the grant of licence by MI a fee shall be payable to MI in accordance with the scale laid down in the Schedule of Fees in these Rules.

108. Validity of Licence.

108.1. A competition licence shall be signed indelibly by the holder on receipt and shall not be valid until so signed.

108.2. F.I.A. licences issued by MI are either:

108.2.1. As per "Issue of Competition Licences". Appendix 1 of the current Yearbook.

108.2.2. Commercial Entrants Licence.

A commercial firm or undertaking, wishing to enter a driver in any type of motor sporting event where the name of the entrant is published in a programme or in any other announcement, is required to hold a current commercial entrants licence, the number of which must appear in the entry form before such an entry can be accepted in accordance with Rule No. 102.

108.2.3. Competitor Entrant's Licence.

All drivers in MI Cadet and Junior Kart Racing Classes, the Junior Rallycross Class, Junior Rallysprint and the Junior Race Class must be entered and accompanied at events by the holder of a current valid Competitor Entrant's Licence, who must be 18 years of age or over. In addition, parents/guardians wishing to enter drivers who have attained 16 years and are under 18 years of age may optionally avail of this licence. It is the Entrant's responsibility to ensure that all persons concerned with the entry observe all the requirements of the GCR's, the Technical Regulations and the Sporting Regulations. If an Entrant is unable to be present in person at the Event, they must nominate their representative in writing, who must also be the holder of a current valid Competitor Entrant's Licence. Such a representative is responsible jointly and severally with the Entrant for the observation of all the rules and regulations. Entrants must ensure that their vehicles comply with the conditions of eligibility and safety throughout practice and the race. Current fees are set out in the Schedule of Fees these Rules.

The holder of a Competitor Entrant's licence may act as Entrant for a maximum of two drivers. In the case where the parent/guardian is acting as Entrant for their own children, this restriction does not apply.

108.3. A Competition licence granted by MI does not constitute a certificate of the competency of the holder. No driver, though the holder of such a licence, who is suffering from any disability, permanent, temporary or otherwise, which prejudicially affects the normal control of his automobile, shall take part in any speed event. Provided that where a driver is suffering from a permanent disability the nature of which he has declared to MI who have nevertheless issued a licence, such disability shall not debar the driver from taking part in a speed event in compliance with any conditions endorsed on his licence.

108.4. If the holder of a competition licence issued by MI is during the currency of such licence, disqualified from holding a licence under the Road traffic Act, then their competition licence ***shall become*** null and void and ***must*** forthwith be returned to MI. The Motor Sport Commission, upon application by the licensee may consider the circumstances under which the disqualification was ordered and authorise the re-issue of such competition licence ***or issue a licence with restrictions or refuse the re-issue of a licence, as it deems appropriate.***

109. Production of Licence.

An entrant or driver at a Meeting shall produce his licence on demand to an official of that Meeting.

109.1. Notwithstanding anything in these rules, MI may in writing waive the necessity for drivers to hold a competition licence in order to be eligible to take part in a particular event if it is primarily an event of a purely social character - (Rule Nos 102 + 110).

110. Certificate of Medical Fitness.

No driver whether the holder of a Licence or not, shall compete in any Race, Speed Event or Rally with special stages or Rallycross, unless he or she has lodged with the Motor Sport Department a Certificate made within the required time by a qualified Medical Practitioner that the driver is medically fit to drive in such events. Refer to Appendix 1 Issue of Competition Licence in the current Yearbook. After an accident or in a case of a medical or physical problem the Chief Medical Officer of the event and/or MI may deem a medical examination necessary. If a competitor experiences medical or physical problems outside of competition and the organiser is made aware of this, the competitor concerned may be subject to a medical examination before the event in which they intend to take part.

111. Production of Medical Certificate.

A driver competing in or proposing to compete in a race, speed event, stages rally or rallycross must



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be able to produce a licence with a valid Medical Expiry date on demand to any official of the meeting.

112. Assumed Name.

If a licence is required in an assumed name, special application therefore shall be made to MI which may, if it thinks fit, issue a licence in such name. A person granted a licence in an assumed name shall, so long as he holds a licence in that name not take part in any competition except under the name shown on such licence.

113. Change of Driver or Automobile.

In a competition, other than an attempt at record, a change of driver or automobile shall be made only if the Supplementary Regulations so provide, and, if it is to take place after publication of the programme, with the consent of the Stewards of the Meeting.

114. Responsibility of Entrant, Driver or Others.

114.1. The entrant shall be responsible for all acts or omissions on the part of his driver, mechanic or passengers, but each of these shall be equally responsible for any breach of the Code, these Rules, any Supplementary Regulations or Instructions to drivers.

114.2. With the exception of the provisions of Rule 108.2.3, where a competitor has not reached their 18th birthday the Entry for an event must be endorsed by the Parent or Guardian as Guarantor. Such Guarantor will be considered as being the Minor's Entrant, and as such will be subject to these Rules.

114.3. In the case of events where speed is the deciding factor the Guarantor, or their representative, must attend the event with the Minor, and sign-on as his Entrant. When a Guarantor's representative attends an event and signs-on as their Entrant, the Representative must produce the Guarantor's written and signed authorisation to so act.

115. Entrants and Drivers Forbidden to Abandon One Competition and Compete in Another.

Any entrant having entered, or any driver having undertaken to drive in any competition, who does not take part in that competition and takes part in another competition on the same day may thereby become liable to any penalty set out in Rule No. 142. Provided that if the two competitions take place in different countries, MI and the other A.S.N. concerned shall agree as to the term of suspension and in default of such agreement the question shall be referred to the F.I.A. whose decision thereon shall be final. Provided also that a competitor who has been notified that his entry has been accepted conditionally or that a decision has been deferred may enter for another competition on the same day on condition that if his entry for the second competition is accepted he shall forthwith notify the first Organiser.

CHAPTER 8

AUTOMOBILES

116. Classification of automobiles.

A classification of automobiles is given in the Code. Such classification shall apply to all attempts at international records. MI may adopt for competitions promoted within its territory other methods of classification than that applicable to records.

117. Identification marks.

During a competition each automobile shall carry displayed such numbers or marks if any as the Supplementary Regulations may require.

118. Dangerous Construction.

The Stewards of a Meeting may exclude any automobile the construction or condition of which they deem to be dangerous on consultation with the Scrutineers (No. 129.9).

119. Protection against Fire.

In all automobiles which take part in competitions there must be some form of protection between the engine and the driver's compartment, suitable and sufficient in the case of fire preventing the passage of flame.

120. Suspension or Disqualification of a Particular Automobile.

MI may suspend or disqualify a particular automobile in consequence of a breach of the Code or of these Rules by the entrant or the driver.

121. Advertisements on Automobiles.

No advertisement or trade sign shall be distributed from any automobile during any competition and only such advertising as may from time to time be authorised by MI may appear on an automobile during any competition - see Rule No 184.

122. Advertisement of Results of Competitions.

Any competitor or other person or body advertising the results of a competition shall state the exact conditions of the performance referred to the nature of the competition, the category, class etc. of the automobile, and the position and the results obtained, and such additional information as MI may require. The publication of an advertisement, relating to the results of a competition, drawn up in a way calculated to mislead the public, or the infraction of this Rule whether by way of omission from or addition to the particulars required to be stated or otherwise, shall render the person or body by whose authority or on whose behalf the advertisement is published or issued liable to the penalties provided by these Rules, and may entail the infliction of a penalty on the person responsible for drawing up the advertisement.



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CHAPTER 9

OFFICIALS AND THEIR DUTIES

123. List of Officials.

The staff of officials, whose duty it shall be to direct and control competitions, may include:

- * The Stewards of the Meeting
- * The Clerk of the Course
- * The Deputy Clerk of the Course
- The Assistant Clerks of the Course**
- * The Safety Officer
- The Secretary of the Meeting
- The Timekeepers
- * The Scrutineers
- The Observers
- * The Stage Commander/s
- The Flag Marshals
- The Judges
- The Handicappers
- The Starters
- ** Radio Controller
- Competitor Liaison Officer (CLO)

They shall be termed "Officials" and may have assistants to whom any of their duties may be delegated.

*All officials marked thus officiating at Stage Rallies, Hillclimbs/Sprints, or Kart or Car Race or Rallycross meetings must obtain an "Officials" Licence from MI before taking up their duties.

** Radio Controller on Special Stage Rallies must hold an official Licence of CoC grade.

124. Right of Supervision.

Apart from the Officials referred to in Rule No. 123, MI may confer on a member or members of its Motor Sport Commission, or on delegates specially qualified for such duty, the right to observe any person or body of persons organising or connected with the organisation of any competition held within the territory of MI, any person acting as an official or competing or driving in any such competition, or any holder of a licence issued by MI in any competition held in any country whatsoever under the Code. The MI may also confer on a duly authorised representative the right to uphold if necessary the interests of its own nationals, vis-a-vis the organisers of a competition organised in the territory of another A.S.N.

125. Essential Officials.

At a meeting there shall be at least two Stewards of the Meeting and a Clerk of the Course, and, in the case of competitions decided wholly or partially by time, one or more Timekeepers.

126. Nomination/Appointment of Officials.

One or more of the Stewards of the Meeting, the Safety Officer and the Scrutineers shall be appointed by MI. All other officials shall be nominated by the organisers subject to the approval of MI. A current valid licence number (Rule No. 123) of such officials

nominated by the organisers must be presented to the Stewards of the Meeting prior to the commencement of the competition. In addition the name and relevant licence number for the Clerk of the Course for Stage Rallies, Car and Kart Races, Rallycross and Hillclimbs and Sprints, must be entered on the permit application form. The duties of the MI Safety Officer are laid down in the Appendices.

127. Separation of Duties.

An official shall not, at any meeting perform any other duties than those which are clearly attached to his appointment or appointments. He shall not be eligible to compete in any competition at a meeting at which he is acting as an official. **All officials at events should adhere to a strict chain of command when looking for advice or reporting incidents or matters, that is each official should only refer a matter or incident to their immediate superior. Other than the Clerk of the Course, the Stewards or the Safety Officer, as applicable, should not contact the CEO or President in connection with a sporting issue during the course of an event.**

128. Responsibility of Stewards of the Meeting .

128.1. The Stewards of the Meeting shall not be in any way responsible for its organisation and shall not have any executive duty in connection therewith. It follows therefore, that in the discharge of their duties they do not incur any responsibility except to MI. As an exception to this principle when a meeting is promoted directly by MI the Stewards of such a meeting may combine their duties with those of the Organisers, but shall not be members of the Organising Committee. In a meeting comprising several competitions there may be different Stewards of the Meeting for each competition. When MI has appointed one or more Stewards of the Meeting such Stewards, or if more than one, the Steward appointed as the Senior Steward, shall act as chairman of the Stewards of the Meeting. There shall be at least two Stewards of a meeting. Where for any reason there are not two Stewards present, the first duty of the remaining Steward is to co-opt a suitable person so as to form a quorum.

128.2. It shall be the duty of the Stewards to remain available at an event until the time limits for the lodging of all Appeals and Notices of Intention to Appeal have expired. (See Rule 167 and Chapter 16)

128.3. In fulfilling the duty of prevention of unnecessary danger the Stewards have the authority (See Rule No 129) which over-rides that of all other officials, but unless there are exceptional considerations of urgency, this authority should be exercised by them as a Body rather than by an individual Steward. However, where an individual Steward considers it necessary to act alone, he/she is empowered to do so, but should inform their fellow Stewards and the Clerk of the Course of their action, at once.



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129. Powers of Stewards of the Meeting.

The Stewards of the Meeting shall have general power and authority to enforce compliance with the Code, these Rules, the Supplementary Regulations, the Programme, and the instructions to Drivers (if any) and to adjudicate upon any dispute and to hear any appeal against a decision given by the Clerk of the Course on a protest received by him, or against any decision by the Clerk of the Course, or any other official of the Meeting which gives rise to the imposition of a disciplinary penalty by the Clerk of the Course, and to hear any other appeal brought in accordance with these Rules, subject to the rights of appeal to the National Tribunal of Appeal, provided by these Rules (see Chapter 16). In particular they shall have power in accordance with these Rules to:

129.1. Decide whether a penalty should be inflicted, waived mitigated or increased for a breach of these Rules.

129.2. Modify the Supplementary Regulations in exceptional circumstances;

129.3. Alter composition of or consolidate heats;

129.4. Authorise a re-run in the case of dead-heats;

129.5. Authorise a change of driver or automobile;

129.6. Accept a correction made by a Judge, also see Rule 136.3;

129.7. Inflict a penalty or reprimand, fine or exclusion, but not suspension or disqualification except for 129.15;

129.8. Amend the results of a competition;

129.9. Prohibit from competition any driver who, or any automobile which, they consider to be dangerous or is reported as such by the Clerk of the Course or a Scrutineer.

129.10. Order the removal from the course and its precincts of any competitor or driver who refuses to obey the order of a responsible official and/or impose penalties as set out in Rule No. 129.7.

129.11. In the case of force majeure or for reasons of safety either

129.11.1. Postpone a competition or

129.11.2. Abandon a competition or

129.11.3. Stop a competition, declare it 'No contest' and arrange for it to be restarted. Provided that when an event is run in more than one heat or part the powers given by this sub-paragraph may, if thought fit, be exercised in respect of one heat or part.

129.12. Modify the position of the starting or finishing line or alter the Programme at the request of the Clerk of the Course or the Organisers where necessary to ensure reasonable safety for drivers or spectators.

129.13. Appoint a temporary substitute or substitutes to replace any Steward or Stewards not able to perform their duties. Such power shall be exercisable by the remaining Steward or Stewards and shall be used to ensure that there are always at least two Stewards of a meeting.

129.14. Enquire into allegations of improper driving (See Rule 139.6. for definition) and if appropriate impose penalties as specified in Rule No. 142 and Rule No. 150 and/or refer the matter to MI for hearing in accordance with Rule No. 139.6.

129.15. For offences listed in Rule 139.7 the Stewards of the Meeting may impose an immediate suspension of up to 30 days. Such sentence of suspension may only be imposed after a hearing by the Stewards and shall remain in force for the period imposed notwithstanding any Appeal by the competitor.

129.16. *During an event there should be no communication, by way of conversation, phone, text or e-mail, with the Stewards, by competitors, their agents or officials other than the CoC or Deputy CoC regarding any matter or incident that may later require adjudication by the Stewards or CoC. Stewards should not become involved in matters which may subsequently give rise to adjudication by the CoC or the Stewards.*

130. Stewards of Meeting to Report .

As soon as practicable after the conclusion of a meeting the Stewards of the Meeting shall, with the help of information provided by the Clerk of the Course, compile, sign and send to MI a report giving particulars of all protests lodged and appeals lodged, heard, action taken thereon, penalties imposed together with any recommendation in respect of such cases. The report shall also contain the Stewards general comments on the organisation of the meeting and the exercise of their own powers in relation thereto and any other observation as to the conduct of the meeting which they consider should be made to MI as the authority under whose permit the meeting was held. There shall be submitted with the report any notice of intention to appeal and appeal fees received in accordance with Chapter 16 Article 10 of these Rules. The completed report should be returned to MI at the latest 14 days after the event.

131. Duties of the Clerk of the Course.

In the case of a meeting comprising several competitions there may be a different Clerk of the Course (CoC) for each competition **at the discretion of the Organising Club**. The Clerk of the Course is responsible for the general conduct of the meeting in accordance with the Supplementary Regulations, Programme and Organising Permit and, in particular, he shall:

131.1. Keep order in conjunction with such police authorities as have undertaken the policing of the



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meeting and who are specially responsible for public safety;

131.2. Ascertain whether all Officials are at their posts, and report the absence of any of them to the Stewards of the Meeting;

131.3. Ensure that all Officials are provided with the information necessary for carrying out their duties;

131.4. Control competitors and their automobiles and take appropriate action in regard to any Entrant, Driver or Automobile he may consider to be ineligible;

131.5. Ensure that each automobile and (where appropriate) each competitor carries the proper identification marking in accordance with the programme;

131.6. Ensure that the correct driver is on each automobile and marshal the automobiles as necessary;

131.7. Send the automobiles to the starting line in the right order, and, if necessary start them;

131.8. Convey to the Stewards of the Meeting any proposal to modify the programme or any report or decision that deals with the misbehaviour, or breach of rule by an entrant or driver.

131.9. Receive protests from entrants or drivers in National competitions against another competitor, driver or automobile and immediately (or as soon as possible) hold a hearing and notify his decision in writing in accordance with Rule 165.

131.10. Collect the reports of the Timekeepers, Scrutineers and Observers together with such other official information as may be necessary for the determination of the results and take appropriate action on all reports received.

131.11. Prepare or arrange for the Secretary of the Meeting to prepare a written statement of the information necessary to enable the Stewards of the Meeting to complete their report (Rule No. 130).

131.12. At his/her discretion enquire into allegations of improper driving and if satisfied that an offence of careless driving (see Rule No.139.6 for definition) has been committed impose penalties in accordance with Rule No. 142.

The CoC may appoint assistants to investigate and report on such matters to the CoC.

131.13. At his/her discretion report to the Stewards any driver who in his view has committed an offence of reckless or dangerous driving (see Rule No 139.6 for definition) in either practice or race.

131.14. At his discretion eliminate from results a competitor for an offence for which such penalty is specified in SRs.

131.15. Impose time penalties in accordance with the SRs or these Rules.

131.16. Refuse to permit to start in any event a vehicle reported by the Scrutineer/s as being unsafe in any respect.

131.17. Impose penalties as set out in Rule No. 142 for breach of these rules or supplementary regulations including refusing to obey the order of a responsible official.

132. Duties of Secretary of the Meeting.

The secretary of the Meeting shall be responsible for the organisation of the Meeting as regards all material and notices required in connection therewith. He shall satisfy himself that the various officials are acquainted with their duties and are furnished with the necessary equipment. If necessary, he shall assist the Clerk of the Course in the preparation of the information required for the Stewards Report (Rule No. 130).

133. Duties of Timekeepers.

The principal duties of Timekeepers shall be:

133.1. At the commencement of the meeting to report personally to the Clerk of the Course for instructions;

133.2. To start competitions if so instructed by the Clerk of the Course, in the case of a handicap competition, where the handicap is applied at the start, the starter shall be a timekeeper;

133.3. In all Races and Speed Events and in other events when so required by MI to use only such apparatus for timing as is approved by MI for their use or, in the case of attempts at records where it is necessary to take times within one-hundredth of a second, such apparatus as is approved by the F.I.A.

133.4. To register such times as are appropriate having regard to the conditions of the competition, or are required by the Clerk of the Course'

133.5. To prepare and sign their own reports relating to the timing which is their individual responsibility, and to send them, with all necessary supporting documents, in the case of a meeting to the Clerk of the Course and in the case of an attempt at record or test to MI.

133.6. To send on request the original time sheets either to the Stewards of the Meeting or to MI;

133.7. To communicate any times or results only to the Stewards of the Meeting and the Clerk of the Course or in accordance with their instructions.



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133.8. When an approved fully automated timing system is in use the Organisers may appoint an experienced race official to monitor the operation of the equipment. This official will act in accordance with GCR No. 133.4, 133.6, 133.7 and may certify the results when satisfied with the performance of the system.

134. Duties of Scrutineers.

The Scrutineers are generally responsible for checking the mechanical state of automobiles both in regard to compliance with the requirements of these Rules, the Supplementary Regulations, and in the interests of safety, in particular, Scrutineers shall;

134.1. At the commencement of a meeting report personally to the Clerk of the Course.

134.2. Make inspection before a meeting at the request of the Motorsport Department or the Organisers.

134.3. Make inspections during or after a meeting if so requested by the Motorsport Department or the Clerk of the Course or as required under Rule 163.

134.4. Use only such measuring instruments as may be specified or approved by MI.

134.5. Communicate official information only to MI, the Organisers the Stewards of the Meeting and the Clerk of the Course.

134.6. Prepare and sign the reports of their inspections and hand them to MI, the Organisers, or the Clerk of the Course as may be appropriate.

134.7. Eligibility Scrutineers may be appointed by Motorsport Ireland to act at meetings involving Motorsport Ireland approved championships, or such other occasions as may be decided from time to time. These Scrutineers will be appointed for the specific purpose of determining vehicle eligibility. They will report their findings to the Chief Scutineer of the meeting and to the Clerk of the Course, or in the case of International events, to the Stewards of the meeting. On other occasions, where appropriate, they will report to the Championship Stewards.

134.8. Vehicles may be subject to sealing (as per Appendix 2) arising from action under 134.2. and 134.3. above from the time of presentation for scrutiny to the end of Parc Ferme regulations.

135. Duties of Observers and Flag Marshals.

135.1. The Observers shall occupy posts along the Course assigned to them by the Stewards of the Meeting, the Organisers, or the Clerk of the Course. As soon as the meeting commences each Observer is under the orders of the Clerk of the Course, to whom he shall immediately report by any means at his disposal (telephone, radio, signals, courier, etc.) all incidents and accidents which occur on the section of road for which he is responsible.

135.2. Flag Marshals are marshals appointed to give signals by flag to the competitors. Flag Marshals may also act as Observers when specifically assigned as such to a particular flag marshalling post as in 135.1. above.

135.3. At the end of each competition all Observers must give to the Clerk of the Course, a written report of all incidents or accidents noticed by them.

135.4. During a competition observers may be required to inform a specified official of the order in which competitors pass their post, lap by lap in the case of a circuit competition.

136. Duties of Judges.

The Judges may perform one or more of the following duties:

136.1.

136.1.1. Starting Judges whose duty is to point out to the Clerk of the Course any false starts immediately after they occur. They shall be Judges of Fact.

136.1.2. Finishing Judges whose duty is to declare the order in which automobiles cross the finishing line. They shall be Judges of Fact.

136.1.3. *In a competition where a decision has to be given whether or not a competitor has touched or crossed a given line, or upon any other fact of a similar type which has been laid down in the Supplementary Regulations for the competition, one or several Judges of Fact shall be nominated for these decisions.*

136.2. An assistant Judge may be appointed to assist a Judge or in the case of absolute necessity, to replace him. In the event of disagreement, the decision shall be given by the Judge.

136.3. The Stewards may use any video or electronic systems to assist them in reaching a decision. The Stewards may overrule Judges of Fact.

136.4. A protest may not be made against the decision of the Judge, which shall be accepted as final unless corrected as hereinafter provided. The finding, although final as regards the fact decided, shall not constitute a statement of results because it will not have taken into account the conditions under which the automobiles have completed the course.

136.5. A mistake by a Judge may be corrected by him with the approval of the Stewards of the Meeting.

136.6. The names of Judges of Fact and the facts to be judged shall be listed in either the Supplementary Regulations, the Programme of the meeting or the Final Instructions to competitors. In the latter case one copy of such listing shall be placed on the official notice board and another copy given to the Stewards of the Meeting.

136.7. At the close of the meeting each Judge shall send to the Clerk of the Course a report of his decisions.



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137. Duties of Handicappers.

The Handicappers shall, after entries have closed, prepare the handicaps in accordance with the Supplementary Regulations. They shall state if any handicap in a competition is to be increased as a result of a performance in a previous competition at the same meeting.

138. Duties of Childrens Officer.

A Childrens Officer shall be appointed for all competitions when the entry list includes competitors under the age of 18 years. It shall be the duty of such Officer to ensure that all officials, entrants, drivers and team members in such events comply with the Code of Conduct and Guidelines for Childrens Sport published by MI and the Irish Sports Council. In addition the Childrens Officer must normally be present at all hearings where persons under the age of 18 years are present as applicants, respondents or witnesses.

CHAPTER 10

PENALTIES

139. Breach of Rules.

Any one of the following offences, in addition to any other offences specifically referred to previously or hereafter, shall be deemed to be a breach of these Rules.

139.1. All bribery or attempt, directly or indirectly, to bribe any person having official duties in relation to a competition or employed in any manner in connection with a competition; and the acceptance of or offer to accept a bribe by such official or employee;

139.2. Any action having as its object the entry or participation in a competition of:

139.2.1. A person or automobile known to be ineligible therefor, or

139.2.2. A person who is not the holder of a licence appropriate to the event concerned and who is not eligible to compete without a licence by virtue of a waiver under Rule No. 102 of these Rules;

139.3. Any fraudulent act or proceeding in connection with a competition or automobile sport generally;

139.4. Any proceeding or act prejudicial to the interests of the RIAC or MI or of automobile competitions generally.

139.5. Competing for, accepting or offering to accept or advertising an award, in the nature of a title or championship in respect of automobile competitions unless such award is recognised by MI.

139.6. Careless, Reckless or Dangerous Driving.

The Stewards having found a driver guilty of careless, reckless or dangerous driving (as defined below) may impose penalties in accordance with Rule No. 142 and 150 and in the case of dangerous

driving must refer the matter to MI who after proper hearing may impose additional penalties including those specified in Rule No. 142.5. and 142.6. MI may however, in its absolute discretion order that a hearing shall take place notwithstanding that the Stewards have made no such recommendation.

139.6.1. Improper Driving.

Careless, Reckless or Dangerous Driving may constitute improper driving and Officials charged with the responsibility of inquiring into allegations of this nature should use the following definitions as a guide.

139.6.1.1. Careless.

Departing from the standard of a reasonably competent driver.

139.6.1.2. Reckless.

Performing an act, or omission, which creates an obvious and serious risk to others without due consideration of the consequences

139.6.1.3. Dangerous.

Performing an act, or omission, which creates an obvious and serious risk to others with deliberate disregard of the consequences.

139.7. Misbehaviour or Unfair Practice actual or threatened physical or verbal assault, abusive or intemperate language (See Rule 129.15.)

139.8. Failure to honour a payment to MI or to a Recognised Club or Event Organiser, will result in suspension of Competition and/or officials Licence until payment of the full amount, plus charges, has been made.

139.9. Anti-Doping and Alcohol Tests

Any form of doping is strictly forbidden. Doping is defined in accordance with the Anti-Doping Rules of the Irish Sports Council and with the addition of: Alcohol, Cannabinoids, Beta Blockers.

139.9.1. Tests.

The anti-doping rules of MI for national events are the Anti-Doping Rules of the Irish Sports Council as amended from time to time,

139.9.1.1. For international events the regulations contained in Appendix A of the FIA International Sporting Code shall be applicable and the Irish Sports Council may carry out such testing if requested by the FIA or MI.

139.9.2. Sampling

139.9.2.1. Sampling for alcohol may not be carried out more than 2 hours prior to the scheduled starting time of an event (including practice).

139.9.2.2. Sampling at events for all substances may be taken as follows.

139.9.2.2.1. For speed or similar events.

At the latest one hour after the finish of the race, run or event as the case may be, or the official notification of a withdrawal.



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139.9.2.2.2. In rallies and similar events.

at the latest one hour after the competitor's vehicle has been placed in parc ferme at the finish of the event or leg.

139.9.2.2.3. Out of competition sampling shall be taken in accordance with the Anti-Doping Rules of the Irish Sports Council. Penalties will be enforced as per Irish Anti-Doping Rules and in addition any competitor or driver may be excluded from an event or from the results of an event for breach of these rules.

139.10. Breach of Rule No.102: will result in a fixed penalty of 28 days licence suspension from the date on which any subsequent application is received by MI, or from the date on which the breach is notified to the competitor, and a fine not greater than twice the cost of the competition licence appropriate to the event concerned or twice the cost of the competition licence already held by the competitor – whichever is the greater.

140. Penalties for participation in an Unauthorised Competition.

Any person who shall enter for, drive in, officiate at, or in any manner whatsoever, take part in a prohibited, or unauthorised competition will be suspended by the ASN which has issued him his licence. Provided that if the said competition has been or is to be held outside the jurisdiction of such ASN, the two ASN's concerned shall agree as to the duration of the penalty, should they fail to agree the matter will be referred to the F.I.A. whose decision will be final.

141. Penalties.

141.1. Any Organiser, Official, Competitor, Assistant, Passenger, Driver, Affiliated Club or other Person committing a breach of the Code or the Appendices thereto, or of these Rules, Regulations and Definitions, or of any Supplementary Regulations, or of any instructions of MI published in MI Motor Sport Bulletin, or of any conditions attached to an organising permit, or of any instructions to Drivers or of any special Track Rules may be penalised as provided in Rule 142.

141.2. In the case of Clubs and Organizations (as described in Rule 13) or Officials who may be in breach of these Rules (see Rule 139), MI reserves the right to refer the matter to the Motorsport Commission who may impose penalties as set out in Rule 142.

142. Imposition of Disciplinary Penalties.

The Disciplinary Penalties which may be inflicted are, in order of increasing severity, as follows:

142.1. Reprimand.

142.1.1. Verbal.

142.1.2. Written.

142.2. Fine (See Schedule of Fees).

142.3. Time or position disciplinary penalty (as per S.R's for event) (See Rule 22.2.4.)

142.4. Exclusion (Rule No. 148).

142.5. Suspension (Rule No. 151).

142.6. Disqualification (Rule No. 152)
[see Rule No. 150 for endorsement].

143. Procedures at Hearings.

Before imposing any disciplinary penalty the Clerk of the Course, the Stewards of a Meeting or Championship or MI as the case may be, shall summon the parties concerned before them. Such summons shall either be delivered personally or in appropriate cases by post to the appropriate address (Rule No. 181) The procedure at any hearing of a Protest by the Clerk of the Course or the Championship Stewards or of an Appeal by the Stewards of the Meeting shall be in accordance with Rule No. 165. The procedure at any hearings of a Tribunal established under the rules of Chapter 16 shall be in accordance with that Chapter. All evidence should be presented before all the parties concerned.

144. Sentence to a Reprimand or a Fine.

A reprimand or a fine may be imposed by MI or by the Stewards of a Meeting or by the Clerk of the Course provided that any fine imposed shall not exceed the amount specified in the Schedule of Fees.

145. Liability to Pay.

An entrant shall, if called to do so, be responsible for the payment of any fine inflicted on his driver assistant, passenger, etc., and in such circumstances in the event of non-payment may be suspended under No. 146 of these Rules equally and simultaneously with the person on whom the fine has been inflicted.

146. Time Limit for Payment of Fines.

Fines shall be paid within 48 hours of their being ordered. Any delay in making payment will entail suspension for the period during which a fine remains unpaid. Such non-payment will result in automatic suspension of competition, entrant or official's licence, or permit suspension, and disqualification from eligibility for membership of, or participation in Bodies or Groups of Motorsport Ireland as listed in Appendix 101.

147. Allocation of Proceeds from Fines.

The proceeds from all fines, forfeited protest fees and appeals shall be remitted to MI.

148. Exclusion.

In addition to Rule 139 - A person body or automobile may be liable to sentence of exclusion:



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148.1. If shown to have been forbidden by the proper authority to take any part in or have any connection with any particular competition, or series of competitions or

148.2. Having taken any part in any competitions, if shown to have been ineligible to do so, to have been eliminated therefrom or to have been forbidden by the proper authority to participate in any award in or connection with the said competitions or to be placed therein.

148.3. Any entry fee paid by or in respect of the persons, body or automobile sentenced for the competitions to which the sentence relates shall be forfeited to the Organisers.

149. Sentence of Exclusion.

A sentence of Exclusion may be imposed by the Clerk of the Course, the Stewards or MI. A retrospective penalty may only be imposed by the Stewards or MI.

150. Endorsement of Licence.

150.1. When a penalty, as defined by Rule No. 142, other than a verbal reprimand, is imposed by the Clerk of the Course, Stewards of the Meeting or MI, details must be notified to MI on form P2 "Record of Penalty Imposed". Only penalties incurring points need be reported.

150.2. Penalty points will only be imposed for the following offences:

150.2.1. Driving - Careless, Reckless or Dangerous as per Rule No 139).

150.2.2. Safety - Failure to comply with flag or light signals.

150.2.3. General Conduct - Misbehaviour or unfair practice including actual or threatened assault (Per 139.7).

150.3. The number of points will be determined by reference to the severity of the penalty imposed for the offences noted at 150.2. above.

150.4. Points.

150.4.1. Verbal warning - 0 points.

150.4.2. Formal written reprimand - 2 points.

150.4.3. Fine, time or place penalty - 3 points.

150.4.4. Exclusion from heat or race - 4 points.

150.4.5. Exclusion from meeting - 6 points.

150.5. For the purposes of Rule No. 150 an endorsement entered on a competition licence will be considered valid for a period of twelve months from the date on which the endorsement is applied.

150.6. A competition licence upgrading signature can not be obtained at an event in which the same competition licence has been endorsed.

150.7. Should a competitor receive twelve penalty points within a twelve month period it will result in the immediate suspension of the licence for three months in addition to any other penalty imposed.

151. Sentence of Suspension.

151.1. Suspension may be either national or international.

151.2. A person, body, automobile or make of automobile shall be subject to suspension when for a certain period is forbidden by the proper authority to take part in or have any connection with any competition within the territory of MI in the case of national suspension, or within any country represented on the F.I.A., in the case of international suspension.

151.2.1. Where an individual is subject to such suspension he/she is not entitled to access official areas of an event other than with the consent of the Stewards of the Meeting. Official areas will be: Parc Ferme, Pit Area, Track or Stage, Control Rooms, Media Centre, and any other area(s) designated by the Stewards.

151.3. Suspension shall render void any entry made for a competition taking place during such suspension and any entry fee paid or payable shall be forfeited to the Organisers.

151.4. Sentence of suspension may be pronounced only by MI, with the exception of a maximum of 30 days suspension by the Stewards of a Meeting as per Rule No. 129.15. Sentence of suspension shall be reserved for grave offences.

151.5. Where a sentence of national suspension relates to a competitor or driver or official he/she shall return the licence to MI. At the expiration of the period of national suspension the licence will be returned.

151.6. Where a sentence of international suspension relates to a competitor or driver he shall immediately hand his licence to MI Motor Sport Dept. who shall return it to him when the term of international suspension has expired. Immediately after ordering a sentence of international suspension MI, shall inform the Secretary General of the F.I.A., who will inform all other A.S.N.'s, so that the sentence may be made effective.

151.7. Delay in handing in a licence in accordance with paragraph 149.5. & 149.6. of this Rule shall automatically result in the extension of the suspension by a period equal to the delay.

152. Disqualification.

152.1. A sentence of disqualification shall entail the permanent loss for the person, body, or automobile disqualified of any right to take part in any capacity whatsoever in any competition.

152.2. Disqualification shall always have international effect and shall be notified in the same manner as a sentence of international suspension.



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152.3. Disqualification shall render void any previous entry made for any competition and any entry fee paid or payable shall be forfeited to the Organisers.

153. Sentence of Disqualification.

153.1. A sentence of disqualification may be pronounced only by MI and shall be reserved for exceptionally grave offences.

153.2. Where the sentence of disqualification relates to an entrant or driver he shall immediately return his licence to MI Motor Sport Department.

154. Statement of reason for Suspension or Disqualification.

In notifying sentence of suspension or disqualification to the person upon whom sentence is passed and to the Secretariat of the FIA it shall be necessary for MI, to give its reasons for inflicting such penalty.

155. Suspension or Disqualification of Automobiles.

A sentence of suspension or disqualification may be pronounced on a particular automobile under the conditions prescribed in Rule No. 120 of these Rules.

156. Loss of Award.

Any competitor who may be excluded, suspended or disqualified from any competition shall thereby forfeit all right to award in that competition.

157. Amendment of Placing and Awards.

In such cases as are provided for in Rule No. 156 of these Rules the authority imposing the penalty shall declare the resulting amendment to the placing and awards, and they shall decide whether the next competitor in order shall be advanced.

158. Publication/Endorsement of Penalty.

158.1. The F.I.A., or MI shall have the right to publish or cause to be published a notice stating that it has penalised any person, body, automobile or make of automobile, and, the reasons therefore.

158.2. Without prejudice to any right to appeal any decision the person, persons or body referred to in such notice shall have no right of action against the F.I.A., or MI or against any person publishing or printing the notice.

159. Remission of Penalty.

MI shall have the right to remit the unexpired term of a sentence of suspension or disqualification under these Rules on such condition (if any) as it may think fit.

CHAPTER 11

PROTESTS TO CLERK OF THE COURSE AND APPEALS TO STEWARDS

160. Right to Protest.

160.1. The right to protest lies solely with any competitor or driver who may consider themselves aggrieved by any act or omission of another competitor or driver in any competition in which he is or has been taking part, or against the eligibility of another vehicle in accordance with Rule 163.2.

160.2. A competitor wishing to protest against more than one fellow competitor must lodge as many protests as there are competitors involved in the action concerned.

160.3. Nothing in this Rule shall affect or prejudice the right and duty of any official, acting in his official capacity to take such action as he may deem proper in any circumstances regardless of whether a protest has been lodged.

161. Lodging of a Protest.

Every protest shall be in writing signed by the competitor or driver making the protest and accompanied by the fee laid down in the Schedule of Fees. Such fee shall only be returned if the protest is upheld. At International Events the competitor (or his authorized representative) must make an appeal which should be addressed to the Stewards of the Meeting in accordance with the Code.

162. To Whom Addressed.

A protest arising out of a competition shall be addressed to the Clerk of the Course, subject to Rule No. 161 above.

163. Time for Protests.

163.1. A protest against another competitor or automobile shall be lodged within 30 minutes after the posting of the *provisional* results of the competition or within such further time as the Clerk of the Course may think justifiable in order to make the lodging of the protest physically possible.

163.2. A protest concerning vehicle eligibility will be lodged in accordance with Rule No. 163.1. above, except for Stage Rallies which shall be in accordance with appendix 29.6.8, and arrangements should be made for a Scrutineer to examine the vehicle and seal the appropriate components if necessary pending their official examination in accordance with the procedures laid down in Appendix 2.

If a vehicle is found not to comply with the technical regulations, it shall be no defence to claim that no performance advantage was obtained (Rule 49.3).



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The Clerk of the Course has the authority, taking all relevant factors into account, to order that a deposit as determined by him/her be lodged with MI by the person(s) protesting, MI's function being only that of a depository pending the outcome of the technical investigation. If on investigation the protest is found to be valid and is upheld the deposit will be immediately returned to the person(s) protesting. If not upheld the deposit will not be returned until it is confirmed that the person(s) protesting has paid the expenses arising, as decided by MI, which may or may not exceed the deposit paid.

The deposit must be lodged within 48 hours of the protest being accepted by the Clerk of the Course, and if not received within this period, the protest will be deemed to have been withdrawn and the protest fee forfeited.

164. Adjudication of Protests.

Any protest arising out of a meeting shall be adjudicated upon by the Clerk of the Course, subject to the rights of appeal provided by these Rules.

165. Hearing of Protests to Clerk of the Course and Appeals to Stewards.

The hearing of a Protest or Appeal shall take place as soon as practicable after the lodging of the Protest or Appeal. All parties concerned shall be given adequate notice by the adjudicators of the hearing of any Protest or Appeal. They shall be entitled to call witnesses, but shall state their case personally (in the case of a company by an officer of that company) unless prior consent has been granted by the Clerk of the Course or the Stewards (as the case may be) to representation by another person. All evidence should be presented before all the parties. Advocates may not be present at any such hearing. If a party to a Protest or Appeal fails to attend the hearing, judgment may go by default.

Before giving judgment in default of appearance the adjudicators must satisfy themselves that the party concerned is aware of the time, date, and place of hearings or has been summoned to appear in accordance with Rule No. 143. If judgement cannot be given immediately after the hearing of the parties, they must be advised of the time and place at which the decision will be given. All parties concerned shall be bound by the decision given, subject only to appeal as provided in these Rules. In making decisions the Stewards may decide that a penalty may be waived, mitigated or increased, or a fresh penalty imposed. All decisions must be given in writing to those who were party to the proceedings and in the case of the Stewards must be reasoned.

166. Distribution of Prizes.

166.1. The distribution of prizes shall not commence until half-an hour has elapsed since the publication of the results of a competition.

166.2. Where a protest or Appeal to Stewards is lodged the distribution of a prize must, if the entitlement to that prize may be affected by the decision of the adjudicators be withheld until the protest or Appeal has been adjudicated upon and either the result of any possible appeal to the National Tribunal arising out of such adjudication is known, or the time for appeal has expired without notice of appeal having been given. The list of awards in so far as it relates to such a prize must be declared to be provisional.

166.3. If after the distribution of prizes a decision is made pursuant to these Rules which affects the results of a competition, any competitor to whom a prize has been awarded but who is adjudged to be ineligible therefore shall return such prize to the Organisers on demand.

166.4. Results of Competitions shall remain provisional when mechanical components of competing vehicles have been sealed by MI Scrutineers or any other authorised body or agent (e.g. Scrutineer of another ASN) for future examination. No results may be confirmed until the findings of all technical examinations are made known.

166.5. A vehicle used in competition which contains components sealed by an authorised body or agent, or components sealed by an MI Scrutineer may be excluded retrospectively from all competitions in which it has taken part from the time when it was sealed to the time when the results of the technical examination are made known, should it prove to be in breach of regulations.

167. Right of Appeal to Stewards.

167.1. An Entrant or Driver shall have the right to appeal to the Stewards of the Meeting against any protest decisions given or any disciplinary penalty imposed by the Clerk of the Course or against any decision, act or omission of an Organiser or Official except that there shall be no right of Appeal against a refusal of entry (Rule No 70) or the decision of a Judge (Rule No. 136).

167.2. Every Appeal shall be in writing signed by the entrant or driver making the Appeal, shall be addressed to the Stewards of the Meeting and accompanied by the fee laid down in the Schedule of Fees. Such fee shall only be returned if the Appeal is upheld.

167.3. Time limits for appeals.

167.3.1. An Appeal as to the validity of entry, exclusion of entrant, driver or automobile, length of course, handicap, make-up of heat or instructions to Drivers shall be lodged at least one hour before the start of the competition, save when the conditions under which the competition is held render this impracticable. In such circumstances the Appeal shall be made with the minimum of delay, and within 30 minutes of the posting of the **provisional**



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results of the competition competition. In the case of rallies "the conclusion of the competition" shall be deemed to be the scheduled time of the last competitor at the final control extended if necessary in the case of official delay.

167.3.2. An Appeal against a disciplinary penalty imposed or decision given by the Clerk of the Course shall be lodged within 30 minutes of such penalty or decision being notified to the competitor or published on an official notice-board, or within such further time as the Stewards of the Meeting may think justifiable in order to make the lodging of the Appeal physically possible.

167.3.3. An Appeal against any mistake or irregularity occurring while a competition is taking place shall be lodged within 30 minutes after the posting of the results of the competition, or within such further time as the Stewards if the Meeting may think justifiable in order to make the lodging of the Appeal physically possible.

167.3.4. An Appeal concerning the results of a competition must be made within 30 minutes of their publication or within such further time as the Supplementary Regulations may allow or the Stewards of the Meeting may think justifiable in order to make the lodging of the Appeal physically possible.

167.4. Any Appeal arising out of a meeting shall be adjudicated upon by the Stewards of the Meeting, subject to the rights of appeal to the National Tribunal of Appeal provided in these Rules (Chapter 16).

168. Re-Run.

Neither the Stewards of the Meeting nor MI shall have power to order a competition to be re-run.

169. Ill-Founded and Vexatious Protests and Appeals.

169.1. If a protest or appeal is rejected, or withdrawn after being brought, no part of the protest or appeal fee shall be returned.

169.2. If a protest or appeal is judged partially founded, the fee may be returned in part, and will be returned in its entirety if the protest or appeal is upheld.

169.3. If it is held by the adjudicators that the author of the protest or appeal has acted in bad faith, MI may inflict on them one of the penalties laid down in these Rules.

CHAPTER 12

Note: Contents of Chapter 12 now included in Chapter 16 as noted below or revised or deleted as shown below from 1.1.10.

170.1. Powers of MI.

170.1.1. See Chapter 16 - Article 9

170.1.2. See Article 13.1.3.

170.2. See Chapter 16 - Article 9

170.3. See Chapter 16 - Article 9

170.4. Now redundant

170.5. See Chapter 16 - Article 9

170.6. See Chapter 16 - Article 9

170.7. See Chapter 16 - Article 9

170.8. See Chapter 16 - Article 9

171. National Tribunals of Appeal and Inquiry.

171.1. Right of Appeal

171.1. to 171.1.3. See Chapter 16 Article 1

171.1.4. See Chapter 16 - Article 16

171.2. National Tribunals

See Chapter 16 - Article 5 and Article 13

172. Jurisdiction of the FIA.

See Chapter 16 - Article 15

173. Procedures and Time Limits for Appeals and Inquiries.

See Chapter 16 - Articles 7, 10, 11 and 12

174. No appeal outside own Country.

See Chapter 16 - Article 12.3

175. Supporting an Appeal to the FIA.

See Chapter 16 - Article 15

176. Judgement and Appeal.

See Chapter 16 - Article 12

177. Order as to Appeal Fees, cost and fines.

See Chapter 16 - Article 13

178. Publication of Judgement.

See Chapter 16 - Article 14.

CHAPTER 13

ADMINISTRATION OF RULES

179. Interpretation of Rules.

While these rules are in conformity with the International Sporting Code they do not cover all items contained in that Code, or the Appendices thereto, but only such items as are, at time of publication, considered essential for the proper control of Motor Sport in the territory of MI Where items covered by the Code, but not specifically covered by these Rules become relevant to the proper control of Motor Sport in the territory of MI the provisions of the Code shall apply. Furthermore MI reserves the right to decide any question arising within its territory concerning the interpretation of the Code or these Rules.

180. Amendments to Rules.

MI reserves the right at any time to amend these rules and the Appendices thereto. Such amendments shall have immediate effect upon publication in the MI Yearbook or in the MI Bulletin, or on the MI website, whichever of these means MI may choose.



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181. Notices.

181.1. Any communications required under these Rules to be made to MI shall be addressed to Motorsport Ireland, 34 Dawson Street, Dublin 2.

181.2. Any communications required under these Rules to be sent to an entrant or driver shall be sent to the address on his entry form or, if they are the holder of a licence from MI or other A.S.N. to the address on the licence. Any communication to be sent to an organiser shall be sent to the address on the relevant application for an Organising Permit, or, in the case of an event not organised under permit, to the Secretary of the organising Club at the address given in the notification of the event to MI or his last known address. Any communications to be sent to an appellant under these Rules shall be sent to the address upon the notice of appeal. Any communications so sent by post shall be deemed to have reached the addressee by normal delivery of post.

CHAPTER 14

CHAMPIONSHIPS

182. Appointment and Duties of Championship Stewards.

182.1. The Motor Sport Commission shall appoint a panel of four persons who shall be Championship Stewards, any three of whom shall constitute a quorum. Such persons may not be members of the Motor Sport Commission.

182.2. Complaints and queries shall be made in writing to the Registrar of the respective Championship, who shall publish a decision on the matter/s within seven days.

182.3. The Championship Stewards shall hear Protests made in relation to Motorsport Ireland approved Championships in matters which have been the subject of a decision by any of the Registrars of the Championships. Such protests shall be addressed to the Championship Stewards c/o Motorsport Ireland and must be in writing and accompanied by a fee, in accordance with Rule No.161.

182.4. Protests brought under this Rule shall be subject to a time limit of 14 days after the publication of the decision of the Registrar. Protest Hearings shall be held in accordance with Rule No. 165.

182.5.

Decisions of the Championship Stewards shall be subject to Appeal as provided in Chapter 16.

183. MI Championships.

183.1. Regulations are published annually for the Championships approved by MI, specifying the classes, specifications, requirements and events for each Championship.

183.2.

183.2.1. MI shall be exclusively entitled to control and licence the audio and visual fixation of championship events by all media including sound recording, film and video, broadcasting and cable transmission. MI shall also retain exclusive rights of distribution over broadcast and telecommunication networks for all MI permitted events.

183.2.2. Organisers, clubs, officials and others associated with the holding of championship events shall take all reasonable steps to ensure that the entitlement of MI under this Rule shall be upheld by everyone involved in the organisation of such events, and further, shall refrain from participating in any agreement with any third party which would conflict with the entitlement of MI under this Rule.

183.2.3. No organisation, commercial entity or individual may associate itself with MI or any championship event without the consent in writing of MI.

183.2.4. MI is the proprietor of all intellectual property rights in the corporate identity, the logo and the trade mark known as "Motorsport Ireland". The same may not be used by any person or body without the consent in writing of MI and may only be used in its original form.

183.3. All organisers entering events for MI Championships must respect the provisions of this Rule and Appendix 15 of the current MI Yearbook.

CHAPTER 15

ADVERTISING AND PUBLICITY MATERIAL

184. Advertising.

The display of advertising and publicity material in all events will be free, except that:

184.1. No advertisement or publicity material may be within 150 mm of any competition number which shall be deemed to be a circle not less than 225mm in radius.

184.2. Where an advertisement or publicity material contains numerals these must not be Black and/or White, nor may they be more than 100mm in height and 25mm in width of stroke.

184.3. All advertisements must be in accordance with the code of the Association of Advertisers in Ireland.

184.4. No political advertising may be carried.

184.5. Deleted from 1.1.11.

184.6. The total area of all advertising and publicity material must be in the same plane, as, and in all over contact with the bodywork, i.e. panels, numbers, etc., standing proud of the bodywork are not permitted. No advertisement may be affixed to any transparent surface, except as allowed in Appendix 2.



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184.7. All advertising must be affixed prior to Scrutiny, and the Scrutineers shall have the power to decide whether an advertisement constitutes a danger.

184.8. Competitors may, if they wish, refuse to carry advertising matter issued by the organisers or sponsors of any event, other than the official numbers issued by the Organisers to Competitors.

185. Organisers.

185.1. Deleted from 1.1.11.

185.2. It shall be entirely at the discretion of Organising Clubs as to whether advertising and publicity material may or may not be carried on cars in a particular meeting, but Organisers exercising their rights to prohibit advertising are subject to the following regulation:

185.2.1. Supplementary Regulations must clearly state that advertising and publicity materials on cars is prohibited.

186. Interpretation.

MI shall be the final judge of whether the overall paintwork of any car including advertising and publicity material, is or is not in accordance with these rules.

187. Application Outside the Republic of Ireland.

187.1. By agreement with the MSA, cars entered by residents of Ireland holding MI Competitor's Licences may carry advertising and publicity material in accordance with these rules, when competing in events held in Northern Ireland under MSA permit. While the MSA may from time to time relax their rules in respect of foreign entrants, the foregoing agreement does not apply automatically to events in the rest of the United Kingdom, and entrants to such events should therefore, confirm the position with the Organisers at time of entry.

187.2. Cars entered by residents of any part of the United Kingdom who hold MSA Competitor's Licences may carry advertising in accordance with MSA Rules when competing in events in Ireland held under MI permit.

CHAPTER 16

RULES AND CODE OF PROCEDURE OF THE MI NATIONAL TRIBUNAL OF APPEAL AND THE MI TRIBUNAL OF INQUIRY

Article 1. Tribunal of Appeal.

The National Tribunal of Appeal will hear:

- Appeals against decisions of the Stewards of the Meeting.
- Appeals against decisions of the Championships Stewards.

- Appeals against decisions of the Tribunal of Inquiry.
- Appeals from MI under the authority of the President against decisions of the Stewards of the Meeting, the Championship Stewards or the Tribunal of Inquiry.
- Appeals by Affiliated Clubs, licence holders or officials against penalties, or sanctions imposed by the MSC under the Affiliation Agreement mentioned in Chapter 2 Rule 13.1.

Article 2. Tribunal of Inquiry.

The Tribunal of Inquiry will hear matters referred to it by MI in accordance with the rules and procedures in Article 9 herein.

Article 3. Clerk of the Tribunal.

MI will appoint a Clerk of the Tribunal who will be responsible for convening the tribunal, selecting the members of the tribunal and administering the preparation for the hearings.

Article 4. Presenters.

MI will appoint one or more Presenters, who will represent the interests of MI at all hearings.

Article 5. Tribunal Members.

5.1. For each hearing the Clerk of the Tribunal shall form a Tribunal of Appeal or a Tribunal of Inquiry of not less than three persons from a panel appointed by MI, who may not be members of the MSC, or Chairs of Committees or Advisory Groups.

5.2. In the case of the National Tribunal of Appeal it shall constitute the final body empowered to settle definitively any dispute arising out of, or in connection with, the control of Motorsport within the Republic of Ireland, save as specifically provided for in Article 15 of these rules.

5.3. No member of the panel may sit on a Tribunal hearing if he has any connection, directly or indirectly, with the matter or competition in question, or have participated in any previous decision on such matter or competition, or if there are any circumstances likely to compromise their independence in respect of one or more of the parties.

Article 6. Representation.

Parties to a hearing who intend to be represented by another person (legal or otherwise) must give at least 7 days notice of the name of such person to the Clerk of the Tribunal.

Article 7. Notification of an Appeal.

7.1. An Appeal must be formally notified to MI by post, fax or e-mail before the expiration of the time limit (as laid down in Article 10), and in the case of a fax or e-mail it is subject to written confirmation by post of the same date.

7.2. For Appeals against decisions of the Stewards of the Meeting an "Intention to Appeal" form must



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have been submitted to the Stewards within one hour of the publication of their decision.

7.3. The Appeal Fee, as per the Schedule of Fees in Chapter 17, becomes payable on notification of the intention to appeal irrespective of whether or not the appeal is proceeded with.

7.4. The Notification of an Appeal must include:

- The identity of an appellant.
- The grounds of the appeal setting out the separate pleas made.
- A copy of the contested decision and copies of any other documents on which the appellant intends to rely.
- The identity of any witnesses that the appellant intends to call, together with a list of any items that will be produced as evidence, including photographs, film, video recordings, or components of a vehicle.
- The signature of the appellant, or a duly qualified or authorised representative.

7.5. The giving of notice of appeal shall suspend the operation of any sentence or decision appealed against until the disposal of the appeal. If an appellant abandons or fails to pursue his appeal the sentence or decision shall be operative from the date of such abandonment or failure to pursue. If a sentence of exclusion, suspension or disqualification was involved, he shall be excluded from the results of any competition in which he competed subsequent to such sentence. In addition the Tribunal to which such appeal was directed may take account of any advantage gained by the appellant and may make such order as it considers appropriate in the circumstances.

Article 8. Withdrawal of an Appeal.

If an appellant wishes to withdraw an appeal after it has been brought, the request must be made in writing and no part of the appeal fee shall be returned.

Article 9. Notification of a Tribunal of Inquiry.

9.1. *The President in consultation with the CEO and the Chair of the relevant discipline*, may refer the following matters for consideration by a Tribunal of Inquiry:

- If it appears to **him** from the Stewards Report or otherwise that the results of a competition have been improperly or incorrectly made out.
- If **he** considers that there has been any breach of rules, breach of conditions of permit, defect in organisation or any other irregularity in connection with a competition.
- If **he** considers there has been a breach of the GCR's or other Regulations or Definitions as approved by MI or the FIA for the control of motorsport.

9.2. In making a decision to order a Tribunal of Inquiry **the President** shall take into account the following:

- Whether the matter is of a serious nature suitable for a Tribunal of Inquiry.
- Whether the matter could more easily or appropriately be dealt with by other means.
- Whether "prima facie" sufficient grounds exist for holding a Tribunal of Inquiry. It may also call for and examine any reports or documents deemed necessary and **he** may also take legal advice.

9.3. Having made the decision that a Tribunal of Inquiry shall be held **the President** shall instruct in writing the Clerk of the Tribunal to convene it.

9.4. Motorsport Ireland may make a submission in writing to the Tribunal in support of the decision and may nominate witnesses to appear on its behalf.

9.5. When the Clerk of the Tribunal has been instructed to convene a Tribunal of Inquiry he shall:

- Notify the Parties concerned, setting out the details of the alleged breaches of the rules, and that penalties may be imposed.
- Send the Parties copies of any relevant reports and documents, and the identity of any witnesses.

9.6. The Respondent shall have 14 days from the date of posting of the notification within which to submit his response, which must include:

- The main grounds of defence.
- The identity of any witnesses or experts he intends to call.
- Copies of any relevant documentation.
- A list of any items that he intends to produce.

Article 10. Time Limits for Appeals.

10.1. An appeal against a decision of the Stewards of the Meeting must be notified within 8 days after the written notification of such decision, provided that the notice of "Intention to Appeal" form has been given to the Stewards within one hour of the notification of their decision, or in exceptional circumstances, within such further time as the Stewards may have deemed justifiable in order to make the lodging of the "Intention to Appeal" form physically possible.

10.2. An Appeal against the decision of a Tribunal of Inquiry or the Championship Stewards or the MSC must be made within 8 days after the written notification of such decision.

10.3. An Appeal from MI under the authority of the President (See Article 1) must be made within 8 days of the written notification of such decision. In this instance no appeal fee is payable.

10.4. When an Appeal has been received, the Clerk of the Tribunal will transmit a copy of all documentation to the Respondent who will then have an opportunity to submit a written response in defence, which must also include the identity of any



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witnesses or experts that he intends to call. This submission must be made within 10 days of the posting of the notification to the Respondent by the Clerk and when received will then be communicated by the Clerk to all the parties concerned.

10.5. Motorsport Ireland may make a submission in connection with an Appeal within 10 days of the posting of the notification by the Clerk. It may also nominate witnesses to appear on its behalf.

10.6. When the circumstances so require, the Clerk, after consultation with the parties, may increase or reduce the time limits for the receipt and exchange of the grounds of appeal and the response.

Article 11. Procedures and Conduct of Hearings.

11.1. The Chairman of the Tribunal shall be entitled to issue directions with respect to the hearing and the conduct of the case in general, including the use of audio conference facilities for the submission of evidence in exceptional circumstances (See Article 11.6. below).

11.2. Hearings may proceed in the absence of any party and judgment may be given in default.

11.3. At the commencement of the hearing the Chairman will announce the purpose of the hearing and the composition of the Tribunal. He will also ascertain the identity of all persons in the room. Observers are not permitted to attend with the exception of those journalists who have made a written application to Motorsport Ireland in advance. The number of journalists shall be at the discretion of Motorsport Ireland. The recording of sound or pictures is prohibited.

11.4. If the Chairman so decides the hearing shall commence in the absence of any witnesses or experts who shall remain outside the hearing room until called and having given evidence may not leave the hearing room and speak to any other witness who has yet to give evidence.

11.5. The Appellant, the Respondent and the Presenter may make statements and call witnesses. The parties involved, the Presenter and the members of the Tribunal shall have the right to question witnesses and experts. At Tribunals of Inquiry the Presenter shall make the opening statement.

11.6. On the direction of the Tribunal, in exceptional circumstances, witnesses may be heard by means of telephonic link or other electronic means, provided that such evidence is audible to every person present at the hearing and that sufficient precautions are taken regarding the proof of identity of the witness.

11.7. If video evidence is to be used for the hearing whether to mute the audio element shall be at the discretion of the Tribunal.

11.8. In exceptional circumstances at the discretion of the Tribunal should the presence of a witness not be possible, written statements may be permitted as evidence, which should have been delivered with the parties' submission. The Tribunal may take account of the fact that the author of a written statement is not available for questioning or cross-examination in weighing the value of any written statement. Without prejudice to the above, evidence from the staff of MI shall be accepted by the Tribunal in the form of written, signed and witnessed statements which shall be furnished to the parties in advance.

11.9. When all witnesses have been heard the Chairman will then invite the Respondent and then the Appellant and the Presenter to make closing statements.

11.10. At the close of the hearing the Chairman will announce the method and date or time of the publication of the decision. The Tribunal will then deliberate in closed session before reaching the decision.

Article 12. Judgement.

12.1. Judgement of all Tribunals shall be reasoned and in writing.

12.2. In the case of appeal, the Tribunal may admit or dismiss the appeal and may decide to confirm, waive, mitigate or increase the penalty inflicted, but shall not be empowered to order any competition to be re-run.

12.3. Judgement of Tribunals of Appeal shall be final except as stated in Article 15.

12.4. In case of an Inquiry, the Tribunal may admit or dismiss the charge brought and may impose penalties as listed in GCR 142.

12.5. The Tribunal may at its discretion reserve judgement, and reconvene to deliver same or may forward judgement by post, fax or e-mail to the parties concerned.

Article 13. Order as to Appeal Fees, Costs and Fines.

When giving judgement a National Tribunal of Appeal or Tribunal of Inquiry shall make an order as to the return or forfeiture of the Appeal Fee in accordance with this rule, and if they think fit, as to costs incurred in the preparation of the case and the meeting of the Tribunal as calculated by the Motorsport Department. The costs shall consist of these expenses alone, to the exclusion of the expenses or defence fees incurred by the parties.

If the Appeal is rejected or if it is withdrawn after being brought, no part of the Appeal fee shall be returned. Should the Tribunal make an order as to



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costs or a fine, then this order must be complied with within 21 days of the date of such order being made by the Tribunal and failure to do so by a competition or entrant licence holder or an affiliated club or official's licence holder or other person, will result in automatic suspension of competition, entrant or official's licence, or permit suspension, and disqualification from eligibility for membership of, or participation in the Bodies or Groups of MI as defined in Appendix 101, until all such costs and/or fines are discharged in full.

Article 14. Publication of Judgement.

MI shall have the right to publish or cause to be published a judgement on an Appeal or an Inquiry, and to state the names of all the parties involved. Without prejudice to any right to appeal, the persons or bodies referred to in such a notice shall have no right of action against the FIA or the RIAC/MI or against any person printing or publishing the said notification.

Article 15. Appeals to the FIA.

15.1 The right of appeal against a decision of the MI National Tribunal of Appeal shall only arise if:

The appeal is brought before the FIA by another ASN and the decision appealed against relates to a licencee of such other ASN competing in an MI International event.

15.2. An MI licence holder wishing to appeal to the FIA against a decision of a National Tribunal of another ASN or a decision of the Stewards of a major FIA Championship must request MI to bring the appeal on his behalf and the procedure shall be in accordance with Article 180 of the Code and the Rules of the FIA International Court of Appeal.

15.3. The MI National Tribunal of Appeal shall be the ASN National Court of Appeal as defined in the Code.

CHAPTER 17

SCHEDULE OF FEES (Subject to review & amendment)

1. Registration of Affiliated Clubs (Rule No. 13).
1.1. Initiation Fee. €2,500

1.2. Annual Affiliation Fee. €200

1.3. Approved Organisation. €100

1.4. Recognised Organisation. €100

2. Organising Permits (Rule No. 57).
Refer to white pages for charges. Late Applications will be subject to the penalties set down in Chapter 4 Rule No. 57.

3. Competition Licences (Rule No. 107)
Refer to Appendix 1 for charges.

Licence fees may be subject to an insurance levy from time to time.

4. Venue Licence & Inspection Fees.

4.1. Permanent Circuits. €250 Plus Costs

4.2. Road Circuits. €300

4.3. Training Venue. €100

4.4. Inspection Fee

4.4.1. Permanent Venue. €2000

4.4.2. Training Venue. €500

5. Inscription Fee.

5.1. MI Calendar (Rule No. 19).

Category A Events: Stage Rallies, Race Meetings, Rallysprints & Hillclimbs = €50 per Event;
Category B Events: All other events not listed in Category A = €30 per Event.

There will normally be no refund for cancelled events. All applicants which require the date and/or type of an event to be altered from that which is inscribed in the Motor Sport Fixture List must be accompanied by an additional fee equal to twice the permit fee applicable to that event.

Such applications to be accompanied by a letter of support from Club/s organising events of a similar discipline on the same weekend.

5.1.1. Permit Fines.

Late Permit Application = Fine 1 extra permit fee
New Event Application = Fine 1 extra permit fee
Cancellation of event = Fine 2 extra permit fees
Alter Date of Event = Fine 2 extra permit fees

5.2. International Calendar (Rule No. 18)

All entries on the International Sporting Calendar are subject to a fee payable to the F.I.A. This fee will vary according to the category of the event. There will also be a supplementary fee for any event inscribed on the Calendar and subsequently cancelled. The scale of such fees are fixed each year by the F.I.A. Application for International status may only be applied for through the A.S.N. under whose jurisdiction the event takes place.

A change of date already published in the International Calendar will also be subject to a supplementary fee. International events which include any form of advertising such as the names of the sponsor, will be subject to double the normal inscription fee.

5.3. Any club which fails to run an event two years in a row may not automatically be granted that date/permit on the third year. Cancellation of an event may be subject to penalties as set out in GCR chapter 17 5.1.



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6. Fines (Rule No. 144)

6.1. Clerk of the Course Maximum Fine - €250.

6.2. Stewards of a Meeting Maximum Fine - €1000
or the amount of the highest cash award for the
event whichever is the greater.

6.3. Safety Plans: (Late Submission)

€12 per day for the first week that the plan
is overdue.

€32 per day for the second week that the plan
is overdue.

€65 per day thereafter.

See Rule No.146 "Time Limit for Payment of Fines"

7. Protest and Appeal Fees

7.1. Protests to Clerk of the Course or Championship Stewards

National €200

7.2. Appeals to Stewards (Rule No. 169)

National €300

International €500

7.3. National Tribunal of Appeal (Chapter 16)

National - €1,000

International - €2,000

8. Championship Permits.

For current rates of fees and fines refer to
Appendix 15.

9. Entrant Licences.

9.1. Commercial Entrant.

National - €350

International - €1,175

9.2. Competitor Entrant.

National €30

International €310



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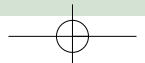
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APPENDICES TO THE GENERAL COMPETITION RULES

REGULATIONS AND DEFINITIONS

In the event of a conflict the following priority will apply:
International Events: FIA Yearbook, MI GCR's, MI Appendices
National Events: MI GCR's, MI Appendices, FIA Yearbook

RIAC / Motorsport Ireland (MI)

All references to RIAC in the
context of Motorsport will be taken to include the title "Motorsport Ireland"
or its abbreviation "MI"



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COMPETITION LICENCES

APPENDIX 1

1

REQUIREMENTS FOR THE ISSUE OF COMPETITION LICENCES

1. EU LICENCE:

1.1. VALIDITY:

Motorsport Ireland Competition licences of the grade "National A" or "International" will be recognised throughout the EU in accordance with Article 47 of the FIA International Sporting Code and 1.2 below. Under a reciprocal agreement licences issued by MI will be deemed valid for all events run under MSA permits, as per rule C (a) 51, in the United Kingdom provided the holder complies with all the requirements for the event.

1.2. AUTHORISATION:

In order to avail of the facility provided for in 1.1 above, the permit for the event must contain the following declaration "National Event with Authorised Foreign Participation (NEAFP)".

2. LICENCE GRADES:

The following Grades of licence will be issued by Motorsport Ireland:

2.1. INTERNATIONAL:

To cover all International Events.

2.2. NATIONAL A:

To cover all National Events run under the jurisdiction of any of the ASNs of the EU. Ref. 1.2 above.

2.3. NATIONAL B:

To cover all National Events run under the jurisdiction of Motorsport Ireland with vehicle c.c. restrictions where applicable noted on the Licence Table Appendix 1 Article 1. This licence is also valid for MSA National B permitted events.

2.4. DELETED.

2.5. NAVIGATOR:

To cover all National events run under the jurisdiction of any of the ASNs of the EU and all International Events run under the jurisdiction of Motorsport Ireland. "Navigator" gradelicense holders are not entitled to drive at any time during an event.

2.6. JUNIOR KART.

Junior Kart Licences (12-16 years of age inclusive) will be issued for Kart Racing and will cover all National events run under the jurisdiction of Motorsport Ireland and MSA and is valid only when accompanied by a Competitor Entrant Licence Ref. GCR Chapter 7, 108.2. Applicants who are over 14 years of age on 1st January of the current year and exceed a height of 182 cm may apply to compete in a senior category or class subject to them having obtained an International C licence while competing in a junior category

prior to application. Once upgraded to a Senior Class a competitor may not revert to a Junior Class. See Appendix 70 Article 12 for class categories.

2.7. CADET KART.

Cadet Licences (8-12 years of age inclusive) will be issued for Kart Racing and will cover all National events run under the jurisdiction of Motorsport Ireland and MSA and is valid only when accompanied by a Competitor Entrant Licence Ref. GCR Chapter 7, 108.2.

2.8. CLUBMAN JUNIOR.

Junior Licences (14-16 years of age inclusive) will be issued for Sporting Trials, **Midget Car Racing**, 4x4 Production Vehicle Trials and Production Car Trials will cover all National events run under the jurisdiction of Motorsport Ireland.

2.9. JUNIOR SPEED.

Junior Speed licences (14-16 years of age inclusive) will be issued for Grass Surface Autocross events **Rallysprint events** and Rallycross events only. The Licence is valid for National events run under the jurisdiction of Motorsport Ireland and MSA (where applicable). Rallycross events only. **The Licence is valid for National events run under the jurisdiction of Motorsport Ireland and MSA (where applicable) and MSA permitted Single Venue Stage Rallies open to Junior Rally drivers in Accordance with R 45.**

2.10. JUNIOR RACE.

Junior Race Licences (14-16 years of age inclusive) will be issued for Circuit Racing events only. The licence is valid only for the Ginetta Junior Ireland Series (see Appendix 53) or the equivalent MSA series. This licence is valid only when accompanied by a Competitor Entrant Licence (GCR 108.2).

3. ROAD TRAFFIC ACT LICENCE.

For Stage Rallies and Navigation/Retrospective Trials, drivers Competition Licences will be issued only to holders of current full Road Traffic Act (RTA) Driving Licences. It will not be necessary to produce your Driving Licence when you are applying for a Competition Licence. Applicants are reminded that it is a breach of GCR No. 139.3 to make an incorrect or false statement on a Competition Licence Application Form and this may result in disqualification of the applicant. Refer to the Licence Table Appendix 1 Article 11 for RTA Licence Requirements.



COMPETITION LICENCES

APPENDIX 1

1

4. EXPIRY DATE.

Competition Licences expire on the 31st December of the year for which they are issued. A competitor may use a **2012** licence during December **2011**.

5. SIGNATURE.

A Competition Licence is not valid without the signature of the holder.

6. LICENCE APPLICATIONS.**6.1. RENEWAL FORMS.**

Renewal forms for Competition Licences will be sent out in November of the previous year.

6.2. CLUB MEMBERS.

Members of affiliated clubs must have their applications stamped in the appropriate box by that club.

6.3. NON-AFFILIATED CLUB MEMBERS.

For Licence renewals non-members are charged at twice the members rate. First time applicants refer to Article 6.8.

6.4. PHOTOGRAPHS.

It is obligatory for all **first time applicants** to supply ONE recent passport size photograph of themselves with their application. The licence is not valid without the photograph of the holder.

6.5. MEDICAL REQUIREMENTS.

All applicants intending to participate as drivers in Speed events including Races, Rallycross, Rallysprint, Autocross, Hillclimbs, Sprints, Karting, Midget Car Racing and all Rallies with Special Stage(s), must submit a completed Medical Declaration and Doctors Certificate in accordance with 6.5.1, 6.5.2 and 6.5.3 below.

6.5.1 MEDICAL DECLARATION. (Red Section)

The Medical Declaration must be completed annually, in full and signed by the applicant. If the applicant is under 18 years of age the declaration must also be signed by a parent or guardian.

6.5.2. DOCTORS CERTIFICATE. (Blue Section)

The Doctors Certificate must be completed, STAMPED, and signed by a Doctor. If your Doctor does not have a stamp, their signature on an official letterhead or prescription page containing their name and qualifications must accompany the completed Doctors Certificate.

The following criteria apply:

8-18 years inclusive-	Doctors Certificate required on first application only and there after the Medical Declaration will suffice.
19-44 years inclusive-	Doctors Certificate required every second year with the Medical Declaration each year.

45 years and over-

Annual Certificate required including satisfactory results of a Stress related ECG, if applicable (ref 6.5.3)

Under certain medical circumstances, Motorsport Ireland reserves the right to request the applicant to submit more than one Doctors Certificate within the relevant period specified above.

6.5.3 ECG.

Applications for "International" grade licences from competitors aged 45 years or over must pass a stress related ECG test and have their Doctor complete sections 2(a) and (b) of the "Doctors Certificate". (Do not send Trace). This test must be completed within one month of the application. This ECG will be valid for the year in which the test was passed plus two calendar years.

6.6. ISSUE.

Licences will be issued by post and should be applied for at least 14 days before they are required to ensure they are received in good time. If an applicant doesn't receive the licence, after allowing for the processing and delivery time, MI should be notified in writing. If the notice is received by MI within 8 weeks of despatch of the licence by MI, a replacement will be issued. After 8 weeks has elapsed, the normal replacement charge will be levied in accordance with Article 7 below.

6.7. PRIORITY.

In extreme cases and at the sole discretion of Motorsport Ireland, "Cover Notes" may be provided. This service will only be considered if the priority fee of €25.00 has been submitted with the application.

6.8. FIRST TIME LICENCE APPLICATIONS.**6.8.1. AFFILIATED CLUB MEMBERS.**

Competition Licences will be issued to first time applicants at a special concessionary price of €40.00. Refer also to Art. 10.

6.8.2. FIRST TIME LICENCE RENEWAL.

Any applicant who applies on or after the 1st September in any given year will be provided with an option to renew their First Time licence for a second year at this concessionary price. Applicants who wish to upgrade their licences will be required to pay the full rate applicable at that time.

6.8.3. NON-AFFILIATED CLUB MEMBERS.

Non-club members will be charged a supplementary fee of €40.00.

6.8.4. FIRST TIME RACE OR STAGE RALLY.

Applicants for a first time Race or Stage Rally Driver licence must first purchase an "Introduction to Motorsport" Starter Pack at a cost of €10, which is available from the Motorsport Ireland Office. In addition applicants must also complete a licence form and successfully complete a Motorsport



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Ireland "Introduction to Motorsport Course" (IMC). Full details are available from the Motorsport Ireland Office.

First time applicants for Competition Licences for all Junior categories (Junior Kart, Cadet Kart, Junior Speed, Junior Race and Junior Clubman) will be issued with licences free of charge.

7. REPLACEMENT.

Applications for replacement of licences will only be accepted in writing and there will be a €20.00 fee.

8. LAPSED LICENCES.

8.1. NATIONAL A.

Deleted.

8.2. INTERNATIONAL RACE

C/INTERNATIONAL RALLY.

Lapsed "International Race C" and "International Rally" Licence holders may be issued with the same grade Licence provided they were already the holders of an "International" grade licence in the previous 6 years i.e. **2006-2011** inclusive. Otherwise, they may be issued with a "National A" grade licence whereby they must upgrade again in accordance with Article 9 below. See also Article 8.4 below.

8.3. INTERNATIONAL RACE B and INTERNATIONAL RACE A.

Lapsed "International Race B" and "International Race A" Licence holders should refer to Article 4.4, Appendix "L" of the FIA International Sporting Code.

8.4 LICENCES HELD PRIOR TO 1986.

Any person who last held a competition licence prior to 1986 may be issued with a "National A" grade licence, on condition that they can provide independent confirmation, by way of programme, results, newspaper, video or photograph, that they competed in the branch of the sport to which their application refers. Otherwise, they will be issued with the appropriate First Time Licence in accordance with Article 6.8 above.

9. UPGRADING OF LICENCES:

9.1. REQUEST:

A request for upgrading of your Licence should be made in writing to Motorsport Ireland. You must ensure that you include copies of the official results with your request. If official results are not available, the signatures of the Clerk of the Course or Motorsport Ireland Steward on your upgrading card will be accepted. Navigating in an event does not entitle the licence holder to an upgrading signature.

9.2. FEE:

The up-grading fee will be the difference in cost plus €10.00.

9.3. UPGRADING REQUIREMENTS:

Licences are divided into 5 specific categories - Race, Kart, Rally, Speed and Clubman. They may be upgraded as follows:

9.3.1. RACE.

To upgrade a RACE category licence competitors may only use RACE event finishes and these must have been obtained within the **36** month period prior to application.

National B to National A - 5 event finishes required

National A to International C - 5 event finishes required

Successful completion of a MI Introduction to Motorsport Course will count as 1 event finish for upgrading. Upgrades beyond International C will be in accordance with FIA requirements.

9.3.1.1 INTER-CATEGORY UPGRADES:

STAGE RALLY National A or International to RACE National A - Completion of a MI Car Racing Extension Course required.

SPEED National A to RACE National B - Completion of a MI Car Racing Extension Course Required.

SPEED National A to RACE National A - 5 additional Hillclimb or Rallycross finishes within the **36** month period prior to application and completion of a MI Car Racing Extension Course required.

SPEED International to RACE/KART National A - proof of 5 finishes in Hillclimb or Rallycross events within the **36** month period prior to application and completion of a MI Car Racing Extension Course required.

KART National B to RACE National B (having completed a MI Introduction to Karting Course) - 5 Kart event finishes obtained on separate dates and within the **36** month period prior to application required.

9.3.2 RALLY:

To upgrade a RALLY category licence competitors may only use STAGE RALLY event finishes as a driver and these must have been obtained within the **36** month period prior to application.

National B to National A - 4 event finishes required

National A to International - 4 event finishes required

Successful completion of a MI Introduction to Motorsport Course will count as 1 event finish for upgrading.

Navigator to International: For the purposes of competing in International events outside the jurisdiction of Motorsport Ireland, an "International" grade RALLY licence, restricted to co-driver only, may be issued subject to the approval of the Motor Sport Commission (MSC) and provided the following conditions are met:



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- (a) The applicant must complete co-driver upgrade application form L3.
- (b) The applicant must be the holder of a current full RTA Driving Licence in accordance with Article 3 above.
- (c) The applicant must submit a completed Medical to Motorsport Ireland in accordance with Article 6.5 above.

9.3.3. SPEED:

To upgrade a SPEED category licence competitors may only use SPEED event finishes and these must have been obtained within the **36** month period prior to application.

National B to National A - 6 event finishes required

National A to International - 6 event finishes required.

9.3.4. KART:

To upgrade a KART category licence, competitors may only use KART race event finishes and these must have been obtained within **36** month period prior to application. Only results obtained in finals will count. For International C licences, age restrictions will be in accordance with CIK requirements. Upgrades beyond International C will be in accordance with CIK requirements.

Junior Kart to International C-Junior - 3 top 6 finisher required.

Junior Kart to International C-Senior - 3 top 6 finishes required.

National B to International C-Senior- 3 top 6 finishes required.

9.3.5. JUNIOR SPEED LICENCE UPGRADE TO JUNIOR RACE LICENCE

To upgrade a JUNIOR SPEED licence to JUNIOR RACE licence - proof of 5 finishes in Rallycross within the **36** months period prior to application and completion of the MI Car Racing Extension Course required.

9.3.6. JUNIOR KART LICENCE UPGRADE TO JUNIOR RACE LICENCE

To upgrade a JUNIOR KART licence to JUNIOR RACE licence - proof of 5 Kart finishes within the **36** months period prior to application and completion of the MI Car Racing Extension Course required.

10. ONE EVENT LICENCES.

These licences will be available for CLUBMAN category events ie. Sporting Trials, 4x4 Production Vehicle Trials, Production Car Trials, Midget Car Racing (Medical Required), specifically approved Autotest events (special safety requirements will apply) and for Navigators in Rallysprint events. These licences will cost €25.00 and may be obtained on the day from the event Secretary. An official event Entry Form must also be completed by the applicant and the appropriate entry fee also paid.

10.1. First Time licence applicants who have bought a One Event licence may, on the inclusion of their blue "competitors copy" receipt with their application, receive a €25.00 reduction in the cost of their FIRST full competition licence.

10.2. *For all events where One Event licences are available, the same age requirements apply as for an annual competition licence.*



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11. 2012 LICENCE CATEGORY TABLES.

4.

SECTION 6. Licence Categories:

- Please refer to the following tables to determine the licence type you need and the requirements for each licence.

☒ Licence suitable for event type
 ☐ N/A Licence unsuitable for event type

RACE/KART						
RACE - Suitable for Car and Kart Racing. KART - Suitable for Kart Racing only.						
LICENCE CRITERIA	CADET KART	JUNIOR KART	JUNIOR RACE	NATIONAL B	NATIONAL A	INTERNATIONAL
AGE	8-12 inc	12-16 inc	14-16 inc	16+	16+	16+
RTA LICENCE REQUIREMENTS	NO RTA	NO RTA	NO RTA	NO RTA	NO RTA	NO RTA
MEDICAL REQUIREMENTS	MEDICAL	MEDICAL	MEDICAL	MEDICAL	MEDICAL	MEDICAL
ENGINE SIZE LIMIT	60 cc - 2 stroke	125 CC Direct Drive	SEE BELOW	SEE BELOW	UNLIMITED CC	UNLIMITED CC
KART RACING	✓ (a)	✓ (a)	125 CC DIRECT DRIVE ✓ (a)	125 cc - 2 stroke 600 cc - 4 stroke ✓ (a)	N/A	✓
CAR RACING	N/A	N/A	1400 CC ✓ (a)(f)	2000 CC INTRODUCTORY COURSE REQUIRED ✓	✓	✓

(a) A Competitor Entrant Licence is required for all Cadet and Junior Race / Kart Licence holders
 (f) As per Junior Class Regulations

CLUBMAN		
Suitable for: Sporting Trials, 4 x 4 Trial, Production Car Trials, Midget Car Racing, Autotests & Autosolos.		
LICENCE CRITERIA	JUNIOR	NATIONAL B
AGE	14-16 YEARS	16+
RTA LICENCE REQUIREMENTS	NO RTA	NO RTA
MEDICAL REQUIREMENTS	NO MEDICAL	NO MEDICAL
SPORTING TRIALS	✓	✓
4X4 PRODUCTION VEHICLE TRIALS & PRODUCTION CAR TRIALS	✓	✓
MIDGET CAR RACING	MEDICAL	MEDICAL
AUTOTESTS & AUTOSOLOS	N/A	✓

RALLY				
Suitable for: Navigation & Retrospective Trials (Road) and Special Stage Rallies (Stage Rally).				
LICENCE CRITERIA	NAVIGATOR	NATIONAL B	NATIONAL A	INTERNATIONAL
AGE	16+	17+	17+	17+
RTA LICENCE REQUIREMENTS	NO RTA	FULL RTA	FULL RTA	FULL RTA
MEDICAL REQUIREMENTS	NO MEDICAL	SEE BELOW	MEDICAL	MEDICAL
ENGINE SIZE LIMIT	N/A	SEE BELOW	UNLIMITED CC	UNLIMITED CC
NAVIGATION & RETROSPECTIVE TRIALS (ROAD)	✓	NO MEDICAL ✓	✓	✓
SPECIAL STAGE RALLIES (STAGE RALLY)	✓	MEDICAL ✓ 1650 CC INTRODUCTORY COURSE REQUIRED	✓	✓

Notes:
 (b) IRDS Insurance Cert. is required for driving in all Rally Category Events.

SPEED				
Suitable for: Rallycross, Rallysprint, Sprint, Hillclimb & Autocross.				
LICENCE CRITERIA	JUNIOR	NATIONAL B	NATIONAL A	INTERNATIONAL
AGE	14-16yrs	16+	16+	16+
RTA LICENCE REQUIREMENTS	NO RTA	NO RTA	NO RTA	NO RTA
MEDICAL REQUIREMENTS	MEDICAL	MEDICAL	MEDICAL	MEDICAL
ENGINE SIZE LIMIT	See (c) Below	1650 CC	UNLIMITED CC	UNLIMITED CC
RALLYCROSS	✓ (c) (d)	✓	✓	✓
RALLYSPRINT	✓ (c) (d)	✓	✓	✓
SPRINT	N/A	✓	✓	✓
HILLCLIMB	N/A	✓ (e)	✓ (e)	✓
AUTOCROSS LOOSE & GRASS SURFACE	✓ (c) Grass surface only	✓	✓	✓

Notes:
 (c) CC Restrictions apply and are as per the relevant Junior Class Regulations.
 (d) Competitor Entrant Licence Required
 (e) Hillclimb competitors competing in classes 3, 6, 7 and 9 are required to have a minimum of a Speed National A Licence.

CHECKLIST

- Have you completed page 1 in full? • Has your Affiliated Club stamped your application form, if applicable? • Have you included 1 passport size photograph? • Have you completed your part of the Medical Declaration on page 3, if applicable? • Has your Doctor completed and stamped the Doctors' Certificate on page 3, if applicable? • Have you enclosed payment including Priority etc. where applicable? • Have you applied for IRDS Insurance if you wish to drive in Rallies or Navigation Trials? • Have you completed an Introduction to Motorsport Course if required? • Have you included your Upgrading Card with the correct number of signatures if you require an upgrade.



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12. ORGANISING CLUB REQUIREMENTS FOR THE ISSUE OF ONE EVENT LICENCES.

12.1. One Event licences will be available for CLUBMAN category events ie. Sporting Trials, 4x4 Production Vehicle Trials, Production Car Trials, Midget Car Racing (Medical Required), specifically approved Autotest events (special safety requirements as listed in Appendix 90 will apply) and for navigators in Rallysprint events.

12.2. Clubs may buy booklets of 5 One Event licences from Motorsport Ireland (MI). These are to be paid for in advance and will cost €125.00 per booklet. Clubs may buy a maximum of 2 booklets at any one time (ie. 10 licences).

12.3. Clubs will be refunded for unused One Event licences on condition that the unused licences are returned to MI.

12.4. A competitor may buy a One Event licence on the day of the event for which it is required, from the organising club, at a cost of €25.00.

12.5. The Event Secretary must ensure that the applicant fully completes and signs the One Event licence. An official event entry form must also be completed by the applicant and the appropriate entry fee paid.

12.6. Each One Event licence has 3 parts which should be dealt with in the following way:

12.6.1. The top white "MI Copy" remains in the booklet.

12.6.2. The pink "Club Copy" should be removed and kept for the club's records.

12.6.3. The blue "Competitors Copy" should be removed and given to the competitor. This copy will entitle the competitor to receive a €25.00 reduction in the cost of their FIRST full competition licence in accordance with Article 10.1 above.

12.7. When all 5 licences have been issued, the booklet containing the 5 white MI copies must be returned to MI. No further One Event licence booklets will be issued to the club if there are outstanding returns which have not been made.

12.8. Motorsport Ireland and its Affiliated Clubs reserve the right to refuse to issue a One Event licence. (GCR 105).

13. SPECIAL LICENCE FOR DISABLED PERSONS FOR DRIVING IN EVENTS

13.1. With the exception of progressive or chronic illnesses which preclude the practice of motor sport (refer to 1.5. of Chapter II Appendix L of the

FIA International Sporting Code) and disqualifying visual disorders (refer to 1.3 Chapter II of Appendix L of the FIA International Sporting Code), any person with a acquired or congenital disability and who, in the opinion of Motorsport Ireland may not obtain a National driver's licence, may obtain a National Licence for Disabled Drivers, if all the conditions stated below are fulfilled. This licence is not eligible for single seater car races.

13.1.1. Participation of competitors holding a National Licence for Disabled Drivers is subject to the agreement of the ASN of the country of the event and of the ASN that issued the licence.

13.1.2. The secretary of a meeting in which both disabled and non-disabled persons are competing should ensure that the rescue services are aware of the competition numbers of the cars of the holders of a licence for disabled persons.

13.2. Conditions of issue.

the criteria of the awarding of this licence are evaluated on three levels: Medical, Sporting and Technical.

13.2.1. Medical Evaluation.

The disabled driver, having applied for the licence, shall be evaluated by a physician appointed by Motorsport Ireland. For licence renewal, a completed Medical Declaration and Doctor's Certificate must be submitted on an annual basis.

13.2.2. Sporting Evaluation.

If the evaluating physician agrees that the licence for disabled persons should be granted, the applicant must undergo a driving evaluation at a venue designated by Motorsport Ireland and be capable of attaining a performance equal to that of non-disabled drivers at the same level of competition. Applicants for a first time Race, Kart or Stage Rally Driver licence must successfully complete a Motorsport Ireland "Introduction to Motorsport Course (IMC)" in accordance with Article 6.8.4. of this Appendix. The applicant must provide his or her own vehicle for their evaluation or IMC course. Furthermore, and as appropriate, the capacity for extricating themselves as quickly as possible from their vehicle, in case of immediate danger (accident, fire etc.) shall be evaluated at the same time:

- The applicant must, from a sitting position in the type of car to be used, be able, alone, to extricate him or herself and move away from the car, whether the car is upright or on its side.
- In a prone position, they must be able to turn easily over both ways.



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13.2.3. Technical Examination.

The applicant must obtain a competition Logbook for their vehicle, which must be examined by a Motorsport Ireland appointed Scrutineer. Any special modifications that have been made to the vehicle must be approved and recorded in the Logbook. If the vehicle has previously been issued with a Logbook, this must be returned to MI and the vehicle must undergo a new inspection. To avoid disappointment, vehicle inspection requests must be received by MI at least 4 weeks prior to any intended participation in competition.

13.2.4. When Motorsport Ireland has received the medical, technical and sporting evaluations, it will take the final decision as to the granting or denial of the licence.

13.2.5. The licence will be valid only for the vehicle for which the applicant was evaluated and if another vehicle is to be used, then an additional sporting evaluation and technical examination must be carried out for that vehicle.

14. SPECIAL LICENCE FOR DISABLED PERSONS FOR NAVIGATING IN NATIONAL RALLY EVENTS

14.1. This licence is reserved for persons physically disabled through injury or infirmity, including disqualifying visual disorders (excluding blindness), and for persons suffering from certain ailments that may be incompatible with the practice of motor sport. It allows participation in rally events as defined in Article 21 of the FIA International Sporting Code.

Under no circumstances does it allow the holder to drive a vehicle during the running of a competition.

The licence will be valid only for the vehicle in which the applicant was evaluated and if another vehicle is to be used, then an additional safety examination must be carried out for that vehicle.

14.2. Medical Evaluation.

Evaluation of the applicant's physical capabilities is required and the applicant must submit a completed Medical Declaration and Doctor's Certificate on an annual basis.

14.3. Safety Evaluation.

A timed evaluation of the applicant's ability to exit the vehicle unaided will be carried out:

- The applicant must, from a sitting position in the type of car to be used, be able, alone, to extricate him or herself and move away from the car, whether the car is upright or on its side.

- In a prone position, they must be able to turn easily over both ways.

14.4. When Motorsport Ireland has received the medical and safety evaluations, it will take the final decision as to the granting or denial of the licence.



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1.
MOTORSPORT IRELAND, 34 DAWSON STREET,
DUBLIN 2
Tel: 01-677 5628 Fax: 01-671 0793

FIRST TIME

APPLICATION FOR 2012 COMPETITION LICENCE **L1**

This form must be completed in full.

**AFFILIATED
CLUB
STAMP**

No Stamp = Twice Price!

- Please read the notes on the Yellow Licence Notes Sheet enclosed CAREFULLY before applying for any Licence. If you have any questions, or need help filling in this form, please phone our Licence Department at the above number.
- **ONE RECENT PASSPORT SIZE PHOTOGRAPH MUST BE INCLUDED WITH THIS APPLICATION, UNLESS PREVIOUSLY SUPPLIED.**
- Licences are valid until 31st December 2012.

SECTION 1. Your Details

SURNAME																								
FIRST NAME																			SEX	M	F			
ADDRESS																								
TOWN																								
COUNTY																								
TELEPHONE - HOME																								
TELEPHONE - WORK																								
EMAIL																								
<input type="checkbox"/> Please tick if you do not wish to receive updates by email																								
DATE OF BIRTH					NATIONALITY																			
CURRENT VALID FULL R.T.A. DRIVING LICENCE NO.						COMPULSORY FOR RALLY DRIVERS										EXPIRY DATE								

SECTION 2. Declaration and Signature/s:

- I hereby apply for registration for the year 2012 on the Competitors and Drivers Register of Motorsport Ireland and I undertake, if registered, to submit to and be bound by the International Sporting Code of the FIA and the General Competition Rules and Regulations of Motorsport Ireland, the Irish Anti-Doping Rules and any regulations supplementary thereto as may be imposed from time to time by Motorsport Ireland.
- I have read and understood the terms of issue and am a permanent resident of the Republic of Ireland and/or I am an Irish National and I do not hold a Competition Licence from any other ASN.
- I hereby agree to abide by the guidelines and regulations contained in Motorsport Ireland's General Code of Conduct and Code of Conduct for Children's Sport.

Your Signature:

Date:

If you are 17 or under, your parent or legal guardian must also sign below.

Parent's or Guardian's name:

Signature:

Relationship to you:

FOR OFFICIAL USE ONLY

€ _____

MEDICAL : NO / YES / YEAR

Continued over the page



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COMPETITION LICENCES

With effect from 01.12.07, first time applicants for Competition Licences for all Junior categories (Junior Kart, Cadet Kart, Junior Speed and Junior Clubman) will be issued with licences free of charge.

2.

SECTION 3. Licences you need: (Refer to the Licence Category Tables on page 4)

- The Licence fee is €40 for members of clubs affiliated to Motorsport Ireland, regardless of how many categories you apply for.
- If you are not a member of an affiliated motor club, you must also pay the Non Club Member Supplement.
- If you require a Cover Note to be issued, you must pay the Priority Fee (Ref. Article 6.7 of the Yellow Licence Notes sheet.)

RACE & STAGE RALLY DRIVER LICENCES:

Applicants must complete an Introduction to Motorsport Course (IMC) prior to being issued with any of the following licences. Refer to Article 6.8.4 on the Yellow Licence Notes sheet.

I wish to apply for the following licence(s) (please place ☒ in the appropriate box(es))

LICENCE CATEGORY	CODE	€	<input checked="" type="checkbox"/>
RACE			
National B - Driver (16 years +)	024	40.00	
Junior Race (14 - 16 years)	177	FREE	
STAGE RALLY			
National B - Driver (17 years +)	027	40.00	

This box **must** be stamped by the school to show that you have successfully completed your IMC Course.

ALL OTHER LICENCES, INCLUDING NAVIGATION TRIALS AND NAVIGATORS:

No course attendance is required prior to issue of any of the following licences

I wish to apply for the following licence(s) (please place ☒ in the appropriate box(es))

LICENCE CATEGORY	CODE	€	<input checked="" type="checkbox"/>
STAGE RALLY			
Navigator (16 years +)	028	40.00	
ROAD (for Navigation & Retro Trials)			
National B - Driver (17 years +)	036	40.00	
KART			
National B (16 years +)	172	40.00	
Junior Kart (12-16 years)	025	FREE	
Cadet Kart (8-12 years)	026	FREE	
SPEED			
National B (16 years +)	029	40.00	
Junior (14 - 16 years)	042	FREE	
CLUBMAN			
National B (16 years +)	030	40.00	
Junior (14 - 16 years)	031	FREE	
OTHER FEES	CODE	€	<input checked="" type="checkbox"/>
Priority Fee = €25.00	021	25.00	
Non-Club Member Supplement = €40.00	023	40.00	

PLEASE INDICATE THE MAIN TYPE OF MOTORSPORT IN WHICH YOU INTEND TO COMPETE.

TICK ONE ONLY

RACE/KART EVENTS	SPEED EVENTS
<input type="checkbox"/> Kart Racing <input type="checkbox"/> Circuit Racing	<input type="checkbox"/> Rallycross <input type="checkbox"/> Rallysprint <input type="checkbox"/> Sprint <input type="checkbox"/> Hillclimb <input type="checkbox"/> Autocross
RALLY EVENTS	CLUBMAN EVENTS
<input type="checkbox"/> Special Stage Rallying <input type="checkbox"/> Historic Stage Rallying <input type="checkbox"/> Navigation Trials <input type="checkbox"/> Retrospective Trials	<input type="checkbox"/> Sporting Trials <input type="checkbox"/> 4x4 Production Vehicle Trials <input type="checkbox"/> Production Car Trials <input type="checkbox"/> Midget Car Racing <input type="checkbox"/> Autotests

SECTION 4. Payment Options:

Please debit my ☐ Visa ☐ Mastercard ☐ Laser Card for the following amount: €

Credit Card Number: Expiry Date:

Name of Cardholder: _____ Signature: _____
IN BLOCK CAPITALS PLEASE

or I enclose: ☐ Cheque ☐ Postal Order ☐ Cash for the sum of €

SECTION 5. Medical Requirements:

- Please refer to Article 6.5 on the Yellow Licence Notes Sheet to check if you need to complete a Medical Declaration or have the Doctors Certificate completed.

Refer to CHECKLIST on page 4



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1.
MOTORSPORT IRELAND, 34 DAWSON STREET,
DUBLIN 2
Tel: 01-677 5628 Fax: 01-671 0793

RENEWAL

APPLICATION FOR 2012 COMPETITION LICENCE **L2**

This form must be completed in full.

**AFFILIATED
CLUB
STAMP**

No Stamp = Twice Price!

- Please read the notes on the Yellow Licence Notes Sheet enclosed CAREFULLY before applying for any Licence. If you have any questions, or need help filling in this form, please phone our Licence Department at the above number.
- **ONE RECENT PASSPORT SIZE PHOTOGRAPH MUST BE INCLUDED WITH THIS APPLICATION, UNLESS PREVIOUSLY SUPPLIED.**
- Licences are valid until 31st December 2012.

SECTION 1.	Your Details	PREVIOUS COMP. LICENCE NO.
SURNAME		
FIRST NAME		SEX <input type="checkbox"/> M <input type="checkbox"/> F
ADDRESS		
TOWN		
COUNTY		
TELEPHONE - HOME		
TELEPHONE - WORK		
EMAIL		
<input type="checkbox"/> Please tick if you do not wish to receive updates by email		
DATE OF BIRTH		NATIONALITY
CURRENT VALID FULL R.T.A. DRIVING LICENCE NO. COMPULSORY FOR RALLY DRIVERS		
		EXPIRY DATE

SECTION 2. Declaration and Signature/s:

- I hereby apply for registration for the year 2012 on the Competitors and Drivers Register of Motorsport Ireland and I undertake, if registered, to submit to and be bound by the International Sporting Code of the FIA and the General Competition Rules and Regulations of Motorsport Ireland, the Irish Anti-Doping Rules and any regulations supplementary thereto as may be imposed from time to time by Motorsport Ireland.
- I have read and understood the terms of issue and am a permanent resident of the Republic of Ireland and/or I am an Irish National and I do not hold a Competition Licence from any other ASN.
- I hereby agree to abide by the guidelines and regulations contained in Motorsport Ireland's General Code of Conduct and Code of Conduct for Children's Sport.

Your Signature:

Date:

If you are 17 or under, your parent or legal guardian must also sign below.
Parent's or Guardian's name:

Signature:

Relationship to you:

FOR OFFICIAL USE ONLY

€

MEDICAL : NO / YES / YEAR

Continued over the page



SPECIMEN FORM

SECTION 3. Licences you need: (Refer to the Licence Category Tables on page 4)

- ### FIRST TIME RACE & STAGE RALLY DRIVER LICENCES:

I wish to apply for the following licence(s) (please place ☒ in the appropriate box(es))

This box must be stamped by the school to show that you have successfully completed your IMC Course.

I wish to apply for the following licence(s) (please place ☒ in the appropriate box(es))

LICENCE CATEGORY	CODE	FEE	<input checked="" type="checkbox"/>	AMOUNT €
CLUBMAN				
National B	019	€ 99.00		:
Junior	020	€ 48.00		:
Priority	021	€ 25.00		:
Upgrade Fee	022	€ 10.00		:
Non Member Supplement	023	= 1x Licence Fee		:

RACE/KART EVENTS	SPEED EVENTS
<input type="checkbox"/> Kart Racing <input type="checkbox"/> Circuit Racing	<input type="checkbox"/> Rallycross <input type="checkbox"/> Rallysprint <input type="checkbox"/> Sprint <input type="checkbox"/> Hillclimb <input type="checkbox"/> Autocross
RALLY EVENTS	CLUBMAN EVENTS
<input type="checkbox"/> Special Stage Rallying <input type="checkbox"/> Historic Stage Rallying <input type="checkbox"/> Navigation Trials <input type="checkbox"/> Retrospective Trials	<input type="checkbox"/> Sporting Trials <input type="checkbox"/> 4x4 Production Vehicle Trials <input type="checkbox"/> Production Car Trials <input type="checkbox"/> Midget Car Racing <input type="checkbox"/> Autoteests

Please debit my ☐ Visa ☐ Mastercard ☐ Laser Card for the following amount: € _____

Name of Cardholder: _____ Signature: _____

or I enclose: ☐ Cheque ☐ Postal Order ☐ Cash for the sum of € _____

Refer to CHECKLIST on page 4

COMPETITION LICENCES

APPENDIX 1

SPECIMEN FORM

1

3.

- MEDICAL DECLARATION -

THIS MUST BE COMPLETED ANNUALLY by applicants intending to participate as drivers in the following Events - Races, Speed, Kart Racing, Midget Car Racing and all **Rallies with Special Stage(s)**.

TO BE COMPLETED BY THE APPLICANT IN BLOCK CAPITALS

NAME: _____

ADDRESS: _____

DATE OF BIRTH: _____

SEX: _____

Current Competition Licence No. _____

1. Have you ever been rejected or accepted at increased premiums for life insurance on medical grounds? YES ☐ NO ☐

2. Have you ever been treated for - do you now have - or have you ever had any of the following?:

a) Nervous breakdown, mental disease or disorder? YES ☐ NO ☐

b) Head injury with concussion or unconsciousness? YES ☐ NO ☐

c) Heart Disease or disorder? YES ☐ NO ☐

d) High blood pressure? YES ☐ NO ☐

e) Dizziness, fainting fits, epilepsy or blackouts? YES ☐ NO ☐

f) Have you ever had any disease, injury or operation to either eye? YES ☐ NO ☐

g) Do you take, or have you ever taken, such drugs as opium, morphia, cocaine, heroin, cannabis etc.? YES ☐ NO ☐

h) Do you have Diabetes? YES ☐ NO ☐

If YES, do you take insulin injections? YES ☐ NO ☐
oral tablets? YES ☐ NO ☐

If you are an Insulin-Dependent Diabetic please provide a letter from your Doctor/Specialist, stating that you are regularly reviewed and are well controlled on your medication.

I hereby declare that the above statements are true and accurate and I give permission to any hospital or medical practitioner to furnish information relating to my medical state to Motorsport Ireland.

APPLICANT'S SIGNATURE: _____

PARENT/GUARDIAN: _____
(Must be signed if applicant is under 18 yrs)

DATE: _____

- DOCTORS CERTIFICATE -**TO BE COMPLETED BY EXAMINING DOCTOR - PLEASE USE BLOCK CAPITALS**

DOCTOR'S NAME & QUALIFICATION: _____

ADDRESS: _____

1. Are you the regular attendant of the applicant? _____

2. Is there any abnormality of the heart or cardiovascular system? _____

a) Blood Pressure Normal: YES ☐ NO ☐

b) If the applicant is over 45 years of age and is applying for an "International" Grade Competition licence - have they passed a stress related ECG test?
(Refer to note 6.5.3. on the Yellow Licence Note Sheet).

YES ☐ NO ☐ N/A ☐

c) Date of ECG (if applicable) _____

3. Has the applicant free controlled movement of both upper and lower limbs?

R Arm _____ L Arm _____

R Leg _____ L Leg _____

4. Height: _____ Weight: _____

5. Eyes:

a) Vision - Unaided
R Eye _____ L Eye _____

b) Vision - Corrected
R Eye _____ L Eye _____

c) Field of Vision
R Eye _____ L Eye _____

d) Pupil reaction to L & A
R Eye _____ L Eye _____

R Eye _____ L Eye _____

e) Is colour vision normal, in that the applicant can distinguish the primary colours of red and green? YES ☐ NO ☐

f) Corrected eyesight
Glasses or Lenses YES ☐ NO ☐

6. Has the applicant been immunised against tetanus in the past 10 years? YES ☐ NO ☐
(If the answer is NO, we recommend immunisation)

7. Is there any evidence of a physical or mental condition past or present, which could in your opinion debar the applicant from competing in Races, Rallies or Speed Events? YES ☐ NO ☐
If the answer is YES, please provide details on a separate sheet

8. Does the applicant require special medical supervision? YES ☐ NO ☐
If the answer is YES, please provide details on a separate sheet

9. Has the applicant been prescribed or are they taking any of the substances shown in the WADA listings? YES ☐ NO ☐
(See www.motorsportireland.com - Rules & Regulations/Medical)
If the answer is YES, please provide details on a separate sheet.

10. In view of the above stated results of my examination, I recommend that:-

a) The applicant is physically and mentally fit to drive in Races, Speed Events and Rallies. YES ☐ NO ☐

b) The applicant be reviewed by the Motorsport Ireland Medical Panel. YES ☐ NO ☐

SIGNATURE: _____ **DATE:** _____

DOCTOR'S STAMP

If no stamp is available refer to note 6.5.2 on Yellow Licence Notes Sheets

Must be stamped with the official Medical Stamp of the Medical Practitioner



COMPETITION LICENCES

APPENDIX 1

1



Motorsport Ireland
34 Dawson Street
Dublin 2, Ireland

Tel: (01) 677 5628
Fax: (01) 671 0793

Application for Annual Registration for a Commercial Entrant Licence

E1

Please complete in full, using BLOCK CAPITALS.

NAME:

ADDRESS:

PHONE:

** State full name of Company or Organisation which is to be printed on the Licence:*

In accordance with Articles 108 and 109 of the FIA International Sporting Code and the General Competition Rules of Motorsport Ireland, I/we hereby apply for Registration/Re-registration for the year on the Competitors (Entrants) Register of Motorsport Ireland and I/we undertake, if registered, to submit to and be bound by the FIA International Sporting Code and General Competition Rules of Motorsport Ireland and any regulations supplementary thereto as may be imposed from time to time by Motorsport Ireland. I hereby agree to abide by the guidelines and regulations contained in Motorsport Ireland's Code of Conduct for Children's Sport.

PLEASE NOTE: A copy of the Company's Certificate of Incorporation as a limited company within the Republic of Ireland must be included with this application and must be identical to that listed above.*

I/we enclose the Registration fee of:

• € 350 for National Licence

• € 1,250 for International Licence.

APPLICANTS NAME:

STATE OFFICIAL
CO. POSITION:

LICENCE IS REQUIRED FOR: Race Rally Rallycross

APPLICANTS SIGNATURE: _____ DATE: _____

**One recent passport size
photograph of applicant
must be included with
this application.**

For Official Use Only

Licence no.....



1



Tel: (01) 677 5628
Fax: (01) 671 0793

E2

Please complete in full, using BLOCK CAPITALS.

[illegible]

I enclose the Registration fee of:

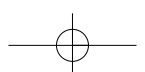
- €30 for National Licence
- €310 for International Licence.

APPLICANTS SIGNATURE: _____ DATE: _____

For Official Use Only

Licence no.....

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COMPETITION LICENCES

APPENDIX 1

1

Parents'/Guardians' Declaration Form:

1. I will respect the rules and procedures set down in Motorsport Ireland's (MI) Code of Conduct for Children in Sport.
2. I will respect my child's fellow competitors, their parents, team managers/owners and coaches.
3. I will give encouragement and applaud only positive accomplishments whether from my child, his/her fellow competitors or the officials.
4. I will respect my child's coach and support his/her efforts.
5. I will respect MI and club officials and their authority during events.
6. I will never demonstrate threatening or abusive behaviour or use foul language.
7. I will encourage my child to treat other participants, coaches, selectors, team managers/owners, MI officials, club officials and event organisers with respect.
8. I have read and accept the guidelines for travelling with children contained in MI's Code of Conduct for Children's Sport.

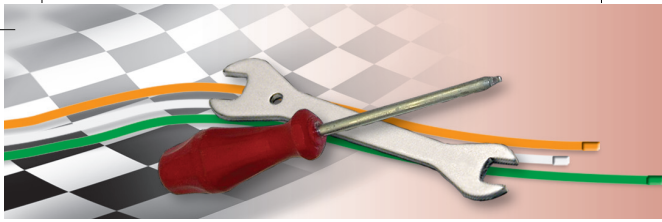
Parent/s and/or guardian/s must sign this form as appropriate.

Name: _____ Signature _____
PLEASE PRINT IN BLOCK CAPITALS

Name: _____ Signature _____
PLEASE PRINT IN BLOCK CAPITALS

Name of competitor: _____ Licence No. _____
PLEASE PRINT IN BLOCK CAPITALS

Date: _____



SCRUTINY

APPENDIX 2

SCRUTINY REQUIREMENTS

INTRODUCTION :

An entrant shall be deemed to have full knowledge of their car and to vouch for its eligibility by the act of their car being presented for scrutiny at the start.

The regulations should be interpreted on the basis that if it does not say you can do it then you cannot.

The details on the top of the MOTORSPORT IRELAND Scrutiny Sheet must be filled in and signed by the Entrant/Driver and all equipment including competition numbers and all advertising must be affixed before the car is presented to the scrutineers. If components or equipment is added or modifications are made after mechanical Scrutiny the vehicle must be represented for re-scrutiny. Cars must not be presented to scrutineers with warm engines and in addition must be presented at scrutiny in a clean and tidy condition, with presentable body and paintwork, ready to compete.

Scrutiny is passed only when a scrutiny sheet is signed by a Race scrutineer (or deputy) nominated for that class. All other events a scrutineer nominated for that event.

The onus rests with the competitor to enter a class for which their car is eligible. Classes are final at scrutiny.

Rallies and Navigation Trials: Competitors are reminded that their cars must comply with the Road Traffic Act at all times and that they must make all reasonable efforts to ensure this.

Race: Transponders to be fitted as low as possible maintaining line of site with the track this position being at the discretion of the class scrutineer. Only the current pass scrutiny sticker may be displayed.

1. HEADLAMPS, DRIVING LAMPS

1.1. Navigation Trials.

Standard headlights may only be supplemented by two additional lights. All such lights must extinguish on dipped beam. If additional lights are fitted, any supplementary manufacturer's lights must be removed. The wattage of all bulbs in forward facing lamps must comply with the RTA.

1.2. Stage Rally and Rally sprints.

The number of auxiliary lamps must comply with F.I.A. Year Book - (Groups A, N and B - Maximum 8). All auxiliary lamps must extinguish on the dipping system.

1.3. Racing.

All lights to be taped.

2. BRAKE LIGHTS, INDICATORS, REVERSE LIGHTS.

2.1. Reverse Lights must only work when the gear lever is in the reverse position. (except Retro's where no provision exists for the switch in the Gearbox) Scrutineers must insist on any incorrect lamp being removed; this includes the rear lamp.

2.2. Racing & Rallycross - Brake & Rain Lights. Saloons and Historic or Classic Cars must have 2 rearward facing brake lights with bulbs of 21 watts minimum. Brake lights clearly visible. (Rallycross, at high level in rear window) All cars must have a rearward facing red rain light of 21 watts minimum. All lights must be in working order.

3. STEERING, BRAKES, SUSPENSION.

3.1. Steering.

Must all be in very good condition. Steering freeplay will not be allowed. Scrutineers will particularly check universals, rubber couplings, the column shaft for any twisted splines and the condition of the steering wheel. Steering anti-theft device (steering lock) must be rendered inoperative by removal.

3.2. Brakes.

A good firm pedal is required, with no fluid leaks, no perished hoses, no pure copper or corroded pipes. Brake and clutch fluid reservoirs, if in the cockpit must be in a leakproof container.

3.2.1. Race / Hillclimb.

A Dual Circuit Braking System is compulsory for all cars. (with the exception of historic cars).

3.2.2. Others.

A Dual Circuit Braking System is compulsory for all cars. (with the exception of historic cars) Mechanical parking brake :- All vehicles must be fitted with a mechanical parking brake which may be applied while vehicle is travelling at any speed, and must be effective on a 1 in 6 gradient.

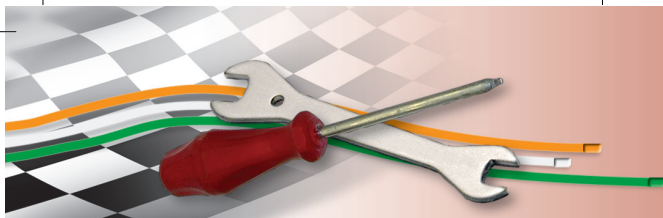
3.3. Suspension.

Ball joints and wheel bearings must be in good condition. Shock absorber mountings must be secure.

4. WIPERS, WASHERS, HORN & MIRROR.

4.1. Saloons.

Wipers must operate at an appropriate speed over an arc of the windscreen glass, which is sufficient to give the driver an adequate view.



SCRUTINY

APPENDIX 2

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4.2. Rallies.

The noise level produced by the audible warning device (horn) must be greater than or equal to 97dB during at least 3 seconds, measured 7m in front of the vehicle, activation must be available to the driver and it is strongly recommended for navigator and to be reachable by either when belted / harnessed.

4.2.3. Rear view.

This shall be provided by an inside mirror (shatterproof) commanding a rear view window with at least a 10 cm vertical opening, maintained along a width of at least 50cm. However, if the straight line connecting the upper and lower edges of the rear window opening makes an angle inferior to 20° with the horizontal, the rear view must be efficiently obtained by other means (two outside mirrors or any other system of equivalent efficiency). Furthermore, all these cars must be equipped with two outside mirrors for circuit events.

5. FUEL TANK, FUEL-OIL-WATER-LINES, ELECTRICAL CABLES.

The ageing of safety tanks entails a considerable reduction in the strength characteristics after approximately five years. No bladder shall be used more than five years after the date of manufacture, unless inspected and certified by the manufacture for a period of up to another two years. Tank, pump and **fluid** lines must be properly secured, routed and hoses clipped. Clear plastic hoses must not be used for fuel. **Fluid** lines, including fuel, oil and brake, which are fitted externally must be protected. If **fluid** lines are routed through the passenger compartment, they must be metal or metal braid (**except containers and fluid lines for windscreen washers and intercooler sprays**) and must not be situated between the rollcage and bodyshell. **Electrical cables must not be situated between the rollcage and bodyshell.** Other than bulkhead connections there must not be any joins in the passenger compartment. Tank Cap must not protrude beyond bodywork; if fitted in the boot, it must have a drain-off funnel with separate drain to outside. Fuel tank breathers should be in a 'U' shape configuration **or vent valve fitted.**

5.1. Rally, Rallysprint, Rallycross, Autocross.

The passenger compartment must not contain any fuel, oil or hydraulic pumps or filters. (This includes power steering systems). Oil reservoirs, if fitted in a two compartment vehicle, must be rearward of the rear axle and enclosed in their own boxed-in section.

5.2. Rally.

In a 3 compartment car the fuel tank may not be located in the center compartment. (The rear compartment may be extended into the center

compartment provided the requirements of App2 15 are observed).

6. FIRE EXTINGUISHERS.

The containers must be securely floor mounted. In all cases their mountings must be able to withstand a deceleration of 25g. **Two screw locked** metal fastening straps, **as per FIA standards** must be fitted. If an other system is fitted, it may be retained, but the apparatus below must also be carried. It is recommended that the regulations described below are taken as a minimum and competitors are free to use additional equipment that have retaining mechanisms which comply as outlined below.

6.1 Race.

6.1.1. Single Seaters, Uno, Fiat Punto and Abarth saloon, Stryker Formula Sheane, Formula VEE and Saloon cars. Plumbed in 1.75 minimum AFFF or FIA approved equivalent including previous FIA standard, Viro3, Powder, DuPont FE-36.

6.1.2. All others (Excluding Rallies).

1 Handheld 1.75 AFFF or FIA approved equivalent including previous FIA standard, Viro3, Powder, DuPont FE-36.

6.1.3. Where a handheld extinguisher is fitted it must be accessible to the driver when normally seated.

6.1.4. Permitted extinguishants.

As per 6.2 below.

6.2. All Rallies.

6.2.1. All cars must be equipped with an extinguishing system from FIA technical list No16 (**must be suitable for type of vehicle**): "Extinguisher systems homologated by the FIA" (Available from www.fia.com). All extinguishers must be adequately protected and may not be fitted forward of either crew members feet (**centrally mounted is allowed**). Exceptions as approved by TAG are Metro 6R4 and Darrian. Any other exceptions will be notified in the MI Bulletin. In all cases their mountings must be able to withstand a deceleration of 25 g. Steel bolts and washers must be used for mounting (no aluminium rivets) All extinguishing equipment must withstand fire. Plastic pipes are prohibited and metal pipes are obligatory.

6.2.2. Both occupants must be able to trigger (**and for electrically operated system, arm**) the plumbed extinguisher when seated normally with their safety belts fastened and the steering wheel in place. All systems must be armed before departure from Stage Arrival Control. Furthermore a means of triggering from the outside must be combined with the circuit breaker switch, or situated close to it. It must be marked with letter "E" in red inside a white circle of at least 10 cm diameter with a red edge.

APPENDIX 2

6.2.3. The system must work in all positions.

6.2.4. Extinguisher nozzles must be suitable for the extinguishant and be installed in accordance with the manufacturers instructions. Nozzles should not point directly at the occupants.

6.3. Manual extinguishers.

6.3.1. All cars must be fitted with one or two fire extinguishers. (minimum sizes are specified below).

6.3.2. Permitted extinguishants: AFFF, FX G-TEC, Viro3, Powder, or any other extinguishant homologated by the FIA.

6.3.3. Minimum quantity of extinguishant.

AFFF: 2.4 litres
FX G-TEC: 2.0 kg
Viro3: 2.0 kg
Zero 360: 2.0 kg
Powder: 2.0kg

6.3.4. All extinguishers must be pressurized according to the contents. AFFF: FX G-TEC, Viro3: and Zero 360: in accordance with the manufacturer's instructions Powder: 8 bar minimum, 13.5 bar maximum. Furthermore, each extinguisher when filled with AFFF must be equipped with a means of checking the pressure of the contents.

6.3.5. The following information must be visible on each extinguisher: - capacity - type of extinguishant - weight or volume of the extinguishant - date the extinguisher must be checked, which must be no more than two years after either the date of filling or the date of the last check.

6.3.6. All extinguishers must be adequately protected. Their mountings must be able to withstand a deceleration of 25 g. Furthermore, only quick-release metal fastenings, with TWO metal straps, will be accepted.

6.3.7. The extinguishers must be easily accessible for the driver and the co-driver.

6.4.1. Historic cars plumbed in with one handheld complying with MSA Yearbook H283 to H313.

6.4.2. Rallycross and Autocross.

1 Handheld 1.75 minimum AFFF or FIA equivalent including previous FIA standard, Viro3, Powder, DuPont FE-36.

6.5. Hillclimb/Sprint.

All Cars must be equipped with either FIA Approved handheld or plumbed in system.

6.5.1. All single seaters must be equipped with FIA approved plumbed in system. The system must be armed when in the start area and during the competition.

6.5.2. Where a handheld extinguisher is fitted it must be armed when in the start area and during the competition and accessible to the driver when normally seated.

7. SAFETY BELTS / HARNESS.

For safety reasons, please note that the homologation of the following harness, whatever their validity deadlines, of this product is withdrawn with immediate effect. TAIWAN Racing Products 3" x 2" Harness D-203 T/98.

7.1. Navigation Trials.

Minimum Lap and Diagonal to comply with the Road Traffic Act. If competition type seats are used, then the belts must pass through the belt holes in the seats (to ensure proper contact with the occupants).

7.2. Racing, Rallies & Rallycross.

For National events current FIA standard including 8853/98 and 8854/98. (Belts will remain valid for 5 years after the date shown on the relevant label).

For International events FIA rules apply. (National rules apply on national section of international events).

7.3. Rallsprint, Autocross.

Current FIA standard including 8853/98 and 8854/98. No date restrictions apply.

7.4. Hillclimbs & Sprints.

Current FIA standard including 8853/98 and 8854/98. (Belts will remain valid for 5 years after the date shown on the relevant label).

For International events FIA rules apply.

In addition for Classes 1, 2, 4, 5 & 8 (as defined in the 2012 Yearbook). No date restrictions apply on national events.

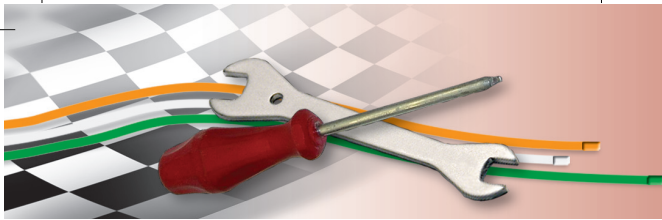
7.5. Deleted.

7.6. Fixing.

Shoulder straps to be fixed within maximum 20 degrees below horizontal plane. Belts should be replaced after a heavy accident. Mounting points to the body should be very secure. Frayed, heavily soiled seat belts will not be accepted. Seat belts must be worn by competitors correctly adjusted at all times during events.

7.7. Recommendations.

It is reminded to all drivers and scrutineers that in case of a severe crash, a car's safety belts will be stretched, and therefore lose their shock absorbing capacity for further impacts. It is therefore imperative to change any harness which has undergone a high deceleration. It is the duty of the scrutineers to refuse any harness which is in poor



SCRUTINY

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condition, even if the validity limit has not yet expired, and to remove the homologation label.

7.8. Seat belts cutters (2x) must be carried in all rally cars. These must be accessible to both occupants whilst seated and belted.

8. TYRES AND WHEELS.

Wheels must not extend beyond the periphery of the bodywork on Saloons all events.

8.1. Rallies.

Cars must be presented at Scrutiny only on tyres which comply with the Road Traffic Regulations.

8.1.1.

If (Racing) slick tyres are used they must comply with the following.

8.1.2. A number of equally spaced grooves to be positioned circumferentially. These will vary according to the width of the tyre, with a minimum of three grooves irrespective of tyre width.

8.1.3. The maximum spacing between grooves or between the outer groove and the tyre shoulder will not exceed 50mm and will be such that the distance between the outermost grooves is not less than three quarters of the thread width.

8.1.4. The measurement of other than straight circumferential grooves of whatever configuration, will be through the centre line of the pattern.

8.1.5. The minimum groove width will be 2mm at all times.

8.1.6. The minimum depth of groove will be 3mm on a NEW tyre, and not less than 1.6 at any other time.

8.1.7. The unauthorised grooving of slick tyres is prohibited.

8.1.8. Racing tyres are prohibited for Rallies e.g. Tyres marked with "for Racing Purposes only" or "Not for public highway use", Buffing of tyre side walls will render unfit for use. Tyres should be sound and in roadworthy condition. The wheels are free, respecting the homologated diameter (article 801.a), and the homologated width (article 801.b) for Group N which is considered as a maximum. and as per FIA Appendix J for Group A Remould Tyres which comply with Irish Standard 412- 1988 and/or British Standard AU 144 or their European Equivalent and carry an "H" Rating or higher may be used on Tarmac Stage Rallies.

8.1.9. The inside of the tyre (the space between the rim and the internal part of the tyre) must be filled only with air (inner tube allowable, maximum wall thickness 3mm). In other words, the use of any device allowing the tyres to

maintain their performance at an internal pressure equal to or less than the atmospheric pressure, is prohibited.

8.3. Racing. As per individual class regulations. A 1.6mm tread depth is the minimum requirement in the case of treaded tyres.

8.3.1. Rallycross.

The complete wheel (flange, rim and tyre) must always fit inside a U shaped gauge of which the extremities are:

250mm for 2 wheel drive
200mm for 4 wheel drive

The measurement to be made on the unloaded part of the tyre, but with the car in race condition and with the driver on board. The use of tyres fitted with studs, spikes or chains is not permitted.

International events.

Slick tyres are prohibited. Grooved/moulded tyres are authorised on the basis of designs homologated by the FIA. Ref. Appendix J 279.3

9. NOISE CONTROL.

Induction and exhaust noise levels must not be offensive to the public. The exhaust system must remain in place and in operation throughout the event. A silencer must be incorporated in the system. Organising clubs may exclude any car with excessive noise.

Induction Noise: For Navigation Trials an air filter must be fitted.

Exhaust Noise: Navigation Events, 100 DBA maximum at 3500rpm. (2500 for Diesel engines). Rallycross and Rallysprint, 105 DBA maximum. Rallies, (National and International) All cars used in Rallies and unless the limits imposed by the local authorities are lower, the noise level on the open road must not exceed 103 dB(A) for an engine rotation speed of 3500 rpm and 2500 rpm for diesel engines. Noise testing to be carried out in accordance with current FIA approved method of testing.

10. SECURING OF EQUIPMENT INCLUDING BATTERY.

10.1. Rally, Rallycross, Autocross, Rallysprint.

Battery, tools & equipment must be firmly secured. Scrutineers strongly advise against locating the battery in the cockpit. In the event of this being necessary, the battery must be a non-spill type and placed in a leak-proof box. In this case, the protection box must include an air intake with its exit outside the cockpit (see FIA appendix J drawings 255-10 and 255- 11). If the battery situated in the cockpit is a dry battery, it must be protected electrically by a cover which

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covers it completely. **Where a passenger is carried it may not be mounted directly forward of their feet (i.e. not behind footrest).** All removable equipment (tools, spares, etc., with the exception of belt cutters) must be mounted rearward of the seats, as per FIA regulations. Exceptions as approved by TAG are Metro 6R4 and Darrian. Any other exceptions will be notified in the MI Bulletin.

10.2. Race, Hillclimb.

Any battery situated in the cockpit must be a dry battery and must be protected electrically by a cover which covers it completely.

11. THROTTLE CLOSING.

Each carburettor must have its own return spring and an additional external spring must be fitted on the throttle linkage (butterfly shaft end).

12. WARNING TRIANGLE - ALL RALLIES.

A free standing triangle must be carried in the cockpit of each competing car.

13. L.P. GAS.

This is allowed but must conform to the Motorsport Ireland requirements; (M.S.B. No. 48, May 1983). This means double straps (i.e. 4) all in leak-proof case with Inspection plate.

14. GENERAL CONDITION.

Cars to be in a clean and tidy condition. All workmanship should be of the highest standard. Load bearing structures to be generally free from rust. All material must be in place before scrutiny. Details are included in the GCRs, Articles 116 – 120 inclusive. Scrutineers will inspect cars during any event and may recommend excluding a car that is in a unsafe condition. The Scrutineers' opinion will determine what is a clean and tidy condition.

14.1. Windscreen and windows shall bear no advertising, with the exception of a maximum 10cm high strip on the upper part of the windscreen, and, provided that this does not interfere with the visibility of the driver, an 8cm high strip on the rear window.

14.2. When an on-board television camera or other recording device is fitted, written notice that the equipment is being carried must be given to the COC and the fitting and the device must be examined by a Scrutineer during pre-event scrutiny.

15. FIRE WALLS.

15.1. Bulkhead.

A fireproof bulkhead is mandatory between fuel tanks and occupants, and between engine and occupants. If the tank is under the floor, the floor is the bulkhead. **Any** firewall should be of a metallic substance and sealed with **fireproof sealant**.

15.2. Exhausts.

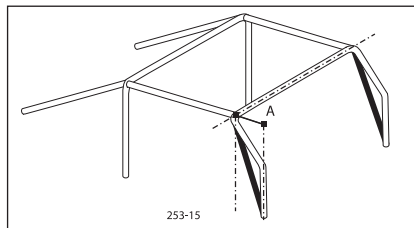
Exhaust Systems on Saloon and Sports Cars must exit beyond the body work.

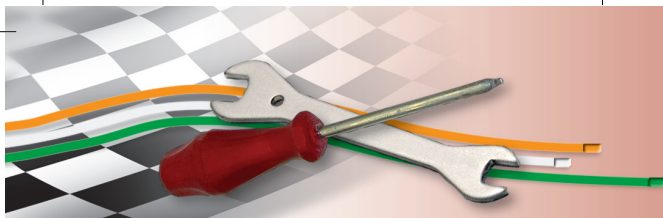
16. COMPLETE ROLL CAGE (including Intrusion Bars).

A structural framework made up of a main rollbar, a front rollbar, other optional rollbar, longitudinal braces, connections and fixation points. The cage should be constructed of cold drawn seamless carbon steel and will have minimum dimensions of 38.0mm O.D. x 2.5mm wall thickness or 40.0mm O.D. x 2mm wall thickness. All sections of the cage will have a 1/8" diameter hole drilled in them, this will be used by scrutineers to check wall thickness. All welding should be of the highest quality possible, with full penetration (preferably using gas shielded arc).

All joints must be fully welded, unless homologated otherwise. Fixation plates must be of at least 2mm thickness. The fixation of the main roll bar pillars must be done with at least 2 bolts of 8mm high tensile steel. Pillar fixation mountings must be reinforced under the body, by plates of 3mm thickness and at least 1.5 times the area of the mounting plates. The fixing of intrusion bars on both sides of the vehicle at the doors is compulsory. The tube making up this reinforcement must be welded or fixed with roll cage joint couplings to the safety roll cage and its angle with the horizontal must not exceed 15° (angled downwards towards the front of the car). No point of the bars should be higher than one third of the total height of the door measured from the base of the door. Care should be taken so the bars do not obstruct access to the front seats and do not encroach on the space provided for the driver and co-driver. Alloy cages are not permitted. Saloon Cars and Specials built or re-shelled for competition use after 1st January 1994 will require a cage as above BUT with a main hoop made of 45mm O.D. x 2.5mm wall thickness or 50mm O.D. x 2.0mm wall thickness C.D.S. steel.

16.1. Windscreen pillar re-inforcement. Mandatory for newly logged cars (from 1.7.11) and highly recommended for existing cars in classes 9-15, if they fall within the requirements of FIA Appendix J, Article 253, Article 8.3.2.1.4. (particularly the following cars – Toyota Twin-cam (RWD), Vauxhall/Opel Corsa/Nova, Honda Civic).





SCRUTINY

APPENDIX 2

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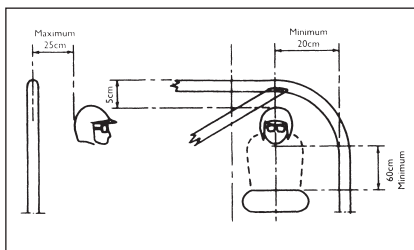
FIA International Sporting Code**Appendix J Article 253****8.3.2.1.4. Windscreen Pillar Reinforcement**

It must be fitted on each side of the front rollbar if dimension "A" is greater than 200 mm (drawing 253-15). It may be bent on condition that it is straight in side view and that the angle of the bend does not exceed 20°. Its upper end must be less than 100 mm from the junction between the front (lateral) rollbar and the longitudinal (transversal) member (see drawing 253-52 for the measurement). Its lower end must be less than 100 mm from the (front) mounting foot of front (lateral) rollbar.

16.2. Protective padding.

Where the occupants' bodies could come into contact with the safety cage, non-flammable padding must be securely fitted to the cage for protection.

16.3. Where the occupants' crash helmets could come into contact with the safety cage, the padding, securely fitted, must comply with FIA standard 8857-2001, type A (see technical list No. 23 on www.fia.com "Roll Cage Padding Homologated by the FIA"). A minimum distance of 15 cm between crash helmet and safety cage padding is recommended.



16.4. The upper part of the driver and co-drivers helmet must be at least 5 cm below the top of the rollbar hoop. Ref. Drawing Appendix 6.

17. HELMETS.

During Special Stages Rallies and all speed events competitors must wear crash helmets carrying a label issued by the relevant standards institute complying to one of the following standards:

FIA STANDARD 8860-2004
BS 6658-85 TYPE A / FR
BS 6658-85 TYPE A (KART Racing Only)
SNELL SA 2000 (USA)
SNELL SA 2005 (USA)
SFI 31.1A
SFI 31.2A
SNELL M 2000 AUTOCROSS (ONLY)
SNELL M95 AUTOCROSS (ONLY)
SNELL K2005 (KARTING ONLY)

FIA CMS 2007

FIA CMR 2007

Refer to Appendix 5 of this Yearbook for specimen labels. FIA Appendix L chapter 3 Art I (available from www.fia.com).

17.1. MI (or MSA equivalent, as long as helmet meets required MI standards listed above) Safety Helmet Approval stickers must be in position on all helmets. These tamperproof stickers will be available from selected scrutineers and will be placed on helmet as close as possible to rear of right ear position.

17.2. Painting a helmet with water base paint is permitted. Covering a helmet with material such as vinyl is not permitted.

17.3. The wearing of a head restraint approved by the FIA, e.g. HANS®, is mandatory for all drivers and co-drivers in events as follows: In all Motorsport Ireland (MI) National and International Stage Rally events (*includes "00" cars*).

17.4. Drivers and co-drivers of the following models of Historic Rally car are exempt from the requirement to wear a head restraint approved by the FIA, e.g. HANS®, in Stage Rally events run under MI permit: Alpine A110, Alpine A310, Austin Healey Sprite, Clan Clover, Clan Crusader, Davrian, Fiat X1/9, Lancia Fulvia Zagato, Lancia Monte Carlo, Mazda RX7, MGB, MG Midget, Opel GT, Porsche 914, Triumph TR4, Triumph TR7.

18. BONNET AND BOOT FASTENINGS.

At least two additional fasteners for the front and rear bonnet and boot lid. The original fasteners having been rendered inoperative. Compulsory for Gp. A and "Modified" Rallycars. Race compulsory by removal. Strongly recommended for Gp. N Rallycars and all Saloon Racing Cars.

18.1. Rallies.

Bonnet and boot lid may be fibreglass / composite but must have internals / bracing similar to original bonnet / boot lid and must be fastened by way of four bonnet pins (two front and two rear), or by original type hinges and two bonnet pins. Application: all modified rally cars.

19. WINDSCREEN/WINDOWS.

All cars must have a laminated (or FIA homologated) windscreen fitted at all times throughout the event which is clearly marked. Scrutineers will reject cars having laminated windscreens which are damaged (including delaminating) to such an extent that visibility is seriously impaired or that there is a likelihood of their breaking further during the event.

APPENDIX 2

19.1. Sunstrip.

A sun strip for the windscreen is authorized, on condition that it allows the occupants to see the road signs (traffic signs, traffic lights etc). This sun strip must not cover more than one third of the depth of the visual area of the windscreen.

19.2. Anti-shatter film.

If the side windows and the glass sunroof are not made from laminated glass, the use of transparent and colourless anti-shatter films on the inside of the side windows and sunroof is mandatory. The thickness of these films must not be greater than 100 microns, but must allow the interior of the vehicle and its occupants to be visible, in normal daylight, from a distance of 5 meters. The use of silvered or tinted films can be authorised for Groups N and A ONLY (not for modified cars) in rallies only, on the side and rear windows and on the glass sunroof, and only on the following conditions:

1. Openings in these films must allow a person outside the car to see the driver as well as the contents of the car.
2. This authorisation must be mentioned in the supplementary regulations of the event.

19.3. Polycarbonate windows.

Polycarbonate windows fitted to vehicles (not allowed in Groups A or N unless homologated) must be not be opaque but must allow the interior of the vehicle and its occupants to be visible, in normal daylight, from a distance of 5 meters.

20. HEADREST & SEATS.**20.1. Race.**

As per Class regulations.

20.2. All Rallies and Rallycross.

All seats must comply with current FIA standard including 8855/99 standard (FIA technical list 12 available from www.fia.com). For safety reasons, please note that the homologation of the following competition seats, whatever their validity deadlines, is withdrawn with immediate effect:

CS.728.96 Recaro (Germany) RACER GT1
CS.912.98 Kingdragon (Italy) Master VTR
CS.972.99 Ektor (France) Runner 2000
CS.980.99 Cobra (UK) Monaco
CS.992.00 Corbeau (GB) Forza
CS.993.00 Corbeau (GB) Pro Series
CS.994.00 Corbeau (GB) Revolution
CS.053.02 Corbeau (GB) Sprint
AS.001.09 OMP Racing (Italy) HTE-One
CS.133.05 Rossi Sports (ARG) Professional II
CS.142.05 Rossi Sports (ARG) Professional III

As these competition seats can no longer be considered to comply with the FIA standards, their use is prohibited in all cases in which compliance with the above-mentioned standards is mandatory.

For MI National events seats remain valid for 5 years after the expiry date shown on the relevant label.

For International events FIA rules apply (National rules apply on national section of international events).

20.3. All others.

Headrest must be present for each occupant. Ref. Appendix J 253 16. Compulsory for all cars, as an integral part of the seat. No 'slip on' or adjustable varieties. High back seats are compulsory for all classes.

20.3.1. Hillclimb / Sprint.

For cars in classes 1, 2, 8 (as per 2012 Yearbook) currently homologated seats will be accepted with no date limit.

Class 4 as per HRCFA & FIA Appendix K (when using currently homologated seats no date limit applies).

All other classes as 20.1 or 20.2 as applicable.

20.3.2. Rallysprint & Autocross/

Currently homologated seats will be accepted with no date limit.

20.3.3. Rally Cars.

Seats with side impact protection are highly recommended for all rally cars.

N.B. All seats must be in good condition; worn/damaged seats will not be accepted.

21. TOWING EYES.

Front and rear towing eyes which are adequate must be fitted and clearly marked and accessible to rescue services with the exception of single seaters. (Exhaust clamp not acceptable)

21.1. All Rallies.

Towing eyes must be clearly visible and painted in yellow, red or orange. (Carrying of tow rope recommended.)

22. GENERAL CIRCUIT BREAKER.

It must be operable from both inside and outside the car, clearly marked with a red spark on a white edged blue triangle. The circuit breaker must stop the engine and isolate all electrical circuits. It must operate independently to a plumbed in extinguisher system. As for the outside, the triggering system of the circuit breaker will compulsorily be situated at the lower part of the windscreen mounting for closed cars (N.S. recommended). The inside switch must be operable by all occupants when seated normally with their seat belts fastened and the steering wheel in place. This is highly recommended from 1.1.11, mandatory from 1.7.11.

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23. CLOTHING AND OVERALLS.

The wearing of rain suits made from flammable material over flame retardant overalls is not permitted.

23.1. Rallies, Rallycross and Racing.

National Events: FIA homologated Flame retardant overalls, underwear, socks, boots, balaclava are compulsory (for Rallies both crew members) with gloves compulsory for drivers. Flame retardant overalls must cover the arms to the wrists, the legs to the ankles and the torso to the neck during the competitive sections of the event. Failure to wear gloves, a crash helmet, flame retardant overalls, underwear, socks, boots, balaclava and seat belts, properly fastened on the arrival to start a stage or during a stage will carry the penalty of exclusion. It is mandatory that FIA approved clothing be used at all times.

23.1.1. Autocross, Hillclimb/Sprint and Ralliesprint.

FIA homologated 3 layer overalls 1986 (or subsequent standard) and gloves to FIA 1986/ISO standard (or subsequent standard). All other clothing i.e. underwear (top & bottom), socks, boots & balaclava FIA 8856-2000.

For Hillclimb/Sprint; ONLY classes 1, 2, 4, 5, 8 (as defined in 2012 Yearbook) may use above. ALL other classes as per (23.1).

The overalls and gloves must be in good condition; worn/damaged items will NOT be accepted.

23.2. International Events.

FIA Approved clothing, overalls, underwear, balaclava, socks, shoes, and gloves (optional for navigator) homologated to Current FIA standard including 8856-2000 standard must be worn.

N.B. FIA Approved flame retardant underwear must be worn. The Name and Blood Group of all competitors should be embroidered (all embroidery/badges to FIA regulations.) or otherwise clearly marked on the overalls. **The name and blood group on the overalls must be that of the competitor who is wearing them.**

24. WINDOW WEBBING.

It is compulsory for all Saloon cars to be fitted with a protective webbing to the driver's **and passengers if applicable** side window **opening** if this window is left open during competition. This will also apply to historic cars fitted with hardtops.

25. SUNROOFS.

Glass sunroofs (if not on homologation papers) must be replaced by a panel of steel and be at least the same thickness as the remainder of the

roof and be firmly secured in the closed position. Application: all groups, unless the series production fitting is maintained.

26. ADDITIONAL SAFETY INFORMATION RALLIES.

26.1. It is strongly recommended that a first aid kit is carried in both Rally car and Service vehicles.

26.2. Spare wheels, jacks, wheel braces, tools and spare parts must be securely fastened in position. Scrutineers or other officials will remove loose items from cars at stage starts.

27. LOG BOOKS.

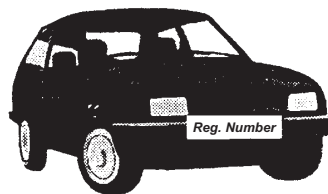
Mandatory for the following disciplines of Motor Sport.

- a) Stage Rallies, Rally Sprints.
- b) Historic Stage Rallies
- c) Rallycross
- d) Autocross
- e) Hillclimbs and Sprints
- f) Racing

27.1. Cars presented to scrutineers without a log book will NOT be permitted to start. Logbooks will not be issued during scrutiny. Log books must carry the current owners name and address, but if the car is hired, documentary evidence of ownership will be required.

27.2. To obtain a log book a Vehicle registration document or Tax Book (where appropriate) containing the vehicle identity number is required.

27.3. A recent colour print photograph (not polaroid) minimum 4" x 3" maximum 6" x 4" is required to be affixed to the log book. Single seater racing cars showing side elevation and saloon cars, a right hand side front three quarter view with the number plate legible (where appropriate).



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27.4. Photographs will not be accepted if they are:

Not in colour
Polaroid
Out of focus.

Incorrect exposure making identity difficult
Showing wrong side of car not showing complete car
Number plate missing (in the case of Rally cars).

27.5. Log Books must be returned to M.I. if asked for by M.I., or one of the Scrutineers listed in Art32 of Appendix 2 if any of the following alterations are required.

Change of ownership.
Change of address
Change of colour (with NEW photo)
Change of engine type or capacity
Change of Body Shell
Vehicle Disposal or Dismantled
Log books refer to complete cars and may not be transferred with a body shell

27.6. Cost is €40 per new/replacement Log Book, for changes of details, (as listed in Art. 27.5), €20.

27.7. Motorsport Ireland Log Books will be accepted by MSA.

27.8.1. MSA Log Books accompanying vehicles held by MSA competitors will be accepted.

27.8.2. From January 1994 Motorsport Ireland licence holders must have motorsport Ireland or FIA logbooks. Only MSA Log Books held by Motorsport Ireland competitors issued prior to January 1994 will be accepted.

27.9. To obtain a log book please contact one of the Scrutineers listed in Art 32 of Appendix 2.

27.10. Lost Logbooks.

Duplicates may be issued with an entry stating duplicate and the original logbook number it is replacing.

27.11. If a vehicle does not pass scrutiny on safety grounds its log book may be retained by Motorsport Ireland, or its officials, until a satisfactory inspection has taken place. If a vehicle is seriously damaged as the result of an accident the log book may be requested to be returned to Motorsport Ireland, or its officials, for retention and return following an inspection after repairs have been carried out.

28. ELIGIBILITY.

28.1. Homologation Papers - Group A and N Cars.
Homologation papers must be produced at scrutiny and available on demand throughout the event. Homologated cars must use homologated safety equipment. Note: Failure to comply will result in a car being automatically declared ineligible.

28.2. Fuel.

Only fuels, in the following list, that are commercially available to be purchased by the public, may be used.

Petrol (gasoline)
Diesel and Bio-Diesel (Derv and Derv/bio-fuel mixtures)
Bio-Ethanol (must conform to EN85 standard)
Vegetable derived oils
LPG (appropriate safety precautions to be taken)

Note: If either Bio-Ethanol or LPG are being used, the competitor must inform the organisers of this when entering the event and the vehicle must display an appropriate warning label to warn of this fact.

Particular classes or championships may limit the type or standard of fuel that can be used. This will be published in their class/championship/supplementary regulations.

No additives, which increase the octane rating of the fuel, may be used. The phrase "power boosting additive" means and has always meant any substance added to a fuel with a view to increasing engine power output. No "power boosting additives" may be used. Only air may be mixed with the fuel as an oxidant. The question of whether a given substance is or is not a component of a fuel is decided by whether such substance can or cannot be found in one of many fuels on bona-fide sale to the general public. The same applies to the proportions in which a given substance may be so found.

28.3. Comparison Rule.

Direct comparison between questionable items and production line components will be used wherever appropriate, in all classes.

28.4. Confiscation of questionable items.

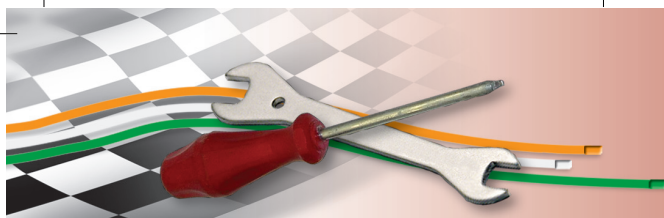
The Scrutineers are empowered to confiscate any questionable component (including the logbook) they may consider necessary for further examination. Motorsport Ireland and its agents will not be held responsible for the safety of any confiscated items.

28.5. Onus for Paying Costs of Eligibility Checks.

The onus is on competitors to prove that their cars comply with the relevant regulations and the cost of any dismantling required must be borne by:

28.5.1. The competitor: when his/her car is either protested by another competitor or examined on the instructions of the organisers or Motorsport Ireland and found not to comply with the regulations.

28.5.2. The person protesting the car: if the car is found to comply with the regulations.



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28.5.3. Motorsport Ireland: If Motorsport Ireland or the organisers or the Stewards of a meeting request proof of eligibility involving stripping/rebuilding, the entrant/competitor will be responsible for the full costs, whether found to be legal or not. If found to be "legal" however, the Scrutineers costs will not be added. The Clerk of the Course has the authority (Rule 163), taking all relevant factors into account to order that a deposit as determined by them to be lodged with Motorsport Ireland by the person(s) protesting. Motorsport Ireland's function being only that of a depositor pending the outcome of the technical investigation. If on investigation the protest is found to be valid and is upheld, the deposit will be immediately returned to the person protesting. If not upheld the deposit will not be returned until it is confirmed that the person(s) protesting has paid the expenses arising from the stripping and rebuilding and cost of the Scrutineer which may or may not exceed the deposit paid (GCR 163). N.B. Formula Vee has specified limits.

28.6 Capacity Checks.

Two camshaft cover or rocker cover and two sump (to enable sealing of the crankcase assembly) securing bolts must be cross drilled to enable wire sealing. Failure to comply will result in a car being automatically declared ineligible.

28.6.1. Where specifications apply two transmission assembly bolts to engine (to enable sealing of the flywheel assembly) and two transmission assembly bolts (to enable sealing of the transmission assembly) must be cross drilled to enable wire sealing. Failure to comply will result in a car being automatically declared ineligible.

28.7. The organising Club must provide the Scrutineers with suitable facilities to carry out post event eligibility checks.

28.8. Historic rally cars.

See Appendix 29 Articles 17 to 20.

28.9. Sealing.

28.9.1. If a vehicle or any of its components are sealed for eligibility purposes or as a result of a protest, then this fact must be entered by the sealing scrutineer in the log book relevant to the vehicle. The licensed Entrant or Driver must remain with the vehicle for the duration of the sealing process and acknowledge the examination procedure initiation by signing the issued Sealing Report. Failure to do so will result in the imposition of a fine as set out in The Schedule of Fees and Rule No.144. All subsequent vehicle eligibility examinations must be completed no later than the expiration of the forty fifth (45th) day after the sealing was carried out. Even if a vehicle, or any of its components, are sealed as a condition of competing in a championship any eligibility inspections requested must still be completed within the same time scale.

28.9.2. Failure to comply with this regulation will automatically result in the relevant vehicle being declared ineligible for the competition for which it was entered and the results for such competitions will be amended accordingly. There is no right of protest or appeal against such a declaration which will be made by the Motorsport Department. If a vehicle is so deemed to be illegal then the seals must be broken by the 'sealing' scrutineer or his nominee and the same vehicle/components re-sealed immediately and a further entry made in the log book."

28.10. Forced induction intake restrictor and mountings.

These must be as per Appendix J of the current FIA Yearbook (available from www.fia.com).

Cars registered before 1st January 1996, on National events, attachment by means of two needle screws will be acceptable. The heads of both screws must be pierced so that they can be sealed. Appendix J requirements must be adhered to in all other aspects. This will be strictly enforced on all turbo charged cars.

29. SOS/OK BOARD.

An "SOS/OK" Board must be carried secured to the underside of the roof within reach of both crew members in all cars competing in rally events. This board must be made of durable material (not cardboard) 29cm x 20cm with lettering 20cm in height.

30. SPECIALS.

Autocross Specials see Appendix 87.

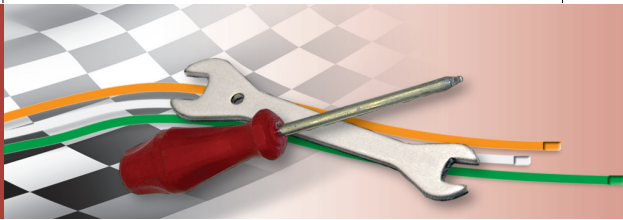
31. ACQUAINTANCE WITH AND SUBMISSION TO RULES.

It shall be no defense to claim no performance advantage was obtained should a vehicle be found not to comply with the regulations. (GCR 49)

31.1. All vehicles eligibility examinations must be completed no later than the expiration of the forty fifth day (45) after which the vehicle was sealed by an authorised official (the vehicle includes any individual part/s or component/s). Failure to comply with this regulation will automatically result in the relevant vehicle being declared ineligible for the competition for which it was entered and the results of such a competition will be amended accordingly. (GCR 163)

32. QUERIES.

Motorsport Ireland
34, Dawson Street, Dublin 2.
T:01 6775628
email: info@motorsportireland.com
www.motorsportireland.com



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LOGBOOKS

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Mobile: 087 7957148, Email: jwolver@iol.ie

SAFETY REQUIREMENTS

APPENDIX 3

Minimum Scrutineering Requirements

COMPETITOR SAFETY						VEHICLE SAFETY																			
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25
RACE																									
Single Seater	A	A	A	A	A	B	A	B**	B**			B	A	A	A	A^	A^		A					A	A
Saloon	A	A		A	A	A	A	A	B		A	B	A	A	A	A	A	A	A			R	A	A	A
Historic	A	A		A	A	A		A or B	A			A	A		A	A	A	A	A			R	A	A	A
RALLY																									
International	B	A		A	A	A or B		A	A	B	A	A & B	A	A		A	A	A	A	A	A	R	A	A	A
National	A	A		A	A	A or B	A	A	B	A	A	A & B	A	A		A	A	A	A	A	A	R	A	A	A
Navigation	A					C		A				R		A						A		R			
Trial																									
Retrospective						C		A				R		A						A	A	R			
Historic	A	A		A	A	A or B		A	B	B	A	A & B	A			A	A	A	A	A	A	R	A	A	A
OTHER																									
Autocross	A	A	A	A	A	A	R	A	A or B	B	A	A	A	A		A	A	A	A			R	A	A	A
Autotest	A							A or B	A														A	A	A
Hillclimb /	A	A	A	A	A	A or B	A	A or B	A and /	A and /	A	A or B	A	A	A	A	A	A	A			R	A	A	A
Sprints																									
Kart	A	A	A	A	A																				
Midget Car	A or B	A	A	A	A	A	A	B	A						A	A	A		A			R		A	A
Rallycross	A or B	A	A*	A	A	A or B	A	A or B	B	B	A	A	A	A	A	A	A	A	A			R	A	A	A
Rallysprint	A	A		A	A	A or B	A	A	A	B	A	A	A	A	A	A	A	A	A	A	A	R	A	A	A
Sporting Trial										A		A										R		A	A
4x4		A	A			A				B	A	A							A			R	A	A	A

A*: Goggles (see Rallycross Regulations, Appendix 82).

B**: Except Stryker.

B***: Stryker - Rollcage.

A^: Required for Stryker only.

SAFETY REQUIREMENTS

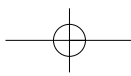
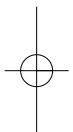
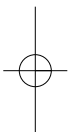
APPENDIX 3

Minimum Scrutineering Requirements

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Section				Standard	Refer to:
1	Overalls	A	Flame retardant +underwear	FIA 8856 – 2000	Article 23
		B	CIK-FIA (short circuit) / Leathers (long circuit)		
2	Helmet	A	Current Motorsport Ireland standard		Article 17
3	Visor or Goggles	A	Compliant with British Standard BS4100 – 1999	BS4100 –1999	
4	Gloves	A	Fire retardant (FIA 1986 Standard gloves end 31.12.2009)	FIA 8856 –2000	Article 23
5	Boots	A	Fire retardant	FIA 8856 –2000	Article 23
6	Seat Belts	A	Full harness – 4 point	FIA 8853 – 1998	Article 7
		B	Full harness – 6 point	FIA 8854 – 1998	
		C	Lap and diagonal		
7	Head Restraint	A	10cm x 10cm behind head within approx. 5cm of helmet		
8	High Backed Seat	A	Built in headrest		
9	Windscreens	A	Laminated		Article 19
		B	Polycarbonate		
10	Safety rollover structures	A	Rollbar		Article 16
		B	Rollcage		
11	Side Intrusion Bars	A	38 x 2.5mm minimum		Article 16
		R	Recommended (as A above)		
12	Fire Extinguishers	A	Handheld (FIA approved extinguishant)		Article 6
		B	Plumbed in FIA homologated (FIA technical list number 16)		
13	Electrical Isolation Switch	A	Internally and externally operated		Article 22
14	Dual Circuit Braking	A	Required		Article 3.2.2
15	Red Red Warning Light	A	21 watt minimum		Article 2
16	Additional bonnet and boot fastenings	A	Required		Article 18
17	Steering Lock	A	Remove or render in-operative		Article 3
18	Central Locking System	A	Render in-operative		
19	Fire Walls	A	Required		Article 15
20	Red Warning Triangle	A	Required		Article 12
21	SOS / OK Board	A	Required		Article 29
22	First Aid Kit	R	Recommended		
23	Window Webbing	A	Required if drivers window is to remain open		Article 24
24	Fuel tank, fuel oil and water lines	A	Compliance with Article 5 mandatory		Article 5
25	Throttle closing	A	Compliance with Article 11 mandatory		Article 11

This summary must be read in conjunction with the detail requirements in appendix 2.





PURCHASE AND CARE HELMETS

APPENDIX 4

ADVICE ON THE PURCHASE AND CARE OF HELMETS

(FOR LIST OF APPROVED STANDARDS SEE APPENDIX 2 ART 17)

1. Fit and Security.

To ensure satisfactory fit and security of your helmet, proceed as follows:

- a) Obtain correct size by measuring the crown of your head.
- b) Check that there is no side-to-side movement, a helmet should be as close fitting as possible consistent with comfort.
- c) Tighten straps securely - the chin strap must be under tension at all times; ensure therefore that the strap cannot slip. **Chin cups are prohibited.**
- d) With head forward and with strap fastened, attempt to pull up the back of the helmet, as shown in the diagram below, to ensure the helmet **cannot** be removed in this way.
- e) Ensure you can see clearly over each shoulder.
- f) Make sure nothing impedes your breathing in the helmet and never cover your nose or mouth other than with a flame resistant balaclava or face mask. Helmets with life-support attachments must only be worn if they are connected to a life-support system.
- g) Never wear a scarf, tie or other loose clothing which could come loose and possibly cause an accident.
- h) Ensure that the visor can be opened with one gloved hand.
- i) Satisfy yourself that the back of the helmet provides protection for your neck.
- j) Do not buy from mail order unless you can satisfactorily carry out the above checks. Return a helmet unused if it does not fit.

2. Condition and Care of Helmet

- a) The user must bear the prime responsibility for ensuring that his helmet is fit for the purpose intended, since significant damage to the helmet may have been sustained without this being apparent to the scrutineer.
- b) Anything other than minor superficial damage is likely to result in the scrutineer impounding the helmet for the event.

c) It is in everyone's interest for the competitor to buy the best helmet he can and to look after it (the best is not necessarily the most expensive). A helmet bag should always be used to store the helmet.

d) There must be no alteration to the structure of a helmet. Where a radio intercom is fitted this should only be done in accordance with the helmet manufacturer's instructions.

e) Use only a weak solution of soft soap and water to clean the interior and exterior of the helmet, do not get the interior too wet.

f) Some moulded plastic helmets although they meet approved standards can be seriously damaged by substances such as petrol, paint, adhesives, cleaning agents and stickers - such damage may not always be apparent; however, crazing or obvious dulling of the surface finish could indicate serious structural weakening of the helmet and is likely to result in the scrutineer impounding the helmet for the event.

g) The helmet should be stored, preferably in a helmet bag, in a cool dry place away from sunlight when not in use. Do not strap the helmet to the roll cage or allow other unrestrained movement which could cause the helmet to be damaged.

h) A good helmet, properly cared for, is one very important link in a long chain of safety measures. Do not allow it to become the weak link. Do not rely on others. You are responsible for your own safety. Do not, through your own fault, become a grave burden to others.

3. Goggles or Visor

Either goggles or a visor must be worn at all times during training practice and competing, unless in a closed vehicle.

Recommended visor standard (minimum)
BS4100.



INTERNATIONAL HELMET STANDARDS

APPENDIX 5

INTERNATIONAL HELMET STANDARDS

Refer to Appendix 2 Art. 17

NORMES RECONNUES POUR LES CASQUES
RECOGNISED STANDARDS FOR HELMETS

LISTE TECHNIQUE N° 25 / TECHNICAL LIST N° 25

5

- **FIA 8860-2004**
- **FIA 8860-2010**
Fédération International de l'Automobile
8, Place de la Concorde
75008 Paris
France
www.fia.com



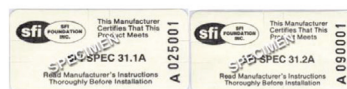
- **Snell SAH 2010**
- **Snell SA 2010**
- **Snell SA 2005**
Snell Memorial Foundation
3628 Madison Avenue, Suite 11
North Highlands, CA 95660
USA
www.smf.org



- **Snell SA 2000**

NOT VALID AFTER
31.12.2014

- **SFI 31.1**
- **SFI 31.1A**
- **SFI 32.2A**
SFI Foundation Inc
15708 Pomerado Road, Suite N208
Poway, CA 92064
USA
www.sffoundation.com



- **BS6658-85 type A/FR**
British Standards Institution
389 Chiswick High Road
London W4 4AL
UK
www.bsi-global.com

NOT VALID
AFTER
31.12.2013

Pour Autocross-division SuperBuggy, Buggy1600 et JuniorBuggy UNIQUEMENT. les normes suivantes sont aussi reconnues:
For Autocross-division SuperBuggy, Buggy1600 and JuniorBuggy ONLY, the following standards are also recognised:

- **Snell M 2010**
- **Snell M 2005**



- **Snell M 2000**

NOT VALID
AFTER
31.12.2014

Prière de se reporter aux exigences supplémentaires concernant l'utilisation des casques à l'Annexe L du Code Sportif International de la FIA.
Please check additional requirements regarding helmets use in the Appendix L of the FIA International Sporting Code.



INTERNATIONAL HELMET STANDARDS

APPENDIX 5

Refer to Appendix 2 Art. 17



Commission Internationale de Karting - FIA



ANNEXE N°2: NORMES RECONNUES POUR LES CASQUES EN KARTING
 APPENDIX No. 2: RECOGNISED STANDARDS FOR HELMETS IN KARTING

5

Snell-SA2010



Snell-K2010



Snell-FIA CMS2007

Obligatoire dès 1/1/2010 pour Pilotes de moins de 15 ans
 Mandatory from 1/1/2010 for Drivers under 15 years old



Snell-FIA CMR2007

Obligatoire dès 1/1/2010 pour Pilotes de moins de 15 ans
 Mandatory from 1/1/2010 for Drivers under 15 years old



Snell SA2005

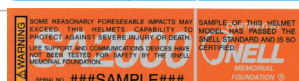


Snell K2005



Snell SA2000

! Echéance 31/12/2014
 ! Not valid after 31/12/2014



Snell K98

! Echéance 31/12/2014
 ! Not valid after 31/12/2014



Snell Memorial Foundation
 3628 Madison Avenue,
 Suite 11
 North Highlands, CA 95660,
 USA

Tel. +1 (916) 331-5073
 Fax +1 (916) 331-0359
 www.smf.org

SFI Foundation Inc
 15708 Pomerado Road,
 Suite N208
 Poway, CA 92064, USA

Tel. +1 858-451-8868
 Fax +1 858-451-0268
 www.sffoundation.com

SFI 31.1A
 SFI 31.2A



INTERNATIONAL HELMET STANDARDS

APPENDIX 5

Refer to Appendix 2 Art. 17



Commission Internationale de Karting - FIA



ANNEXE N°2: NORMES RECONNUES POUR LES CASQUES EN KARTING APPENDIX No. 2: RECOGNISED STANDARDS FOR HELMETS IN KARTING

5

FIA 8860-2004
Fédération Internationale
de l'Automobile
8, Place de la Concorde
75008 Paris
France
www.fia.com

FIA 8860-2004



FIA Standard 8860-2004
Manufacturer: **Helmet Inc**
Model: **ysomus**
Size: **56**

BS6658-85 type A, type A/FR

! Echéance 31/12/2013
! Not valid after 31/12/2013

British Standards Institution
389 Chiswick High Road
London, W4 4AL, GB

Tel. +44 (0)20 8996 9000
Fax +44 (0)20 8996 7400
www.bsi-global.com





COMPETITION NUMBERS AND ADVERTISING

APPENDIX 6

COMPETITION NUMBERS AND ADVERTISING REGULATIONS

See also GCR 184

Advertising will be free with the following limitations:

- a) No advertising may be within 7cms of the circle containing the competition number. The circle containing the number is deemed to be 40cms in diameter.
- b) The layout of competition numbers must be as follows:
 1. One number on the front of the car.
 2. One number on each side of the car.
- c) All numbers must be 20 cms high with a stroke of 4 cms. Each number must comply with the specimen layout as specified in the Yearbook.
- d) The number will be black on a white background.
- e) The best position for a number on the front of a single-seater is at the very front of the nose-cone, as this is usually angled downwards. When a number is positioned further back, just in front of the base of the screen, it is often on a horizontal surface and cannot be read as quickly. Regarding the angling of a number to the right, this should not be more than 30 degrees from the straight ahead position. Racing numbers must be of the exact style illustrated, which experience has shown to be the most easily identified. Numbers shall be in position before the vehicle is presented for scrutiny and will be subject to approval as to position, legibility and size. Modifications may be required as a result of performance of any vehicle whose number is not readily apparent to the lap recorders.

- f) Where a competitor is sponsored by a company whose name or logo, contains numerals, these numerals may not constitute the competition number, nor may that part of the advertising be on the same plane or panel of car, nor may the numbers be black or exceed a size of 15cms.
- g) Windscreens and windows shall bear no advertising, with the exception of a maximum 10cm high strip on the upper part of the windscreen, and, provided that this does not interfere with the visibility of the driver, an 8cm high strip on the rear window.
- h) No advertising may carry obscene or offensive details.
- i) Associated Committees may allocate the number sequence for each season to the competitors in each class.
- j) Multiple drives: There may be only one number visible on the competing car when it enters the start area. This does not mean a strip of tape through one of the numbers in an attempt to obliterate it.

k) Racing car numbers must conform to the attached diagram.

6

1234567890



K) Racing Car Numbers

Single Seater
/ Sports Cars

28 cm

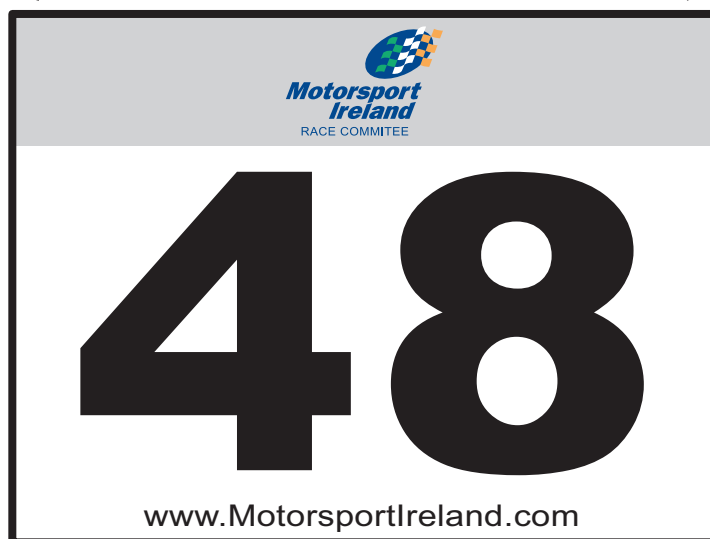
28 cm



Saloon / GT Cars

40 cm

30 cm





COMPETITOR PA INSURANCE

APPENDIX 7

COMPETITORS' PERSONAL ACCIDENT INSURANCE

A COMPULSORY PERSONAL ACCIDENT INSURANCE scheme for all competitors, including passengers, taking part in motorsport events is in operation for all events run under a Motorsport Ireland permit.

Organising clubs must collect the premium from competitors prior to the start of the event. The total premium collected must be returned to Motorsport Ireland by cheque within 14 days of the completion of the event (excluding Rallies and Navigation/Retrospective Trials).

For Rallies and Navigation/Retro Trials, the amount of premium collected will be declared by the organising club on the CPA Return Form, as for all other events. This amount will then be added to the sum due for the IRDS Insurance which is payable by lodging a blank post dated cheque with the permit application form when applying for an event permit.

The Benefits payable under the scheme are as follows:

DEATH, PERMANENT TOTAL DISABLEMENT, LOSS OF ONE OR TWO LIMBS, LOSS OF SIGHT OF ONE OR TWO EYES: up to € 125,000

TEMPORARY TOTAL DISABLEMENT: 70% of weekly wage up to max. €500 per week (applies after 4 week deferment period).

For those not in gainful employment: Up to €250 per week payable for a maximum of 26 weeks (in all not necessarily consecutive) for receipted medical and other expenses made necessary by the accidental bodily injury.

In clarification of the benefits listed, if a competitor were to receive weekly benefits for a period, and then be assessed to be Permanently Totally Disabled, the total amount paid would not exceed €125,000 as listed (any amount already paid as weekly benefit would be deducted from the capital sum becoming payable).

As competitors aged 17 years and under are charged a reduced premium for each event, they are eligible for a reduced capital payment in the event of death or serious injury and are not eligible for weekly payments in the event of Temporary Total Disablement.

MI Licence holders are reminded that any events in which they may compete outside the Republic of Ireland are not covered by the scheme, which is exclusive to MI.

Competitors are strongly advised to consider this scheme as a minimum and to consider taking out their own additional insurance to cover themselves and their dependants in the event that they are involved in a serious accident.

The rates of premium for each branch of the sport are listed below and have been classified 1, 2 or 3 according to the risk involved.

CLASS 1: All Special Stage Rallies (age 18 and over) €25 per person, (age 17 and under) €13 per person

CLASS 2: Navigation & Retrospective Trials, Road Races, Circuit Races, Kart Races, Rallycross, Autocross, Rallysprint, Midget Car Races, Sprints and Hillclimbs (age 18 and over) €15 per person, (age 17 and under) €8 per person

CLASS 3: Autotests, Sporting & 4x4 Trials, Production Vehicle Trials.....(age 18 and over) €5 per person, (age 17 and under) €3 per person

THE PREMIUM IS PER PERSON AND THIS WILL MEAN THAT IF YOU HAVE A 'BOUNCER' OR NAVIGATOR OR CO-DRIVER THEN THEY MUST ALSO PAY THE APPROPRIATE PREMIUM.

THE OCCUPANTS OF "00" CARS ARE COVERED UNDER THE "PERSONAL ACCIDENT INSURANCE" FOR OFFICIALS (see Officials' Sign-on sheet – coloured yellow) AND NOT BY COMPETITORS' PERSONAL ACCIDENT INSURANCE.

THERE IS NO EXTRA CHARGE FOR THIS.

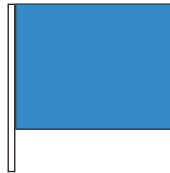
N.B. ONCE A COMPETITOR HAS CHECKED-IN, THE INSURANCE PREMIUM IS NON-REFUNDABLE



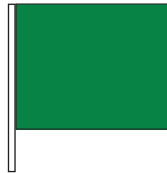
FLAG SIGNALS

APPENDIX 8

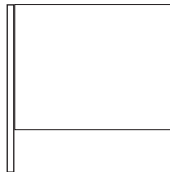
FLAG SIGNALS



Blue
Overtaking Flag



Green
Road clear

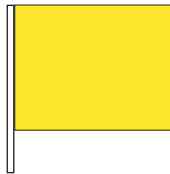


White
Service car
or
slow moving
vehicle



**Black/White
diagonal**
Warning flag

7

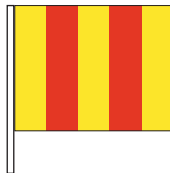


Yellow
Danger



**Black,
Orange disc**
Mechanical failure

3

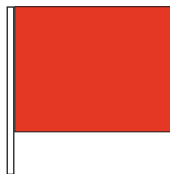


Yellow/Red
Slippery surface

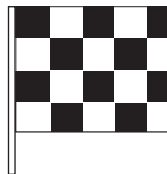


Black
Report to C of C

3



Red
Cease racing



**Black/White
chequered**
End of race

**Yellow/Black
quartered**
*Deleted
from 1.1.11*



CALENDAR OF EVENTS

APPENDIX 9

CALENDAR OF EVENTS 2012

KEY TO AUTOCROSS EVENT TYPES

(GS) = Grass Surface

(LS) = Loose Surface

KEY TO MOTORSPORT IRELAND NATIONAL CHAMPIONSHIPS

(A) = National Autocross Championship

(H) = National Hewison Autotest Championship

(KC) = National Karting Championship

(HC) = National Hillclimb/Sprint Championship

(NT) = National Navigation Trial Championship

(SR) = National Stages Rally Championship

(FR) = National Forestry Rally Championship

(RX) = National Rallycross Championship

** Italics indicate an MSA event.*

2012 DATE APPLICATIONS

DATE/S	CLUB	EVENT TYPE	STATUS
JANUARY			
1	New Year's Day		
1	ALMC	Autotest	
2	IMRC	Autotest	
7	MEC	Freeman Cup Sporting Trial	
8	Midland	Autotest (H)	
14	Co. Monaghan	Navigation Trial	
21	GSMC	Kehoe Cup Sporting Trial	
21/22	Carrick-on-Suir	Autotest (H)	
21/22	Cork	1000 Shakes Navigation Trial (NT)	
28	Donegal	Forestry Rally (FR)	
29	Co. Cavan	Autocross (LS)	
29	TDC	Multi – Venue Autotest	
FEBRUARY			
3/4/5	Galway	International Rally (TROA)	
4	MEC	Jenkins Cup Sporting Trial	
4/5	Skibbereen	100 Isles Navigation Trial (NT)	
6	Carlow	Rallycross – Mondello (RX)	
11/12	Midlands/Westmeath	Navigation Trial (NT) (Reserve Event)	
18	ALMC	Endurance Rally	
18	Leinster	Lincoln & Nolan Cup Sporting Trial	
18/19	Donegal	Navigation Trial	
18/19	Cork	4x4 Production Vehicle Trial	
19	Carlow	Rallycross – Mondello (RX)	
19	Co. Monaghan	Autotest (H)	
19	Skibbereen	Autocross (LS)	



CALENDAR OF EVENTS

APPENDIX 9

26	Birr	Stages Rally (SR)
26	MEC	Freeman Cup Autotest (H)

MARCH

3	Carrick-on-Suir	Wm.Loughman Forestry Rally (FR)
3	GSMC	Three Rock Sporting Trial
3/4	Co. Cavan	Navigation Trial (NT)
4	Limerick	Autotest
4	Co. Kildare	Rallycross (RX)
10/11	Kerry	Autotest (H)
11	Carlow	Sprint – Tynagh
11	Mayo	Stages Rally (SR)
11	Munster KC	Kart Race Meeting Round 1 (KC)

17 St. Patrick's Day

17	IMRC	Autotest
17	Leinster	Healy Cup Sporting Trial
17	Co. Monaghan	Navigation Trial (NT)
17/18	Cork	West Cork Rally
18	Birr	Autocross (GS) (A)

25	Laois	Autotest (H)
25	Midland	Stages Rally
25	Munster CC	4x4 Production Vehicle Trial
31	MEC	Devereux Cup Sporting Trial
31	500 MRCI	Race Meeting
31	TDC	Endurance Trial
31.03/1.04	Wexford	Hillclimb (HC)

APRIL

31.03/1.04	Wexford	Hillclimb (HC)
1	Co.Cavan	Autocross (LS)
1	Kerry	Circuit of Kerry Stages Rally (SR)
1	SKC	Kart Race Meeting Round 2 (KC)

6 Good Friday

6/7/8/9	UAC	<i>Circuit of Ireland International Rally (TROA)</i>
8	ALMC	Multi – Venue Autotest

8 Easter 9 Easter Monday

14/15	IMRC	Race Meeting
14/15	Galway/Clare	Hillclimb (HC)
14/15	MEC	Weekend Sporting Trial
15	Kerry	Autosolo
15	Munster CC	Moonraker Forestry Rally (FR)
15	Connacht	Autocross (LS)
15	Tipperary	4x4 Production Vehicle Trial
21/22	MRCCI	Midget Car Race
21/22	Carrick-on-Suir	Autocross (GS) (A)
22	Co. Monaghan	Stages Rally (SR)
22	IKC	Kart Race Meeting Round 3 (KC)



CALENDAR OF EVENTS

APPENDIX 9

28	500 MRCI	Race Meeting
28/29	Clare	Autotest
29	Tipperary	Rallysprint
29	Co. Cavan	Autocross (LS)

MAY

4/5/6	Killarney	International Rally of Lakes (TROA)
5/6	TDC	James Pringle Memorial Autotest
6	Connacht	Autocross (LS)
6	MRCCI	Midget Car Race
7	May Holiday	
13	Carlow	Stages Rally (SR)
13	Kerry	Autosolo
13	Mayo	Autocross (LS)
13	TBA	Kart Race Meeting Round 4 (KC)
13	Co. Kildare	Rallycross
19	Cork	Jim Walsh Cork Forestry Rally (FR)
19/20	Laois	4x4 Production Vehicle Trial
19/20	Co. Kildare	Race Meeting
20	Donegal	Autocross (LS)
20	Tipperary	Dungarvan Veteran & Vintage Charity Run
20	NEMC	Autocross (GS)
26/27	RIAC	Pioneer Run
26	500 MRCI	Race Meeting
26/27	Skibbereen	Autotest
27	Co. Cavan	Stages Rally
27	MRCCI	Midget Car Race

JUNE

2/3	Carlow	Hillclimb/Sprint (HC)
3	Limerick	Circuit of Munster Stages Rally
4	June Holiday	
8/9/10	IVVCC	Gordon Bennett Veteran & Vintage Rally
9/10	Imokilly	Autocross (GS) (A)
9/10	MEC	Race Meeting - Mondello
10	Kerry	Autosolo
10	Laois	Mini Stages Rally
10	Mayo	Autocross (LS)
10	MRCCI	Midget Car Race
13	RIAC/IVVCC	Picnic in the Park
13	TDC	Autotest
15/16/17	Donegal	International Rally (TROA)
16/17	Tipperary	Tipperary Light Car & Motorcycle Veteran & Vintage Run
23	500 MRCI	Race Meeting
23/24	Castle	Autocross (A)
24	Carrick-on-Suir	Raven's Rock Stages Rally (SR)
24	TBA	Kart Race Meeting Round 5 (KC)
24	TDC	Multi - Venue Autotest



CALENDAR OF EVENTS

APPENDIX 9

30
30.06/1.07
30.06/1.07

Forestry SC
Race Committee
500 MRCI

Forestry Rally (FR)
Race Meeting - Mondello
Sprint - Kirkistown (HC)

JULY

30.06/1.07
30.06/1.07
1
1

Race Committee
500 MRCI
Skibbereen
MRCCI

Race Meeting - Mondello
Sprint - Kirkistown (HC)
Autocross (LS)
Midget Car Race

7
7/8
8
8
8
8

Connacht
Galway
Tipperary
Leinster
Tipperary
Cork

Stages Rally
Autocross (GS) (A)
Dungarvan Veteran & Vintage Fun Run
Veteran & Vintage Classic Car Rally
Sean Conlon Memorial Forestry Rally (FR)
Autosolo / Multi - Venue Autotest

15
15
15
15
15

Skibbereen
ALMC
Munster KC
Clare
MRCCI

Economy Run
Stages Rally
Kart Race Meeting (O Plate)
Autocross
Midget Car Race

21
21/22
21/22
22

Kerry
Carrick-on-Suir
Carlow
Co. Monaghan

Rallysprint
Hillclimb
Race Meeting - Mondello
Autocross (GS)

28
28
29
29
29

500 MRCI
Imokilly
Birr
Mayo
Carlow

Race Meeting
Single Stage Rally
Multi - Venue Autotest
Autocross (LS)
Rallysprint

AUGUST

4
4/5
4/5
4/5
5
5
5

Provisional

Maiden City MC
IMRC
Killarney
Limerick
Tipperary
TBA
MRCCI

Dogleap Forestry Rally (FR)
Motorsport on Show
Autocross (GS)
Hillclimb / Sprint
Carrick-on-Suir Veteran & Vintage Run
Kart Race Meeting Round 6 (KC)
Midget Car Race

6

August Holiday

11/12
11/12
12

Mondello SC
Midland
Tipperary

Race Meeting
Autocross (GS) (A)
Stonethrowers Stages Rally (SR)

17/18
18/19
18/19
18/19
19
19
19

NIMC
TBA
MEC
Skibbereen
Birr
TDC
IKC

Ulster International Rally (TROA)
Phoenix Park Race Meeting
Sprint Weekend (HC)
Autotest
Autocross (LS)
Autotest
Kart Race Meeting (C Plate)

25
25/26

500 MRCI
Carlow

Race Meeting
Autocross (GS) (A)



CALENDAR OF EVENTS

APPENDIX 9

26	Galway	Summer Rally
26	TDC	Multi – Venue Autotest
26	MRCCI	Midget Car Race

SEPTEMBER

1	Munster CC	Autotest (H)
1/2	Donegal	Hillclimb (HC)
2	Cork	Autotest (H)
2	Castle	Hellfire Stages Rally
2	Tipperary	Autocross (LS)
8/9	Laois	4x4 Production Vehicle Trial
8/9	Wexford	Stages Rally
8/9	Leinster	Race Meeting (Leinster Trophy) Mondello
9	Kerry	Autosolo
9	TBA	Kart Race Meeting Round 7 (KC)
9	Connacht	Autocross (LS)
9	NEMC	Autocross (LS)
15	MEC	Rhodes Cup Sporting Trial
16	Westmeath	Autocross (GS)
16	Clare	Stages Rally (SR)
16	MRCCI	Midget Car Race
22	ALMC	Endurance Trial
22	Tipperary	Foyle Veteran & Vintage Memorial Run
22	Omagh	Forestry Rally (FR)
23	Mayo	Autocross (LS)
23	IMRC	Sprint
28/29/30	UAC	Circuit of Ireland Retrospective Trial
29	500 MRCI	Race Meeting
29	Leinster	GVB Cup Sporting Trial
30	Co. Cavan	Autocross (LS)
30	TBA	Kart Race Meeting Round 8 (KC)

OCTOBER

6/7	Munster CC	Cork 20 International Rally (TROA)
6	Co. Monaghan	Navigation Trial
7	Laois	Sprint - Tynagh
7	Tipperary	4x4 Production Vehicle Trial
7	Wexford	Autotest
7	Connacht	Autocross (LS)
13	Donegal	Harvest Stages Rally (SR)
13	MEC	Noonan Cup Sporting Trial
14	GSMC	Mini Stages Rally
14	Mondello SC	Race Meeting
20	Birr	Navigation Trial
21	Co. Cavan	Autocross (LS)
21	Co. Kildare	Rallycross (RX)
21	TDC	Autotest
27	Galway	Autotest (H)
27/28	Donegal	Navigation Trial
28	IMRC	Autotest
28	Skibbereen	Fastnet Stages Rally
28	Connacht	Autotest (H)
29	October Holiday	



CALENDAR OF EVENTS

APPENDIX 9

NOVEMBER

3	Galway	Endurance Trial
3	GSMC	Hand Cup Sporting Trial
3	Awards	Forestry Rally Championship
3/4	Cork	Startrek Navigation Trial
4	Carlow	Rallycross – Mondello (RX)
4	Carrick-on-Suir	Rallysprint
4	Limerick	4x4 Production Vehicle Trial
10	Co. Monaghan	Drumlin Navigation Trial
11	Kerry	Mini Stages Rally
11	NEMC	Autocross (LS)
17	Leinster	Cahill Cup Sporting Trial
17	Awards	Dunlop National Rally Championship
18	Birr	Autocross (LS)
18	Cork	4x4 Production Vehicle Trial
18	TDC	Howard Wilde Memorial Autotest (H)
24	Donegal	Closed Mini Stages Rally
24/25	Carlow	Autotest (H)
24/25	Midland	Navigation Trial
25	Co. Cavan	Autocross (LS)
25	Co. Kildare	Rallycross (RX)

DECEMBER

1/2	Killarney	Historic Car Stages Rally
2	MEC	Rallysprint – Mondello
8	MEC	Kilkenny Cup Sporting Trial
8/9	Skibbereen	Carbery Navigation Trial
25	Christmas Day	
26	St. Stephen's Day	
26	Munster CC	Autocross
26	TDC	Christmas Autotest
27	Carlow	Rallysprint – Mondello
27	Cork	Autocross (LS)
28	Leinster	Le Fanu / Brabazon Cup Sporting Trial
30	Co. Cavan	Autocross (LS)



CALENDAR OF EVENTS

APPENDIX 9

2012 FIA Formula One World Championship (20 events)

18 March	Australia
25 March	Malaysia
15 April	China
22 April	Bahrain
13 May	Spain
27 May	Monaco
10 June	Canada
24 June	Europe (Valencia)
8 July	Great Britain
22 July	Germany
29 July	Hungary
2 September	Belgium
9 September	Italy
23 September	Singapore
7 October	Japan
14 October	Korea
28 October	India
4 November	Abu Dhabi
18 November	United States
25 November	Brazil

2012 FIA World Rally Championship (13 events)

17 January	Rally Monte Carlo
9 February	Rally Sweden
8 March	Rally Mexico
29 March	Rally Portugal
27 April	Rally Argentina
25 May	Rally Greece
22 June	Rally New Zealand
2 August	Rally Finland
24 August	Rally Germany
13 September	Rally GB
4 October	Rally France
18 October	Rally Italy
1 November	Rally Spain



CLUBS

APPENDIX 10

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CLUBS

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WARNING SIGNS

APPENDIX 11

1. NOTICE FOR ALL MOTORSPORT VENUES AND EVENT PROGRAMMES.

SAMPLE WARNING NOTICE (Approx. 24" x 18") (Red on White)

WARNING
MOTORSPORT CAN BE DANGEROUS
DESPITE THE ORGANISERS TAKING
ALL REASONABLE PRECAUTIONS.
UNAVOIDABLE ACCIDENTS CAN HAPPEN.
THEREFORE YOU ARE PRESENT AT
YOUR OWN RISK.

11

2. NOTICE FOR ALL CLOSED ROADS APPROACHES.

SAMPLE WARNING NOTICE (Approx. 24" x 18") (Red on White)

WARNING
YOU ARE APPROACHING A MOTOR RALLY
SPECIAL STAGE.

These roads are closed to the public and you are warned
that Motor Sport can be dangerous and that you are present
in the vicinity of the course entirely at your own risk.



RALLIES, RACING & HILLCLIMBS

APPENDIX 15

CHAMPIONSHIPS - GENERAL CONDITIONS FOR RALLIES, RACES & HILLCLIMB CHAMPIONSHIPS & SERIES

1. DEFINITIONS.

1.1. Championship/Series: An event or group of events or Challenge organised for the purpose of establishing the right to an individual or collective title including "Cup" or "Trophy" or "Challenge" (See also GCR 17).

1.2. A Championship or Series may be one of the following status:

1.2.1. International.

Open to competition licence holders from all countries affiliated to the FIA. Subject to approval by the FIA and MI.

1.2.2. National.

Open to all competition licence holders from countries within the European Union affiliated to the FIA and includes in its title the initials MI. Subject to approval by MI and FIA sporting code No. 47.

1.2.3. Restricted/Approved.

Open to all competition licence holders from countries within the European Union affiliated to the FIA and meets the requirements as laid down in these General Conditions. This type of championship will normally incorporate a restriction on either the number of venues, events or organising clubs for the purposes of engendering local competitions. Subject to approval by MI.

2. CHAMPIONSHIP REGULATIONS.

2.1. Championship Regulations must be in compliance with GCR 183.

2.2. Championship Regulations must contain details of status, championship title, details of registration, name and address of Registrar, awards, method of resolving ties, eligibility of competitors and vehicles, event date/s, organising club/s, venue/s and in the case of single make championships, the name of an eligibility scrutineer.

2.3. Organisers of all National and Restricted Championships counting towards an individual or collective title must submit a copy of the proposed regulations to MI for approval prior to the publication of regulations and acceptance of entries.

2.4. The Championship regulations may not be amended after publication except as approved by MI before issue. Such amendments must be

circulated to all registered competitors, relevant scrutineers and clubs organising rounds of the championship.

3. SPONSORSHIP.

3.1. The right to associate the name of a commercial company, organisation or brand with an MI series or championship which already has an existing sponsorship agreement with MI is reserved exclusively for MI.

4. CHAMPIONSHIP/SERIES PERMIT

4.1. All Championships/Series must be approved by MI by the issuing of a permit. Applications may only be made by clubs affiliated to MI. A permit fee of €130 for will be charged for Tarmac and Forestry Rally Championship and €65 for Navigation Trial Championship. National and Restricted Racing and Hillclimb Championships will be subject to a fee of €130 and €65 respectively.

4.2. A written application for a Championship /Series Permit must be made to MI including two draft copies of the proposed regulations no later than four weeks prior to the first counting round.

4.3. The granting of a permit for a championship or series does not give the championship organiser the right to prevent other championships for that class of vehicle (subject to the approval of MI).

4.4. Championship /Series Regulations may be amended by MI as a condition of granting the Permit, and the Permit having been granted, must then only be issued in their approved or amended form to all prospective entrants.

4.5. Failure to apply for a permit for a Championship /Series 14 days before the first event takes place will result in a fine of €250 in total being placed on the applicant Clubs. Failure to apply for a permit for a Championship /Series before the second event takes place will result in a fine of €250 on each of the Championship /Series participating clubs. For example, a four round Championship /Series - no permit - fine €1,000. (GCR 146).

5. TELEVISION AND INTELLECTUAL PROPERTY RIGHTS.

5.1. In accordance with GCR 183.



RALLIES, RACING & HILLCLIMBS

APPENDIX 15

6. POINTS SCORING.**6.1. Championship/Series.****6.1.1 Stage Rallies.**

National Rally Stages Championship:

See Appendix 31 Article 5.1

National Forestry Stage Championship:

See Appendix 32 Article 7.

Other International / Restricted / Approved Rally Championships/Series:

Regulations must contain details of proposed points scoring system(s) when lodged with MI for approval.

6.1.2. Navigation Trials.

National Navigation Trials Championship:

See Appendix 34 Article 4.

Other Restricted / Approved Navigation Trial Championships/Series:

Championship/Series Regulations must contain details of proposed points scoring system(s) when submitted for approval to MI.

6.1.3. Racing.**6.1.3.1. Racing Points.**

20/17/14/12/11/10/9/8/7/6/5/4/3/2/1 applying to all Championships as approved by Race Committee.

6.1.3.2. Classes may opt to award an extra point for pole position and/or fastest lap. This must be specified in the approved class regulations.

6.1.3.3. If two competitors record equal fastest lap, the first recorded time will count.

6.1.4. Qualifying Numbers.

For a race to qualify as a championship scoring round a minimum of 7 starters is required. When a class has two races at one event - a "double header" - the requirement for a minimum of 7 starters applies to the first race.

6.2. For Racing purposes only, each round of a championship is considered an Event for the purpose of the regulations.

6.3. Points will be awarded as above on the basis of provisional results of each counting round and made final after all judicial hearings and/or eligibility examinations have been finalised/completed.

6.4. Exclusions.

Any competitor competing in a Championship /Series whose vehicle is excluded from the results of an event for breach of the Technical Regulations must count the event as one of the events contributing to his/her total championship score, be excluded from the event concerned forfeiting all championship points, prize money and awards associated with that event. These penalties will be applied automatically by Championship/Series organisers after the process of Protest and Appeal has been completed.

6.5. Where a competitor is excluded from more than one event for the same infringement of the Technical Regulations, then all the results for those events must be amended accordingly and any points gained must be redistributed to the other competitors, including those for fastest lap where applicable.

6.6. A decision to exclude a competitor may be made by the Clerk of the Course, Stewards of the Meeting or MI. If it has been proven that an illegal part or component has been used at more than one championship event, then the championship organisers will apply an exclusion for all such events. Such a decision may be subject to a query to the Championship Stewards as Rule 182.

6.7. A competitor excluded from an event for a breach of GCR No. 139.6 (improper driving) must count the event as a scoring round even if this results in a minus score. ***Any extra points gained are also forfeit (see 6.1.3.2).***

6.9. Maximum Scores.

6.9.1. The maximum number of rounds permitted in any Racing Championship shall be 16 rounds. All Classes will have the option to drop one championship round when there is 10 rounds and 2 rounds if there is more than 10 rounds.

6.9.2. Final 2 Championship rounds: Only registered championship competitors may take part in the last 2 championship rounds, unless agreed in writing by all competing registered competitors in the final 2 rounds. In such a case no individual non-registered competitor may be refused entry, who is otherwise eligible to compete.

6.10. Resolving Ties.

Unless the particular championship regulations specify differently, ties will be decided in the following order:

6.10.1 By considering in the best scoring events of the competitors concerned, the number of first places, second places and so on.

6.10.2. By considering the performance in sequential order of championship events.

6.10.3. By considering the placings in all Championship events.

6.10.4. If this does not produce a resolution, a tie will be declared.

6.11. Interim and/or Final: Results must be published by championship Registrars from time to time and will remain provisional until ratified by MI. For MI Championship/Series events and for non-MI Championship events the results should be submitted when available. All such issues must include a date of publication and the name of the Championship Registrar.



RALLIES, RACING & HILLCLIMBS

APPENDIX 15

6.12. Event results.

A complete set of results for each event should be delivered to MI no later than 7 days after the completion of the event (including practice times where applicable).

7. ADVERTISING.

Competitors may be required to carry advertising as supplied to publicise the championship in accordance with GCR Chapter 15 and in order to qualify for points or awards.

8. CHAMPIONSHIP/SERIES TITLES.

The Championship or Series must be referred to at all times and in all documentation by its correct and approved title.

9. DATES.

Championship organisers will publish the list of qualifying dates in the Championship Regulations. These may be subject to alteration, but the original number of events may not be increased. No alteration may be made without the prior approval of MI.

10. AWARDS.

Affiliated Clubs applying for a Championship Permit must satisfy MI, where prize money or bonuses are applicable, that there are sufficient resources available to provide an adequate awards ceremony.

11. FEES.

Registration Fees: Competitor Registration Fees or any other fees whatsoever associated with a Championship or Series will be subject to prior approval by MI.

12. CHAMPIONSHIP STEWARDS.

Please refer to Rule 182. of General Competition Rules.



JUNIOR RALLY CHAMPIONSHIP

APPENDIX 16

MOTORSPORT IRELAND BILLY COLEMAN AWARD

INTRODUCTION.

It has been the long held view in Motorsport that a young rally driver should be assisted towards succeeding in International Rallying and this came to fruition in the year 2000 with the inauguration of the BILLY COLEMAN AWARD. The winner, Rory Galligan, received support to the value of £25,000 towards furthering his career in 2001. Over the next 10 years, winners have competed and won at BRC, IRC, FIA and WRC level. This success has also given Motorsport Ireland the opportunity of nominating winners to represent our country at International Shootouts and recently to the FIA Institute Academy.

At the same time the IRISH SPORTS COUNCIL officially recognised motorsport by becoming a partner in the award scheme, thereby ensuring that the award, whilst taking on a much higher profile, also provides a much-improved package in real terms.

The rules and objectives of this prestigious award are currently being reviewed with a view to maximising the benefit to the stakeholders. The working group is set to complete its deliberations in December and the new update will be published by the end of January 2012 at the latest.

The objective of the award is to select a young driver who is prepared and able to compete Internationally, represent Motorsport Ireland abroad, and use the award to get International experience and promote Irish Motorsport.

WINNERS.

2000 Rory Galligan
 2001 Stuart Darcy
 2002 Shaun Gallagher
 2003 Dessie Keenan
 2004 Eamon McElvaney
 2005 Gareth McHale
 2006 Owen Murphy
 2007 Keith Cronin
 2008 Ross Forde
 2009 Craig Breen
 2010 Robert Barrable



SEXTON TROPHY

APPENDIX 17

THE DUNLOP SEXTON TROPHY YOUNG RACING DRIVER 2012

1. FORMAT.

The format for the "Young Racing Driver of the Year" will be similar to that utilised last year to select the winner of this scholarship scheme.

During the racing season a competitor will be selected for each of the six months, April through September, by a panel appointed by the Motorsport Commission. The monthly winners will go forward to a selection process the initial part of which will require each to:

- make a written submission covering such topics as motorsport career to date, future career development etc.
- be interviewed by a separate panel appointed by Motorsport Ireland, this panel will include:
 - a representative of the President of Motorsport Ireland.
 - a representative of the Irish Sports Council.
 - a representative of the Media.
 - a representative of an established PR entity.
 - the Chairman of the Race Committee.

This part completed, the number of finalists will be reduced to three, each of whom will then it is intended undergo a driving skills test before a competent and independent consultant. Finally the panel, with input from the consultant, will decide the winning order, its decision being final.

2. THE AWARDS.

The awards will be as follows:

The award will take the form of support to compete in an approved championship to the value of €50,000 subject to the support of the Irish Sports Council.

3. ELIGIBILITY FOR AWARD.

In order to be eligible to be considered for the "Young Racing Driver of the Year" award competitors must be:

- under 23 years of age on the first of January in the year in which the award is made
- hold a competition licence issued by Motorsport Ireland. Application for a waiver of this requirement may be made to the Motor Sport Commission (MSC) no later than 31 March of the year of the award. Each application will be judged solely on its merits and the decision of the MSC will be deemed final and absolute.

- be ordinarily resident in Ireland

- be competing nationally or internationally in FIA/MI approved motor racing events

While performances over the course of the season will be the most important factor in the determination of the award, competitors should also note that behaviour both on and off the track, personal presentation, communication skills and long term commitment to the sport will be taken into account in the final judging.

4. FUNDING AND TRAINING.

The award will apply for the 2013 season.

The funds to be paid in instalments, will only be made available provided the award winners are competing in an approved series.

In addition MI reserves the right to redirect funds to other placed winners should any recipient fail or cease to participate in his/her agreed series. MI also reserves the right not to make awards in any given year without prior notice.

The winners of the awards will be required to:

- meet with representatives of Motorsport Ireland and the Irish Sports Council to discuss the use of the financial packages during **2013**.
- display on their competition cars and their racing overalls etc. at all times Motorsport Ireland decals and other signage as provided.
- to sign a contract with Motorsport Ireland to cover the provisions for the receipt of the award.

ANY ONE PERSON MAY ONLY WIN THE AWARD ON A MAXIMUM OF TWO OCCASIONS.



MILLARD TROPHY

APPENDIX 18

JIMMY MILLARD TROPHY

1. INTRODUCTION.

The Trophy is awarded to a competitor who has made a contribution to the sport over and above their competition record. It is not bound by age, but is to recognise the sporting spirit of the individual. It has been presented in memory of Jimmy Millard who was a founder member of the MG Car Club Ireland and a competitor and official from the 1950's onward.

2. ELIGIBILITY.

The award is open to any holder of a current competition licence for any branch of the sport Except for :

2.1. Any individual who has been awarded the Sexton or Billy Coleman trophies, or has been on the top three short list for these awards

2.2. Any individual who is a past or present member of the Motorsport Commission

3. NOMINATION.

Affiliated clubs may make a nomination for any competitor who fulfils the criteria in 2 above.

The nomination must be a brief citation of the individual's achievements both as a competitor and in his other contributions to the sport. Ideally the nomination should be no more than one A4 page.

4. SELECTION PANEL.

A panel consisting of a nomination by each of the specialist committees (not Advisory Groups) will consider the club nominations and decide the award

4.1. The panel will decide the award on the information contained in the nomination; no interview stage is involved

5. CONVENING THE PANEL.

The Motorsport Commission will nominate one of its members to oversee the work of the Selection Panel and certify the voting of the Panel.



DUNLOP HAWTHORN TROPHY

APPENDIX 19

DUNLOP HAWTHORN TROPHY 2012

HISTORY.

The Dunlop Hawthorn Trophy was presented in 1959 by the Irish Dunlop Company to commemorate the memory of Mike Hawthorn, the Formula 1 World Champion, who enjoyed greatly racing in Ireland in the early part of his career.

1. ELIGIBILITY.

All racing competitors are eligible for this award. The counting rounds are published annually on the MI website and bulletin. Interim results are published periodically on the MI website. The award is presented the winner at the MI Annual Awards.

2. SCORING.

Points are located for first 30 competitors from 30 for 1st place to 1 for 30th place. Competitors do not need to finish a race. A class with less than 8 starters will not count for points.

Points will be awarded for a specific competitor in each class. If a competitor competes more than one class he will have separate scores in the overall points table for each class.

3. BONUS POINTS.

Bonus points will be allocated to the above score depending on the number of starters:

5 points to the winner

15 or more starters - 5 bonus points to all starters.

4. QUALIFYING RACES.

Where there are qualifying races, points will be awarded initially to competitors in the Final race. Should less than 30 competitors be in the Final, the balance of points will be allocated to non-qualifiers on the basis of their finishing times or positions in the qualifying races.

5. MERGED RACES.

Where championship classes are merged in a race then each class will be treated as a separate class for scoring points. However each class must have a minimum of 8 starters to qualify for points.

Formula Libre races will be treated as a single class each for Open and Closed wheel Libre.

6. QUALIFYING CLASSES.

All competitors in MI recognised Classes will qualify for the award.

A minimum of 8 starters is required for a race to qualify for award points.

7. COUNTING EVENTS.

Each Class will have 8 nominated events over the season for scoring points. As far as practicable the qualifying events will be held on the Island of Ireland.

If it is not possible to nominate events before the start of the season counting events are to be agreed between the Chair and Secretary of Race Committee and the Championship Registrar.

8. TIED SCORES.

Should there be a points tie at the end of the years competition the winner will be the person with most wins. If a tie remains the award will go to competitor with most 2nd places, followed by most 3rd places, etc. If a tie then remains the person with the most bonus points will win the award. Finally a remaining tie will be resolved by awarding the Championship to the competitor who competed in the class with the largest number of entrants (based on a cumulative count of all Qualifying events).



CHAMPIONSHIP WINNERS

APPENDIX 20

CHAMPIONSHIP WINNERS 2011

Billy Coleman Award
Sam Moffett

Dunlop Sexton Trophy Award
Aron Smith

Ian Webb Memorial Trophy
Tom Walsh

Dunlop Hawthorn Trophy
Lee Newsome (Formula Vee)

Dunlop National Stages Rally Championship - Vard Trophy
Tim McNulty and Paul Kiely

Forest Rally Championship - Dr Jackson Memorial Trophy
Owen Murphy and James O'Brien

Navigation Trial Championship 2010/2011
Arthur Kierans and Ashley McAadoo

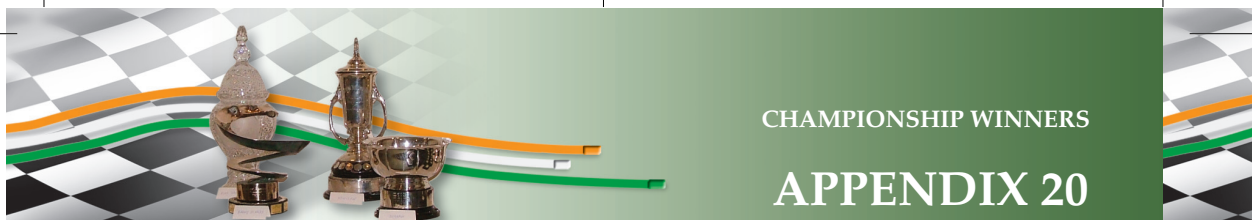
D&B Windows National Autocross Championship
Dermot Whyte (Special)
Ger Brennan (Saloon)

Connaught Competition Engines Hillclimb / Sprint Championship
Simon McKinley

George Mangan Trophy (Historic Cars)
Alan Kessie

National Rallycross Championship 2010/2011
Tommy Graham (Ford Fiesta)

Hewison Trophy National Autotest Championship
Steven Ferguson



BILLY COLEMAN AWARD

2000	Rory Galligan	2011	<i>Sam Moffett</i>
2001	Stuart Darcy		
2002	Shaun Gallagher		
2003	Dessie Keenan		
2004	Eamon McElvaney		
2005	Gareth MacHale		
2006	Owen Murphy		
2007	Keith Cronin		
2008	Ross Forde		
2009	Craig Breen		
2010	Robert Barrable		

HAWTHORN TROPHY

2004	Stephen Kershaw (Leystone)	2011	<i>Lee Newsome (Sheane Vee)</i>
2005	Quentin Smith (Punto Abarth)		
2006	Ray Moore (Leystone)		
2007	Paul Sammin (Sheane)		
2008	Jackie Cochrane (Sunbeam Tiger)		
2009	Alan Watkins (Stryker)		
2010	Jackie Cochrane (Sunbeam Tiger)		

NATIONAL RALLYCROSS CHAMPIONSHIP

2000	Laurence Gibson	2010/11	<i>Tommy Graham</i>
2001	Michael Coyne		
2002	Dermot Carnegie		
2003	Mike Manning		
2004	Dermot Carnegie		
2005	Dermot Carnegie		
2006	Dermot Carnegie		
2007	Derrick Jobb		
2007/08	Dermot Carnegie		
2008/09	Michael Coyne		
2009/10	Tommy Graham		



CHAMPIONSHIP WINNERS

APPENDIX 20

MILLARD TROPHY

1998	Gavin Smith	2011	<i>Shay Power</i>
1999	Peter Hennessy		
2000	Anthony Cross		
2001	Eoin Murray		
2002	Ciaran Dwyer		
2003	Emmett Queenan		
2004	Richard Hill		
2005	Tom Kirwan		
2006	Eamonn Byrne		
2007	Gerry Kelly		
2008	Des Ryan		
2009	Alan Kessie		
2010	Martin Rafferty		

GEORGE MANGAN MEMORIAL TROPHY

2000	Frank Cassidy	2011	<i>Alan Kessie</i>
2001	Jim O'Reilly		
2002	Ciaran Molumby		
2003	Jim O'Reilly		
2004	Jim O'Reilly		
2005	Jim O'Reilly		
2006	Ed Cassidy		
2007	Mick O'Shea		
2008	Ed Cassidy		
2009	Ed Cassidy		
2010	Matt Dunne		

D&B WINDOWS NATIONAL AUTOCROSS CHAMPIONSHIP

SPECIALS		SALOONS	
2001		Cancelled (Foot & Mouth Disease)	
2002		Ger Brennan	
2003		Gerard Lucey	
2004	Lionel Percy		Ger Brennan
2005	John Cavanagh		Ger Brennan
2006	John Cavanagh		Ger Brennan
2007	Chris Culleton		Ger Brennan
2008	Chris Culleton		Ger Brennan
2009	Chris Culleton		Ger Brennan
2010	Chris Culleton		Ger Brennan
2011	<i>Dermot Whyte</i>		<i>Ger Brennan</i>



CHAMPIONSHIP WINNERS

APPENDIX 20

NATIONAL FOREST RALLY CHAMPIONSHIP FOR THE DOC JACKSON MEMORIAL TROPHY

Previous Winners

1990/91	Andy Mackarel / Damien Courtney	Vauxhall Nova
1991/92	Joe McHale / John Noonan	Vauxhall Chevette HSR
1992/93	Dermot Kelly / Kevin Casey	Ford Escort Mk2
1993/94	Roy Haslett / Mike Gaston	Ford Escort G3
1994/95	Kevin O'Kane / Mark Murphy	Ford Escort Cosworth
1995/96	Kevin O'Kane / Mark Murphy	Ford Escort Cosworth
1996/97	Peter McCullagh / Joe Shinnors	Ford Escort Cosworth
1997/98	John McKeown / Padraig Barry	Ford Sierra Cosworth
1998/99	Dermot Kelly / Greg Shinnors	Ford Escort Cosworth
1999/00	Dermot Kelly / Greg Shinnors	Ford Escort Cosworth
2000/01	Niall Driver / Frank Corrigan	Mitsubishi Evo 6
2002	Eugene Donnelly / Frank Corrigan	Toyota Celica GT4
2003	Eddie Kinirons / Greg Shinnors	Toyota Celica GT4
2004	Kevin Lynch / Francis Regan	Subaru Impreza WRC
2005	Gareth MacHale / Paul Nagle	Toyota Corolla WRC
2006	John McCarthy / Mick Morrissey	Toyota Corolla WRC
2007	Stephen Moore / Tony McHugh	Ford Focus WRC
2008	Ray Breen / Enda Shiels	Ford Focus WRC
2009	Trevor Harding / Andrew Purcell	Mitsubishi Lancer Evo9
2010	Pat O'Connell / Mark Willey	Mitsubishi Lancer Evo9
2011	Owen Murphy / James O'Brien	Mitsubishi Lancer Evo9

IRISH NATIONAL LAND SPEED RECORD (4 WHEEL VEHICLE)

The RIAC Motor Sport Commission have ratified the following record which was set on 11th October 1999 in accordance with the FIA International Sporting Code, Appendix "G":

Competitor:	Brendan O'Mahony
Vehicle:	Jet Car Dragster
Venue:	El Mirage, USA
Weather:	Hot/Dry
Records set:	Flying 1/4 Mile - 301.409 mph

MOTORSPORT IRELAND HALL OF FAME

2000 Derek Daly
2001 Rosemary Smith
2002 Michael Roe
2003 Billy Coleman
2004 Alec Poole
2005 Noel Smith
2006 Martin Donnel
2007 Not awarded
2008 Not awarded
2009 Not awarded

2010 Not awarded
2011 Eddie Jordan



CHAMPIONSHIP WINNERS

APPENDIX 20

PREVIOUS SEXTON TROPHY WINNERS

1948	Dudley Colley	2008	Peter Dempsey
1949	Joe Kelly	2009	Patrick McKenna
1950	Pierce Cahill	2010	Patrick McKenna
1951	Irwin Catherwood	2011	Aron Smith
1952	Joe Flynn		
1953	Dick Odium		
1954	Joe Kelly		
1955	Mick Heather		
1956	Billy Bradshaw		
1957	Joe Flynn		
1958	Billy Bradshaw		
1959	Billy Bradshaw		
1960	Joe Flynn		
1961	Dan McAlister		
1962	Dan McAlister		
1963	Frank Keane		
1964	Mike Ivis		
1965	Robin Rennicks		
1966	Richie Heeley		
1967	Richie Heeley		
1968	John Keaney		
1969	John Keaney		
1970	David Furlong		
1971	Richie Heeley		
1972	Des McCoy		
1973	Ivan Rothwell		
1974	Ken Fildes		
1975	Derek Shortall		
1976	Derek Shortall		
1977	David Hall		
1978	Frank O'Rourke		
1979	Frank O'Rourke		
1980	Frank O'Rourke		
1981	Derek Shortall		
1982	Derek Shortall		
1983	Dick Smyth		
1984	Vivion Daly		
1985	Leslie Wright		
1986	Vivion Daly		
1987	John Hayes		
1988	Kerry Lawless		
1989	Fionn Murray		
1990	David Wright		
1991	Maurice Cassidy		
1992	Padraig Owens		
1993	Michael Cullen		
1994	Steve Griffin		
1995	Michael Barrable		
1996	Alan Byrne		
1997	Neil Shanahan		
1998	Neil Shanahan		
1999	Jonathan Fildes		
2000	Paul Dagg		
2001	Michael Devaney		
2002	Michael Devaney		
2003	Eoin Murray		
2004	Eoin Murray		
2005	Peter Dempsey		
2006	Paddy Hogan		
2007	Niall Quinn		



CHAMPIONSHIP WINNERS

APPENDIX 20

NATIONAL RALLY CHAMPIONS

Year	Driver	Navigator
1965/66	Noel Smith	not awarded
1966/67	Noel Smith	Ricky Foott
1967/68	Not awarded (foot and mouth disease)	
1968/69	Noel Smith	Ricky Foott
1969/70	Noel Smith	Ricky Foott
1970/71	Eamonn Cotter	Paul Phelan
1972	Eamonn Cotter	Paul Phelan
1973	Arnie Poole	Derek Johnston
1974	Sean Campbell	Paul Phelan
1975	Sean Campbell	Kenny Johnston
1976	Sean Campbell	Derek Smyth
1977	John Coyne	Christy Farrell

From 1978 separate championships were held for stages and navigation events.

STAGES CHAMPIONSHIP

Year	Driver
1978	Mick O'Connell
1979	Jer Buckley
1980	Jer Buckley
1981	Donie Keating
1982	Eddie Colton
1983	Bertie Law
1984	Bertie Law
1985	James Doherty
1986	Frank Meagher
1987	Vincent Bonner
1988	Ken Colbert
1989	George Robinson
1990	Richard Smyth
1991	Donie Keating
1992	Peadar Hurson
1993	Ian Greer
1994	Micheal Farrell
1995	Stephen Murphy
1996	Stephen Murphy
1997	John Gilleece
1998	John Gilleece
1999	Niall Maguire
2000	Paul Harris
2001	Not Awarded (Foot & Mouth Disease)
2002	Niall Maguire
2003	Eugene Donnelly
2004	Niall Maguire
2005	Ray Breen
2006	Charlie Donnelly
2007	Aaron MacHale
2008	Patrick Elliott
2009	Patrick Elliott
2010	Melvyn Evans
2011	Tim McNulty

NAVIGATION CHAMPIONSHIP

Year	Driver	Navigator
1978	Eamonn Cotter	Paul Phelan
1979	David Yeates	Leslie Fannin
1980	Frank Fennell	Frank O'Donoghue
1981/2	David Yeates	Paul Phelan
1982/3	David Yeates	Paul Phelan
1983/4	David Yeates	Paul Phelan
1984/5	David Yeates	Paul Phelan
1985/6	David Yeates	Paul Phelan
1986/7	David Yeates	Paul Phelan
1987/8	David Yeates	Paul Phelan
1988/9	Ronnie Hawe	George Hamilton
1989/90	Luke McCarthy	James O'Brien
1990/91	Sean Keenan	Paul Phelan
1991/92	Michael Bones O'Connor	Paul Phelan
1992/93	Michael Bones O'Connor	Paul Phelan
1993/94	Lloyd Hutchinson	Liam McGuinness
1994/95	Michael 'Bones' O'Connor	Paul Phelan
1995/96	Bertie Wedlock	Paul Hughes
1996/97	Michael Bones O'Connor	Paul Phelan
1997/98	Damien Courtney	Aidan Courtney
1998/99	Michael Bones O'Connor	Paul Phelan
1999/00	Michael Bones O'Connor	Paul Phelan
2000/01	Michael Bones O'Connor	Paul Phelan
2001/02	Not Awarded (Foot & Mouth Disease)	
2002/03	Andy Mackarel	Liam Higgins
2003/04	Derek Coleman	Conor Maguire
2004/05	Andy Mackarel	Liam Higgins
2005/06	Andy Mackarel	Liam Higgins
2006/07	Andy Mackarel	Liam Higgins
2007/08	Arthur Kierans	Ashley McAdoo
2008/09	Arthur Kierans	Ashley McAdoo
2009/10	Arthur Kierans	Ashley McAdoo
2010/11	Arthur Kierans	Ashley McAdoo

CHAMPIONSHIP WINNERS

APPENDIX 20

IVAN WEBB MEMORIAL TROPHY

The Ivan Webb Memorial Trophy is awarded each year to the person in Irish Motor Sport who has made the greatest contribution to the Sport either during the year or over a number of years.

PREVIOUS WINNERS

1975	Wilford J. Fitzsimmons	2000	Michael O'Carroll
1976	Nathan Lepler	2001	Michael FitzSimons
1977	Jimmy C. Millard	2002	Joe Shirley
1978	Dr. Norman Jackson	2003	Martin Birrane
1979	Paul Phelan	2004	Declan Mullally
1980	Maurice Russell	2005	Joe Corcoran
1981	Barney Manley	2006	Dermot Carnegie
1982	Dudley Reynolds	2007	Ronan Morgan
1983	Edmund P. Gill	2008	Robert Moody
1984	Alex Watkins	2009	John Naylor
1985	Nuala Ní Bhriain	2010	Robert Lyttle
1986	Dominic Murphy	2011	Tom Walsh
1987	Douglas Hughes		
1988	John F. Wood		
1989	Donald Grieve		
1990	George Clancy		
1991	David Sheane		
1992	Peter Jenkins		
1993	Pat Sheil		
1994	Reg. Redmond		
1995	Norman Williams		
1996	Jimmy Cleary		
1997	Cecil Sparks		
1998	Ed. Colton		
1999	Alex Sinclair		

NATIONAL HILLCLIMB CHAMPIONSHIP PAST WINNERS OVERALL

1978	Derek Shortall	2000	Karl Cleary
1979	Emmet Hart	2001 (Challenge)	Frank Byrnes
1980	Ken Fildes	2002	Frank Byrnes
1981	Ken Fildes	2003	Michael B. Roche
1982	Ken Noblett	2004	Pat Roche
1983	David Manley	2005	Michael B. Roche
1984	Shay Lawless	2006	Frank Byrnes
1985	Shay Lawless	2007	Paul O'Connell
1986	Shay Lawless	2008	Paul O'Connell
not held in 1987 and 1988		2009	Paul O'Connell
1989	Richard Young	2010	Simon McKinley
1990	Richard Young	2011	Simon McKinley
1991	Seamus Noonan		
1992	Jenny Kennedy		
1993	Andy Kittle		
1994	Andy Kittle		
1995	Andy Kittle		
1996	Richard Young		
1997	Ronnie Maybin		
1998	Donal Griffin		
1999	Donal Griffin		



CHAMPIONSHIP WINNERS

APPENDIX 20

HEWISON TROPHY WINNERS (1958 and 1959 championships were for teams)

1938	Wilfie Fitzsimmons and Charlie Manders	2000	Eamonn Byrne
1939	George Mangan	2001	J.J. Farrell
1940	Paddy Le Fanu	2002	J.J. Farrell
1947	Pierce Cahill	2003	Eamonn Byrne
1948	Wilfie Fitzsimmons	2004	Eamonn Byrne
1949	Jimmy Millard	2005	Eddie Peterson
1950	Cecil Vard	2006	Eddie Peterson
1951	Kevin Murray	2007	Eddie Peterson
1952	Kevin Murray	2008	Eddie Peterson
1953	Kevin Murray	2009	Eddie Peterson
1954	Kevin Murray	2010	Eddie Peterson
1955	Paddy Hopkirk	2011	Steven Ferguson
1956	Declan O'Leary		
1957	Paddy Hopkirk		
1958	MG CC (Jimmy Millard, Kevin Sherry, Cecil Vard)		
1959	Leinster-DU (Sam Logan, Alec Malcolm, Reg Redmond)		
1960	Shea Griffin		
1961	Shea Griffin		
1962	Shea Griffin		
1963	Steve Griffin		
1964	Steve Griffin		
1965	Steve Griffin		
1966	Con Linehan		
1967	Dermot Carnegie		
1968	Not awarded (foot and mouth disease)		
1969	Leslie Vard		
1970	Dermot Carnegie		
1971	Dermot Carnegie		
1972	Dermot Carnegie		
1973	John Lyons		
1974	John Lyons		
1975	Dermot Carnegie		
1976	Alan Murray		
1977	Dermot Carnegie		
1978	Alan Murray		
1979	Dermot Carnegie		
1980	Ken Irwin		
1981	Alan Murray		
1982	Dermot Carnegie		
1983	Charlie Irwin		
1984	Dermot Carnegie		
1985	Ken Irwin		
1986	Dermot Carnegie		
1987	Sam Bowden		
1988	Sam Bowden		
1989	Sam Bowden		
1990	Eamonn Byrne		
1991	Sam Bowden		
1992	Sam Bowden		
1993	Eamonn Byrne		
1994	Eamonn Byrne		
1995	Eamonn Byrne		
1996	Eamonn Byrne		
1997	Eamonn Byrne		
1998	Peter Grimes		
1999	Eamonn Byrne		



REQUIREMENTS FOR ALL RALLIES

APPENDIX 25

MOTORSPORT IRELAND REQUIREMENTS FOR THE ORGANISATION AND SAFETY OF ALL RALLIES INCORPORATING THE 'RALLY CODE'

SPECIAL STAGE RALLIES

All Stage Rallies **MUST** comply with the following requirements which have been revised to include both the Rally Code and Safety Requirements and which are designed to assist organisers and competitors, to increase the safety of the events and to keep inconvenience to non-competitors and local residents to a minimum.

Appendix 29 has also been revised to include a sample of the Supplementary Regulations, which must be sent to all entrants. Promoters are urged to give proper consideration to the information contained in the Supplementary Regulations, as it is not possible to amend these except in exceptional circumstances. Permit Applications (with draft supplementary regulations) for all stages rallies must be submitted to MI a minimum of 12 weeks before the date of the event. A reply will be forthcoming within 2 weeks. Supplementary regulations may not be issued, either in printed form or on a website, until the draft regulations have been approved by MI. Promoters are also reminded that the FIA, General Prescription - Article 72 states that no entries shall be received by the organisers until a permit has been issued.

Organisers are reminded that entries must be selected according to the order of them being received or by means of ballot, unless otherwise stated in the supplementary regulations. The system for dealing with entries received for events that are counting rounds of the National Championship is governed by Appendix 31. The remaining entries must be placed on a reserve list.

The following items must be given careful consideration when organisers are drafting the Supplementary Regulations:

1. Officials.

These are subject to approval by Motorsport Ireland - FIA General Prescriptions Art.135 and certain officials must hold Officials Licences - GCR 123 as issued by Motorsport Ireland.

2. Classes.

The classes included in the Supplementary Regulations must be those specified in Appendix 29. The classes for events, which are counting rounds of the National Championships, are included in Appendix 31 and 32'.

3. Pace Notes

The times and dates when making of Pace Notes, if Pace Notes are allowed for the particular event, may be included in the Supplementary Regulations. Alternatively the times and dates can be notified to the competitors by the way of Final Instructions. Organisers are reminded that restrictions apply to the periods allowed for the making of Pace Notes. Organisers and Clerks of the Course **do not** have the discretion to allow a competitor make Pace Notes outside of the approved times.

1. PRE-PLANNING

1.1 SAFETY PLAN

The organisers must produce a Draft Safety Plan in accordance with the standard safety plan requirements and it must contain all the details set out in that document. The MI Safety Officer accompanied by a senior member of the organising Club must inspect the route a minimum of 8 weeks prior to an event. MI will appoint an experienced competitor to assist the MI Safety Officer in inspecting the stages. The competitor and Safety Officer should traverse the proposed stages together. This competitor cannot take part in the event as a driver, navigator or entrant. The stage inspection should involve a hazard identification, risk assessment and risk reduction process. This process to be applied to the entire stage i.e. all areas designated by a location no. and all sections between these locations. Consideration to be given to spectator safety – primarily location set up, prohibited areas etc. and competitor safety – primarily the identification of 'prominent' hazards (piers, walls, poles, trees, walls, bumps etc.). The use of bales / tyres / chicanes etc. as appropriate, should be employed as a risk reduction measure in such cases. Tyres may not be used to define chicanes. Details of all risk reduction measures to be included in the event Safety Plan. During this inspection amendments, if necessary, can be made to the Draft Safety Plan.

If the layout of the Rally precludes one individual from inspecting all Stages adequately a second Safety Officer may be appointed with the same responsibilities. During this inspection amendments, if necessary, can be made to the Draft Safety Plan.

The Safety Plan should detail the number of Marshals per stage and their location, Medical, Ambulance and Rescue locations with emergency routes for ambulances, and details of the communication network etc.



REQUIREMENTS FOR ALL RALLIES

APPENDIX 25

The Safety Questionnaire must be signed off by the MI Safety Officer when he / she considers the Safety Plan to be complete. The Safety Plan and Safety Questionnaire must be received by Motorsport Ireland a minimum of 4 weeks prior to the event. Failure to comply with this rule will mean that a permit WILL NOT be issued for the event. The plan and questionnaire will be carefully scrutinized and only if they appear to cover adequately all safety requirements for the event will a permit be issued. Clubs should note particularly that all the provisions set out in the Safety Plan and Questionnaire will be checked before the event commences and will be regarded as the basic safety standards necessary. The first Stewards meeting at an event should deal with the examiners comments and any changes to the Safety Plan.

1.2. SAFETY OFFICER and ZERO CARS.

1.2.1. The Safety Officer is appointed by MI. He/She must be independent of the Club Organisation but must work with them to approve the Club's Safety Plan for the event. He/She must act in accordance with the duties of a Safety Officer as set out below. He/She must be listed as an Official in the Supplementary Regulations. His/Her duties will primarily be concerned with Spectator Safety as well as safety of officials, marshals and competitors. He/She must be in contact with the Clerk of the Course and will have the personal responsibility to recommend to The Clerk of the Course the cancellation of any stage if he is not satisfied with Spectator Safety arrangements. The Clerk of the Course or Deputy C.O.C. and MI Safety Officer will travel through each stage, with clearance for competing cars to start each stage being given by the Clerk of the Course in consultation with the MI Safety Officer.

Clubs must appoint a Club Safety Officer who will have responsibility to fully implement the agreed Safety Plan and will be directly responsible to the Clerk of the Course.

The MI Safety Officer will inspect the stage after the organisers have declared it ready to run. If the MI Safety Officer is not satisfied that the stage is both safe and set up in accordance with the Safety Plan he/she will not give approval for the stage to run. He/she will notify the Clerk of the Course and the Stewards of the event of his/her recommendation.

1.2.2 OO Safety Cars (Previously Zero Cars) 1.2.2.1 Organising Club Guidelines and Responsibilities

The Role of the OO Safety Cars crew is to drive the special stages to ensure that they are properly set up in accordance with the event Safety Plan. The OO Safety Car check is the final check on the special stage before the first competitor starts each stage. The focus of OO Safety crew is event safety, not speed. Ample time allowance is

provided in the event schedule for OO Safety crews to perform their duties. Radios for direct communication with Rally Control will be provided to OO Safety crews.

The choice of OO Safety crews by organising clubs must be done with safety as the foremost factor. Competent crews with experience on stages and familiarity with the Safety Plan must be used. Clubs must not be seen to assign OO Safety positions to crews on the basis of favouritism or friendship.

Organising clubs must include OO Safety Crews in Safety Plans as named officials. It is highly recommended that at least one crew member should hold an officials licence.

Clubs must provide a Senior Official to carry out OO Safety Crew briefing. This should draw attention to general hazards and safety of marshal and spectator locations.

The MI Safety Officer must be confident that OO Safety crews are able to deal with any item which he requires them to address.

The schedule for the OO Safety Cars will depend on the length and nature of the stage and will be included in the Safety Plan. For a 20 km stage it is recommended that there be an approximate 18 minute gap (calculated at an average speed of 60kph) between the MI Safety Officer car and the first competitor. This gap may be adjusted down proportionately for shorter stages.

Crewmembers of OO Safety cars shall not be charged an Entry fee or be asked to make a financial contribution to the organisers.

OO Safety Car drivers must be included in the Irish Rally Drivers Scheme (IRDS) as per Appendix 28

The decals of OO Safety cars must include the word "Safety" as per sample, with the word "Safety" appearing above the number.

Rally Radio Control may refer to OO Safety Cars as 001, 002 etc without the use of the word "Safety"

1.2.2.2 OO Safety Car Requirements

- All OO Safety Cars should be rally prepared and must pass pre-event scrutiny.

- The crew must have the relevant competition licences and be signed-on as event officials.

- WRC cars may only act as OO Safety cars on events which permit WRC cars to compete.



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- 4WD Rally cars, but not WRC cars, may act as OO Safety cars on Mini Stage events

1.2.2.3 Crew Briefing and Responsibilities

- Crews will be briefed before the event by the organising club.

- A specific time and place for the briefing should be notified to the crew.

- The crew is to be provided with a summary Safety Plan, covering stage information only.

- The crew is to be made aware of any particular areas on stages that may cause problems, such as areas where spectators gather or residences.

- The crew should have a working understanding of the Safety Plan, particularly the importance of 200m run-off areas, box junctions and chicane layout.

- Should the OO Safety crew be unhappy with a particular matter on a stage they should have the competence to deal with or report the matter as necessary. If this requires them to alight from the car they must report that they are stopping to Rally Control, inform Rally Control of the circumstances and confirm when they are resuming progress.

- OO Safety crews **MUST** stop at ALL radio points unless otherwise instructed by the Start Official or Rally Control.

- OO Safety Crews must understand the importance of their role as officials of the event. In accepting the role they must behave responsibly and at no time bring the event into disrepute by any misbehaviour or bravado driving. Video or other evidence may be used to monitor crews and penalties may be applied where reckless or careless behaviour is found to have occurred.

1.2.2.4 Organisational Matters

- The running order for course cars will be included in all event schedules and the time intervals will be agreed with the MI Safety Officer and included in the Safety Plan.

- Clubs may use up to four OO Safety Cars after the MI Safety Officer. The last five course cars into the stage to be MI Safety, 004, 003, 002, 001. (in that order where possible)

- Retired cars from the main field may only be used as OO Safety cars in the event of a shortage / retirement of appointed OO Safety cars such that only two OO Safety cars remain. This must be agreed between CoC and MI Officials, preferably with the MI Safety Officer.

- The final OO Safety car must only stop in case of emergency (other than being held at mid-point radio) and must notify Rally Control immediately.

- All OO Safety cars must carry a Stage Stop board/ flag to halt the stage if necessary.

- OO Safety Cars must not be given a countdown at stage starts. A finish time may be recorded on the time card for schedule purposes only.

Sample of Safety 001 decal

SAFETY
001

1.3. TRAINING.

Clubs must carry out advance training of Officials and Marshals. Training aids in the form of booklets will be provided for Clubs and Marshals and films are available.

No Marshal under eighteen years of age shall be appointed. Inexperienced Marshals should always work with experienced marshals.

1.4. CHIEF MARSHAL.

The Chief Marshal will be responsible for the following:

a) For ensuring there are sufficient marshals for the event.

b) For ensuring the marshals are trained.

c) The deployment of marshals at the event.

d) Appointment of Sector Marshals.

So as to fulfil this role he/she will need to work closely with and consult with the COC, the various Stage Commanders, the event Safety Officer and be fully familiar with the marshal requirements in the Safety Plan.

1.5. SECTOR MARSHAL.

Sector marshals to be identified by distinctive tabard, and supplied with location set-up diagram, yellow flag, whistle and SOS board.



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2. ROUTE.

2.1. PUBLIC RELATIONS.

Residents and other interested parties on and adjacent to the route who are likely to be disturbed or inconvenienced must be notified in advance about the event. Where at all possible this note must be in writing and should enable residents and the Gardai to contact the organisers if necessary. Organisers should route events around towns and villages where possible, unless petrol or meal stops are planned.

For Stage Rallies clubs must seek every opportunity to inform the public of the need for their full co-operation to achieve safety and enjoyable spectating by means of advance publicity, press coverage and by informative use of loud-speaker cars used for crowd control purposes.

2.2. GARDA LIAISON.

The Garda officer in charge of the area affected by the Rally Route, including the Rally HQ and the service area should be contacted in writing at least three months in advance and a meeting requested to discuss the event. This meeting will be useful in agreeing a plan for traffic management and any other issues, which may arise during the event. A senior member of the organising committee, preferably some one other than the Clerk of the Course should be nominated as the liaison between the organisers and the Gardai.

2.3. ROAD CLOSING / ROAD OPENING.

2.3.1. Road Closing.

Road Closing orders should be planned to allow for the first car due at 1 hour after the commencement of the order. This should allow the stage to be set up and checked properly and not delay the 1st car due time. The schedule for the event should be finalised in advance of the road closing application being submitted. A copy of the road closing order must be made available to the Stewards and Safety Officer.

2.3.2. Road Opening.

The stage services (Doctor / Rescue Unit / Ambulance) must sweep the stage after the last run through the stage is completed, **lead** by the Stage Commander or his / her Deputy. A 'Road Open' car must immediately follow this convoy **to the stage exit**. Stage Commanders must be supplied with 'Road Open' doorplates to enable **them or a deputy** to run **immediately** behind the stage services and to perform this function in the event of the 'Road Open' car being delayed or unavailable. **The road open car must not be asked to perform any other duties whilst traversing the stage. Each stage should have its own dedicated road open car.**

2.4. STAGES.

In multi-stage rallies no stage may exceed 20 kilometres in length but this may be extended in exceptional circumstances for existing classic stages located in remote unpopulated areas. Not more than six separate stage locations may be included in any one day. For forestry events on unsurfaced roads total stage length must not exceed 115 kilometres per day, utilising more than 6 stages if necessary. No stage to be used more than twice. Single stage rallies will be limited to 20 kilometres with three timed-runs and one reconnaissance run. The maximum distance for forest stages should be 25 kilometres. If a forestry stage exceeds 13 kilometres, a full set of services, (Doctor, Rescue, and Ambulance) must be located at midpoint, with a second deputy or assistant stage commander who must be in radio contact.

2.5. STAGE SELECTION.

All Rally routes should avoid towns and villages as far as possible. Special Stage selection should include, as one factor, access for spectators and suitability of viewing points. The use of built up areas in towns and villages is not permitted. Built up areas are defined as roads bounded by kerbs or footpaths backed by either private houses or commercial buildings which have direct access on to the footpath. They may also include obstacles such as lampposts, telegraph poles, traffic signs etc. MI Safety Officers have been asked to become involved in the planning of events as soon as possible after their appointment. In particular the route of each event should be vetted by the Safety Officer. Clubs should pay particular attention to the location of any Pubs in relation to a stage and each situation will be judged by the Safety Officer on its merits as to its suitability. If a classic stage should include a short length of built up area application for its use must be made 3 months prior to the event and if granted will require special safety precautions equivalent in full to those required for street racing circuits.

2.5.1. Motorway or dual carriageway flyovers may not be used as part of a Special Stage.

2.5.2. All railway bridges must be marshalled.

2.5.3. Fire points should be established at all numbered locations. Where the distance between numbered locations substantially exceeds 2km additional Fire Points should be inserted at intervals of approx. 2km. Fire Points consist of two fire extinguishers and should be clearly identified by signage.

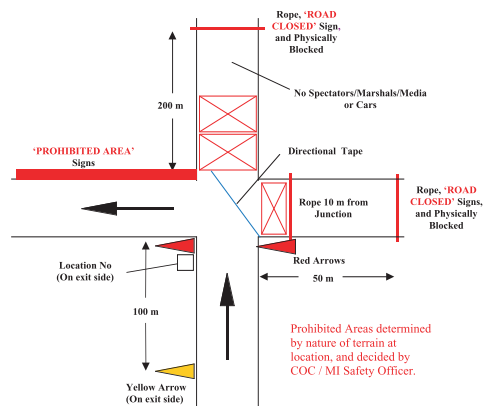
2.6. ACCESS ROADS.

All access roads to a Stage should be closed by Road Closing Order for a minimum of 200 metres (or to the next junction) before the Stage.

Article 2.6

Location Set-Up Requirements

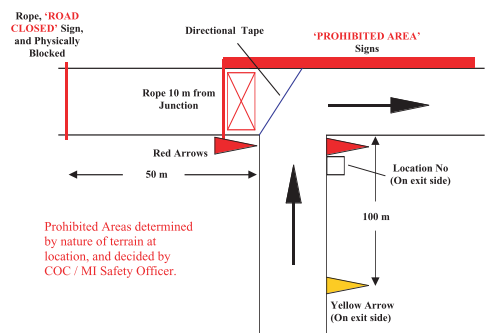
Crossroads



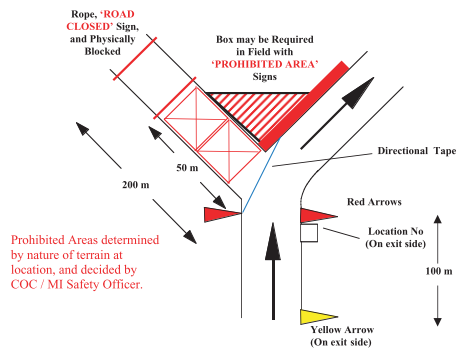
Spectators to be kept at least 10 m back from junction on road opposite direction of rally traffic.

Direction of rally route to be highlighted with 'directional' tape, where a change of direction occurs. (Use tape of a different colour to that used for box junctions etc.)

'T' Junction



'Y' Junction



At y-junction, spectators, if allowed to stand on the road, must be back at least 50 m. This to be decided between COC and MI Safety Officer.

Special Stage Taping Requirements – Gates/Gaps/Entrances

Single Gate:



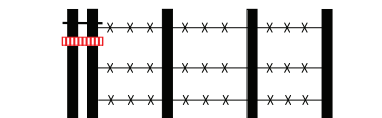
Tie tape around gate and post below top bar and around bolt in closed position

Double Gates:

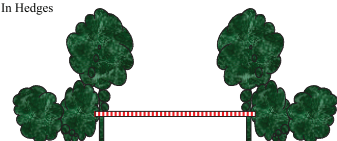


Tie tape around gates below top bar and around bolt in closed position

Wire Gate - (Slap):



Gaps – In Hedges



Tie tape from bush to bush

Gaps -- In Open Ground:



Drive two stakes and tie tape from stake to stake

Gaps – Between Piers:



Tie tape between two piers

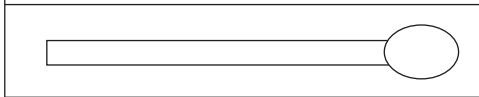


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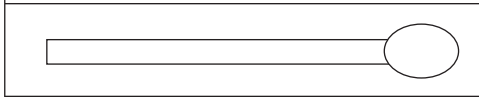
GATE TAGS: Material: Gate must be made of water and tear resistant paper and be highly visible.

**THIS ENTRANCE HAS BEEN
SEALED FOR SAFETY
REASONS AS THERE IS A
MOTOR RALLY TAKING PLACE
ON THIS ROAD**



FRONT

**STAGE NAME
THIS RALLY TAKES PLACE
ON: DATE
BETWEEN: TIMES
PLEASE DO NOT USE THE
ROAD DURING THESE TIMES**



BACK

All escape roads to be physically blocked at a minimum of 200 metres from the location. (The block point to be marked on the road). All other access roads to be physically blocked a minimum of 50 metres from the location. Ropes should be used to seal off the roads at these points and a warning notice should be fastened to these ropes.

In addition all entrances to dwellings and lands along the route must be sealed. Gates should be tied such that they cannot be opened without breaking/undoing the tape. The tape should be tied in such a manner so as to be easily visible when being checked by the officials as they proceed through the stage. A physical barrier must be used to seal off access roads.

The use of: "Gate Tags" is optional to seal off entrances to dwelling and lands along the route. See diagram above.

Escape roads must be kept clear of spectators and vehicles at all times for a distance of 200 metres. (See diagrams on previous page).

2.7. FORBIDDEN GROUND.

It is obligatory to display FORBIDDEN GROUND notices where necessary at all junctions and at corners etc. on all special stages where spectators are likely to congregate in a dangerous place. These notices must be, wherever possible, mounted on stakes and driven firmly into the

ground. They must be properly printed but may carry advertising provided this does not dominate the message FORBIDDEN GROUND. This can be difficult to pinpoint in a forest where the use of distance in metres is recommended. Spectators must stay behind the "Tree Line" at junctions (Assuming of course that the trees are both mature & substantial) Road margins are of course out of bounds.

2.7.1. Junction set-up diagrams MUST have FORBIDDEN GROUND areas and box junctions outlined with reference to landmarks such as poles, gates, house entrances etc. If landmarks are not readily identifiable specified distances e.g. 50 metres or 100 metres, may be used to describe such areas. In forestry events such areas to be decided in consultation with the Safety Officer.

2.8. SPEED LIMITATIONS.

2.8.1. No competitor may be required to average more than 50 kph except during Special Stages. The maximum required average speed for loose surface stages shall not be more than 110 kph and for tarmac stages 130kph, as per GCR Chapter 2, Rule 22.2.1. The length of each Special Stage and the target times ('Bogey Time') shall be published in the Safety Plan, Road Book and on the Time Card.

Time Allowances must be based on the following:



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2.8.1.1. Stages.

Competitors' Stage Maximum.

Distance in kilometres X1.35 (round down to nearest minute).

For all stages up to 11 kilometres in length, a 15 minute stage maximum will apply.

(This may have to be amended for short forestry stages. However, care should be taken to provide sufficient time to check the stage in safety).

Course Car Schedule Timing.

Distance in kilometres X1.20 (round up to nearest minute) - i.e. average 50kph.

Road Sections (Competitors and Course Cars).

Up to 2 kms 6 mins

2 kms to 5 kms 9 mins

5 kms and upwards Distance X 1.20 + 3 mins for formalities.

(eg) 6 km Section 6 X 1.20 + 3 = 10 mins.

12 km Section 12 X 1.20 + 3 = 17 mins.

Where secondary roads are concerned distance in Kilometres X 1.5.

2.8.2. All events must be scheduled to ensure that, under normal circumstances, the first car will have completed the final stage before 1700 hours.

2.9. START, FINISH.

Start areas should provide sufficient space to avoid congestion and should NOT be on a Public Road. Where a large number of officials are required they should meet at some place other than the start and convoys of official cars should be kept to a minimum.

The Stage Arrival Control [SAC] should be at least 500 metres along the closed road from the public road. Stage start is recommended to be at least 300 metres from the SAC. For very short Forestry stages however with a "Gate to Gate" length of 7 kilometres or less the SAC could be on a public road but the stage start must be a minimum of 300 metres into the forest.

The Stop Car on these stages must be a minimum of 100 metres from the exit gate. The use of a red flag between Finish & Stop car is recommended in such cases.

At the end of a Stage, the distance between the Finish board and the Stop car should be at least 300 metres and suitably marked with 3.2.1 boards. No spectators will be allowed on the road or roadside in this area, or for a distance of 50 metres after the Stop car. The position of the stage finish must be chosen with care to ensure maximum safety for the Flying Finish, Stop and Radio Cars. It is recommended that the Stop Car be positioned a minimum 650 metres from the public road. There should be no change of

direction between the Flying Finish and Stop. The location for the Arrival Control, Start Control, Flying Finish, Stop Car and Radio Car will be agreed and included in the Safety Plan.

2.10. SERVICE.

Every effort should be made when planning the route to ensure that a suitable off-road area is available for servicing. The use of Public Roads should be as a last resort. A suitable stretch of Public Road should be selected and roads closure order applied for and this can then be used as a service area. The location of service should not be indicated in the programme and should not be made known to spectators. In SERVICE AREAS on public roads, only one side of the road is to be used. Clubs must use a Time-in and Time-out control system. Marshals must be provided to control Competitors and Spectators, prevent double parking and keep traffic flowing. A minimum service time of 30 minutes is recommended. Service time may not be reduced by more than 20% of time published.

2.10.1. Highly recommended at service areas on all stage rallies.

Doctor and ambulance with AED unit and Comprehensive Burns Kit.

2.11. CONTROLS & CHECKS.

All official cars at controls and checks must be clearly identifiable as laid down by the Supplementary Regulations for the event. Route checks should not be incorporated unless necessary for safety. A control, time point or check shall be deemed to extend for 25 metres around the actual point at which the official is sited (which must be on the specified approach or departure road), unless clearly visible signs are displayed to define a different area. If officials are not present a scheduled manned control or check during the whole period when competitors may report all performances at such a point will be ignored in compiling the results. Performances at a control or check not sited at the point specified will also be ignored.

2.12. DIRECTION OF TRAVEL.

At no time during an event should competing cars be required to travel in opposite directions over the same piece of road unless the time schedule precludes any possibility of overlap, and unless the piece of road is of a broad two lane type adequate for two-way traffic.

2.13. FLAG SIGNALS.

2.13.1. Yellow Flag.

The marshal in charge of areas of particular risk should be provided with a YELLOW FLAG to be used as an additional hazard warning of potential danger, if an incident occurs or spectators encroach in his area. These flags will NOT involve stopping the stage.



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Use of Yellow Flag:

Yellow Flag – Stationary: Danger – hazard ahead, slow down, (no overtaking).

Yellow Flag – Waved: Great danger, slow down considerably, be prepared to suddenly change from the projected racing line or take other evasive action, be prepared to stop if necessary (no overtaking).

2.13.2. Stage Stopped Board / Flag.

The Clerk of the Course and Safety Officer will decide in advance where this board/flag may be deployed and it must be under the control of the Senior Marshal on the junction or at the hazard point. A stage stopped board/flag must be issued to each radio-point and should be demonstrated to competitors at briefing.

If spectators refuse to move from what the Organisers have agreed is to be a prohibited area the marshal has the authority to walk a hundred yards back into the stage and display the stage stopped board/flag to the drivers. This will have the immediate effect of telling the drivers that the Stage Timing is being stopped and they must then slow to 50 kilometres per hour or less while driving through the remainder of the stage. The board/flag measuring 600 x 600 or larger will comprise a White background with a Red clock face crossed by a black diagonal bar.

Where a Stage Stop board/flag is shown competitors will be given a time as for an interruption of a Timed Stage.

2.14. ALTERNATIVE ROUTE.

Clubs are required to provide an alternative route in the Competitors Roadbook for use in the event of cancellation of a Stage or other cases of force majeure rendering the passage over a timed Stage impossible.

2.15. COMMUNICATIONS.

As a minimum requirement radio communication must be provided linking the Clerk of the Course, the start and finish of each Stage, and intermediate points on any Stage over 13 Kilometres in length, the MI Safety Officer, Event Safety Officer, MI Steward(s), the Chief Scrutineer and Ambulance locations. Any stage over 13 kilometres in length must have at least two radio points in addition to start and finish. All stage radio operators must have communications as their SOLE function. Radio points and stage finish locations must be manned by at least two people.

Radios are provided for the safe and efficient running of a Rally and all messages should relate to this. With this in mind all messages should be transmitted through Base Control or the C.O.C. Where the radio operators are not very familiar with radio procedure their name or title within

the rally should be used. All mobile radios should report to Base Control any problems they encounter on a stage and report when they have completed the stage, stating they are happy with the stage.

Clubs COCs are reminded of the need for sensitivity with regard to data transferred over the radio system on events, in particular, the controller should be reminded to instruct Radio operators that competitor names and/or competition numbers must not be transmitted.

2.16. DOCTOR, RESCUE, AND AMBULANCE.

Ambulance to be located at the start of stage with the doctor and rescue vehicle. In the event of an accident the Doctor, Rescue Vehicle and Ambulance, should on the instruction of the stage commander, proceed down the stage. Should the accident happen after the mid point, the doctor should collect the ambulance from the mid point and take it with him to the scene. A Health Services Executive (HSE) standard of ambulance, and personnel if possible, to be used at the start of all special stages. A spare Ambulance and personnel, **which must be supplied by an Approved Service Provider (CPG 3rd Edition) as listed by the Pre Hospital Emergency Care Council www.phecit.ie. Two crew must be supplied, of which one crew must be at least Paramedic standard and one at least Emergency Medical Technician (EMT) standard.** To be located at a central control point and be contactable / equipped with a rally network radio.

2.17. GROUPING OF STAGES.

Grouping of Stages to cover 2/3 runs can confer benefits to the Club by concentration of Marshals and reducing movement of Spectators from stage to stage. In this case it is important to reduce the time interval between the end of the run and the start of the next. The loud-speaker car could be used to give spectators information and check their positions.

2.18 SCRUTINEERING

Event organisers must provide suitable covered facilities in order that pre and post event eligibility checks may be carried out by scrutineers.

3. OPERATIONAL.

3.1. STARTERS.

3.1.1. The number of competition cars, which may pass through any control, including the Start Control, [prior to the introduction of any specified classes listed at Art 3.1.2 below] is as follows:

3.1.1.1. 131 if any stage location is used on more than two (2) occasions for the duration of the event.



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3.1.1.2. 151 if any stage location is not used on more than two (2) occasions for the duration of the event.

This applies equally to International, National or Closed events.

3.1.2. Certain classes e.g. Historic Cars or a Junior Rally, or a 'one-make championship' participating using the same permit or a separate permit, may be permitted to join national multi stage rallies (not Single Stage or Mini Stages) at a point other than the start. Organisers must specify, in the Supplementary Regulations, the time control and stage number at which these competitors will join the event. This cannot be earlier than the Arrival Control of SS4 **and 20 additional competitors are allowed to start at this control.** These additional competitors are allowed to start at this control. These competitors cannot be included in the overall results classification for the main event. The above additional competitors must be run at the back of the main event and must be numbered in a different number sequence e.g. H1, H2 etc. or J1, J2 etc. In the interests of safety, organisers should consider whether the additional entries are likely to be considerably faster than the later competitors in the main field. If so an additional Zero Car must be used in advance of the additional competitors. In all other cases a maximum gap of 3 minutes may be given.

3.1.2.1. For International events [i.e. Galway, Rally of the Lakes, Donegal & Cork '20'] the additional competitors outlined above is in addition to any Historic Rally forming part of the main event.

3.1.2.2. In the case of a 2 or 3 day event, where a class, as permitted above, is included, the following will apply:

Additional starters for the last 4 stages only: 40 are permitted.

Additional starters for complete final day if a Sunday Run is being run: 20 are permitted

Additional starters for the complete final day if a Sunday Run is not being run: 40 are permitted.

3.1.3. A Junior Rally is defined as a rally to cater for cars up to 1650cc (including Super 1600 and Kit-Car variant 1600), where the driver is under 26 years of age on the 1st January of the year in which the event takes place.

3.1.4. Organisers availing of the procedure at Art. 3.1.2. above may do so on the strict understanding that no more than 131 or 151 competition cars, as applicable, , may start the main event. All competition cars must have a time recorded at all controls. The penalty for non-compliance is exclusion. [Appendix 29, Art. 9.2.2]

3.1.5. The promoters cannot give approval to competitors to miss controls, unless turned around, on alternative roadbook, in the event of a stage cancellation.

3.1.6. Where it is obvious that all Competitors will not be able to traverse a Stage (allowing for the Stage maximum time as published plus 15 minutes maximum lateness) before the expiry time of the Road Closing Order the Stage should not be started and the Stage Interruption Rule cannot be applied.

3.1.7. No cars that were homologated under Group B regulations (except those specified in Appendix 29) may take part in competitive events run under MI permits. The regulations allowing such vehicles to take part in non-competitive demonstration runs are set out in Appendix 25 Chapter 5.

Note: Organisers may not permit competitors travel to the start of a stage in the hope that the number of starters would not reach the maximum allowed is prohibited.

3.2 DRIVERS BRIEFING

A Competitors Safety Briefing is compulsory for all events. The briefing must highlight safety requirements particularly the procedure dealing with SOS & OK boards. A two minute penalty may be applied if at least one member of each crew has not attended this briefing.

3.3 SWEEPER CARS/S.O.S. BOARDS.

3.3.1. The present use of Zero Cars must continue and all Zero Cars must be in radio contact with Base Control. There should be minimum delay between last Zero Car and first Competitor.

The use of S.O.S. Boards must be enforced for crews in competing cars who need to summon assistance. Clubs may also provide some stage marshals at selected points, such as spectator points, with S.O.S. Boards which must ONLY be used to summon assistance in case of an emergency with spectators, competitors or residents. No Car should be allowed to start the stage until instructed by the Stage Commander on receiving clearance from the COC in consultation with the MI Safety Officer.

3.3.2 After the passage of the last Competitor a car (Car accountability) should travel the stage and account for cars who failed to appear at the finish. This procedure to be followed whether the road is to be opened between runs or not and applies to both Tarmac and Forestry Stages. After the last running of a stage the Emergency Services must follow car accountability through the stage. ***The occupants of this vehicle must fully understand their role and duties, the responsibility for this is under the remit of the Clerk of the Course.***

3.3.3. A sweeper car (NOT a rally car) should traverse the stage between the last competitor and Car Accountability. ***The function of this car***



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is to act as a link between Rally Control and the Emergency Services, in the event of the last competing car requiring assistance. This car can have no other function. The occupants of this vehicle must fully understand their role and duties, the responsibility for this is under the remit of the Clerk of the Course.

3.3.4. Certain Officials may also be included in the Irish Rally Drivers Scheme Insurance. See Appendix 28.

3.4. SPECTATOR CONTROL.

A loud speaker car must traverse each Stage after the road has been closed to provide information to Spectators and to assist in crowd control. The Siren must only be used on the approach to junctions. The Spectator Control car should travel in tandem with Safety Officer.

3.5. STAGE MARKING.

Advance arrow, yellow 100 metres before junction. Double red arrow's at junction.

Hazards should be marked with a Yellow advance warning board 100 metres prior to the hazard and red warning board at hazard.

Secondary hazards (i.e. not warranting a full caution), may be highlighted by use of a 'Think' board, positioned 100m before the hazard, (no advance 'Think' board).

Count down boards must precede a chicane. A fire extinguisher is also required at a chicane.

All road junctions and cautions etc. to be numbered.

Stage entry, start, finish etc. to be named not numbered.

Numbering to commence at the first location after the stage start.

All numbered locations to be marshalled.

Minor "straight ons" should not be numbered. Numbers should be kept to a minimum compatible with safety.

Location numbers, size to be - A4 size card, Font -Arial, Font size/style - 500 Bold. The number should appear on the red arrow on the exit side of the junction. **Radio Boards**, indicating the location of Radio Points on stages must be used - yellow for the advance board and blue for the actual radio point.

3.5.1. Chicanes should in general be right hand entry, but if the location is best suited to a left hand entry then this is permitted - in any case, right or left hand entry should be clearly stated in the event road book and the layout of the chicane should be clearly marked on the road for pace note

making. The elements of the chicane should be a minimum of 10 metres apart and consideration should be given to the ability of an ambulance to negotiate all chicanes. Only the first element of a chicane should have a directional chevron.

3.6. OFFICIALS.

3.6.1. Press Sign On

3.6.1.1. All bona-fide members of the Press should be afforded the facility to sign-on. A separate sign-on sheet is provided for them. The event PRO is the person best suited to dealing with this on behalf of the organisers. He/she is responsible for establishing the bona-fide of the people who present themselves at press sign-on. Individually (eg. numbered) identifiable tabards should be given to the press. These must be an alternative colour to those in use by marshals on the event and must be worn so as to be visible and identifiable. Members of the press should not be afforded the facility of driving through the stages during the road-closing period. Special arrangements may be put in place to facilitate TV crews. This must be discussed with the event Safety Officer in advance.

3.6.1.2. Sign-on for Press to include a declaration /undertaking, not to stand in prohibited areas, box junctions etc., and to move immediately if so requested by a marshal / official. (Details of failure to comply to be reported to COC/MI). Only MI accredited Press may sign-on.

3.6.2. Official Sign-On

3.6.2.1. Organisers must ensure that all Officials, Marshals etc. have signed on at the start of the event, and that all entrants, drivers and passengers have signed an indemnity.

3.6.2.2. The following Officials must have a current Officials licence (GCR 123), details of which must be contained in the Safety Plan: C.O.C, D.C.O.C., MI Steward, MI Safety Officer, Scrutineers, Stage Commanders and Radio Controller.

3.6.2.3. Children up to the age of sixteen must not be carried in Official cars.

3.6.2.4. The Chief Medical Officer must be a doctor. It is essential that the Chief Medical Officer is aware that he/she is fulfilling a specified role concerning the overall safety of the event.

3.6.2.5. The Chief Medical Officer is responsible for the supervision of all medical resources, including doctors and ambulance personnel at an event. This supervisory role is exercised where appropriate in consultation with the COC.

3.6.2.6. Marshals must be identified by tabards and supplied with whistles.



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3.6.2.7. Officials in charge of Special Stages must not leave until they are quite certain that all competitors who start have been accounted for at the finish of the stage. Competitors who retire should report the fact to the organisers at the next Control.

3.7. HELICOPTERS

Helicopters are banned at all events except for emergency rescue use, and must then be under the control of the Clerk of the Course. It is accepted that it is difficult for organisers to prevent individual members of the public from using helicopters at an event. However if the organisers are satisfied that the users of the helicopter are closely associated with a competitor in the event then the organisers can insist that the helicopter leave the area.

3.8. STEWARDS.

The organiser shall provide the MI Stewards of the Meeting with a copy of the route card and marked maps (same scale as for event) showing all time points, for attachment to his report for submission to MI.

Organisers must permit MI Stewards to drive through any stage from its start, departing at least 15 minutes before the first Competitor.

The Organisers must provide radios on the Organisers frequency for MI Stewards and MI Safety Officer(s).

Clerk of the Course and/or Stewards of the Meeting are requested to **notify the Motor Sport Dept.**, MI of any complaint or inconvenience made by members of the public and of any significant accidents, particularly those involving third parties. Such reports should be submitted within 48 hours of the complaint, and should include the time and place and any other relevant details. Marshals at all Special Stages should be instructed to watch for acts of vandalism by spectators on landowners property and wherever possible to prevent it.

3.9. PARC FERMÉ.

After the final control all cars will enter a secure Parc Fermé. Competing cars must remain in Parc Fermé at the conclusion of the event for a period of 30 minutes after their recorded time at Parc Fermé In Control. Where competing cars have earlier been diverted to a Ceremonial Finish Control, the 30 minute period may run from the recorded time at that control. The area between the finish of the last stage and the entrance to Parc Fermé is also regarded as Parc Fermé and as such it must be treated in the same manner FIA General Prescriptions, Article 42 (b).

3.10. RESULTS.

Results must be posted on the Official Notice Board. The time and location will have been

specified in the Supplementary Regulations. The results will remain provisional for a period of 30 minutes. After the expiration of that period the results become final. After re-posting results, following Protests or Queries, Appeals, protests or Queries will only be accepted regarding original Protests or Queries. No Protests or no new, matters can be entertained.

3.11. RETIREMENTS.

Any car that retires from the Rally must advise the next course car that passes of their retirement and must **surrender their time cards**. The onus rests on the competitors to make sure that the organisers are aware of their retirement.

3.12. SPECTATORS.

3.12.1. No spectators will be permitted to stand on the road verge in any area of significant risk during a stage. They must all be positioned behind the road boundaries such as banks, walls, fences etc. or behind ropes or barriers erected by the club.

3.12.2. No spectators will be permitted to stand in the hazard areas defined in the Safety Plan, or similar positions which have FORBIDDEN GROUND notices displayed.

3.12.3. No Spectators can be permitted to enter a Stage from the START or FINISH after the Safety Officers' car has passed. Spectators should not be allowed to congest Start or Finish Areas.

3.12.4. Spectator Control by Clubs must benefit by not publicising advance details of the location of some stages, excluding those with poor access or hard to marshal but emphasising others which they can more easily control. All stages must be adequately marshalled, with additional emphasis on those at which most spectators are expected.

3.13. REGULATIONS FOR RECONNAISSANCE AND THE MAKING OF PACE NOTES.

3.13.1. The use of "pace notes" will be permitted on all Special Stage events except Forestry and Single Stage events, subject to the following regulations.

3.13.2. A club may decide to forbid the use of pace notes at any of its events.

3.13.3. Pace notes may be made only at these times.

3.13.3.1. Sunday 1 day events - on the day before (i.e. Saturday)

3.13.3.2. Saturday 1 day events - on 1 day of the previous weekend, the club to specify which day.

3.13.3.3. 2/3 day events - the 2 days prior to the event or the previous weekend only.



REQUIREMENTS FOR ALL RALLIES

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3.13.3.4. International Rallies: By prior arrangement with MI Rallies Committee. The times and dates must be agreed prior to the publication of the Supplementary Regulation for the event. The decision of the Rallies Committee is final and is binding on the organisers.

3.13.3.5. Organisers may request permission from the Rallies Committee in writing to vary this arrangement at (3.13.3.1.), (3.13.3.2.) and (3.13.3.3.) above. Such permission must be prior to the publication of Supplementary Regulations. The decision of the Rallies Committee is final and is binding on the organiser.

3.13.3.6. ALTERNATIVE RECONNAISSANCE

The Organisers may, in exceptional circumstances and after prior consultation with the MI Stewards appointed for the event, permit a competitor to carry out reconnaissance on different day(s) to those specified in above, subject to:

3.13.3.6.1. The supervision and conduct of such reconnaissance, by the Organisers and by the competitor(s) respectively, shall be in strict compliance with the event regulations.

3.13.3.6.2. Any competitor(s), to be eligible for such reconnaissance, must first submit a signed written request to the Organisers setting out in detail the precise reason(s) for the request. This provision will apply to both crew members.

3.13.3.6.3. The proposed participation by a competitor in an MI permitted rally that takes place on the nominated recce days for an International Rally is specifically excluded as a valid reason for alternative reconnaissance. A competitor granted permission to, and who does, recce on alternative day(s), and who also takes part in such an event, shall be deemed to have conducted illegal reconnaissance.

3.13.3.6.4. Alternative reconnaissance shall be in substitution only for that specified in the event regulations. Any extra reconnaissance by a competitor on the nominated recce days shall be deemed to be illegal reconnaissance.

3.13.3.6.5. The identity of any competitor(s) authorised to recce on alternative day(s), and the reasons for granting permission to recce, must be notified in writing to the MI Stewards appointed to the event, and to MI, before the reconnaissance takes place. It shall be the joint responsibility of the Organisers and the competitor(s) involved to ensure that this occurs. Failure to do so will render such reconnaissance illegal.

3.13.3.6.6. The identity of the competitor(s) authorised to recce on alternative day(s), and the date(s) for which they were so authorised, shall

be posted on the official notice board at Rally HQ no later than the opening time of Documentation Scrutiny.

3.13.3.6.7. The MI Stewards shall record all such information, including copies of the correspondence requesting and granting/refusing permission, in their report to MI.

3.13.3.7. Reconnaissance is limited to a maximum of three passes per stage or any part thereof.

3.13.4. Pace notes may only be made in daylight hours, with a maximum of 9 hours allowed, finishing at least 1 hour before sunset. The hours are to be specified by the organisers in their regulations. If a Special Stage is to be used in opposite directions, the Organisers will specify the times at which the stage may be reconnoitred in each direction.

3.13.5. On Single Stage events the use of "Safety Notes" is permitted. The "Safety Notes" cannot be made available to the competitors until 7am on the day of the event. The organisers must ensure that sufficient checks for illegal "Pace Notes" are carried out during the event. **Competitors will be allowed one reconnaissance run only prior to event to check safety notes.**

3.13.6. Competition licence holders who use the roads which are to be used as special stages for social, domestic or business use must **contact the Clerk of the Course** for official permission to use such roads.

On Forestry events the use of safety notes is permitted. A DVD which will be available for sale at documentation sign on, on the day before the event, can be viewed in conjunction with the safety notes for the event, and edits to the event safety notes may be made.

3.13.7. Clubs must appoint sufficient Judges of Fact on each special stage, and sufficient personnel at scrutiny and registration to ensure compliance with all the above regulations. The names of the Judges of Fact must be advised to competitors. Clerks of the Course are reminded that in accordance with GCR 131.8 they must convey to the Stewards of the Meeting any report which deals with misbehaviour of, or breach of rule by an entrant or driver or any person acting on his behalf.

3.13.8. Clubs must not, under any circumstances, provide, supply or be seen to endorse pace notes prepared by a third party. Clubs must obtain a letter of indemnity from any person supplying pace notes for use in the event, indemnifying the club and MI from any claim arising out of the use of said notes.



REQUIREMENTS FOR ALL RALLIES

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3.13.9. Clubs must inform all residents of the special stages in detail about the reconnaissance period.

3.13.10. The use or carrying of 'Pace Notes' when specifically forbidden or any other information concerning Special Stages, except official information, will be reported to MI. to consider imposition of penalties. Pace Notes are defined as anything in writing or material on video or audio tapes or discs which could be considered as giving the crew knowledge of the course. Annotations to the Road Book will be considered as 'Pace Notes'.

3.13.11. A DVD of the stages may be made available at reconnaissance sign-on for tarmac multi-stages and mini-stage rallies. Clubs must not, under any circumstances, provide, supply or be seen to endorse DVD's prepared by a third party. Clubs must obtain a Letter of Indemnity from any person supplying DVD's for use in the event, indemnifying the club and MI from any claim arising out of the use of the said DVD's.

3.14. RESCUE VEHICLES.

Use must be made of the rescue vehicles currently in operation. The following is a list of Motorsport Ireland approved Rescue Units, which can be booked for events by contacting .

1. Fergal Henehan,
Hill of Berries, Kiltoom, Athlone,
Co. Westmeath.
Phone - (0906) 44576
Mobile - (086) 8208054
Email - fergalhenehan@eircom.net
2. Athlone Rally Rescue
Hill of Berries, Kiltoom, Athlone,
Co. Westmeath.
Phone - (0906) 492474
3. Ivor Gleeson,
109 Monalea Manor
Knocknacarra, Galway.
Phone - (091) 591497
4. Motor Sport Rescue Services Ltd.,
11 Kinvara Avenue,
Navan Road, Dublin 7.
Phone - (01) 838 2745
5. Russell Rescue Club,
Tobermore House,
Kilmacthomas, Co. Waterford.
Phone - (051) 294426
Mobile: (087) 6950900
e-mail: prussell@waterfordcity.ie

6. Mitchell Henderson
7 Legaloy Road, Ballyclare,
Co. Antrim, BT3 99PS,
Northern Ireland
Phone - (048) 93341130
7. Martin Byrne
Killiney, Castlegregory, Co. Kerry
Phone - (066) 7139294
Mobile - (086) 2294063 or (087) 2824147
Email - southwestmotorsportrescue@gmail.com
8. Leinster Rescue
Arthur Walshe
8 Coalgreeney Road,
Arklow, Co. Wicklow
Mobile: (086) 0728532
Email: leinsterrescue@yahoo.co.uk

A List of Equipment for a rally rescue unit is set out in Appendix 108.

3.15. MEDICAL KITS.

Medical Kits are carried on the MI approved rescue vehicles. The cost for use of the medical kits per event are as follows:

- a) For Hillclimb and Single stage events €100
- b) For International, Multi-stage and Forestry events €300

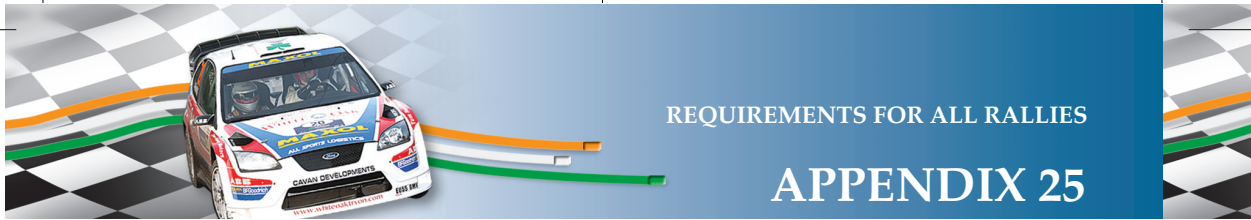
3.16. DEFINITIONS.

3.16.1. Single Stage Rally: an event held on closed public roads for rally type cars only. Cars may start at 30 second intervals and both driver and co-driver must be carried. The course must not exceed 16 Kilometres in length but all other requirements of a multi-stage rally must be adhered to.

3.16.2. Mini-Stages Rally: is an event held on closed public roads for rally type cars. It consists of up to two stage locations run twice for Mini-Stage 2 and three times for Mini-Stage 3.

3.16.3. Rally Sprint: as defined in GCR 25 (a) and Standard Regulations in Appendix 30, is an event held in private grounds or in a forest for rally type cars only. Starting intervals are subject to approval by MI Safety Committee. Only a driver and co-driver can be carried in the competing car.

3.16.4. Average Speed: the average speed calculated from the time allowed to competitors to cover a certain distance. In the case of roads open to the public this distance shall be determined from 1:50,000 Ordnance Survey maps along the route specified by the organisers, or if no route is specified, by the shortest route, and except in the case of Special Stage Rallies (GCR 22) shall not exceed 50 KPH.



3.16.5. Control or Check: any point that competitors must visit during an event. Officials will man a Control. A Check may or may not be manned. If the term "Time Control" or "Time Check" is used this will imply that the competitor's time of arrival at or departure from this point will be recorded.

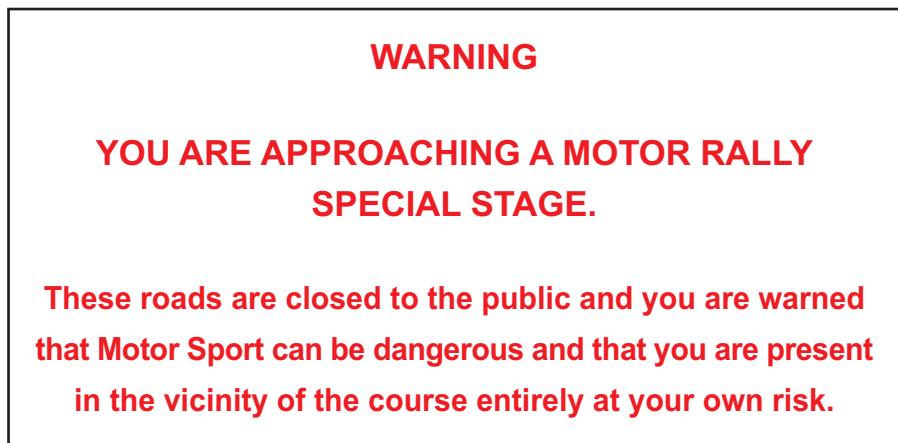
3.16.6. Road Book or Route Card: a document supplied to competitors containing appropriate instructions in order to complete the information it is necessary for competitors to have; such information as to the Controls and Checks which

they are to visit during the event, how they will establish that they have visited unmanned Checks, details of Tests, Stages etc. The term Route Card or Road Book is reserved for documents which also instruct competitors in detail as to how to proceed from one point to another and may or may not contain other information appropriate to a road book.

3.16.7.

Time Card: a document upon which a time of a competitor's arrival at, or departure from, a Control or Check is recorded.

SAMPLE WARNING NOTICE (Approx. 24" x 18") (Red on White)





REQUIREMENTS FOR ALL RALLIES

APPENDIX 25

5. STAGE RALLY DEMONSTRATIONS RUNS

5.1. Definition.

A stage Rally Demonstration Run is a display of car performance. It is not a competition, and timing is not permitted. The participating cars shall be confined to Historic Rally Cars and/or "Group B" rally cars, or replicas thereof. (Cars must be manufactured pre 31.12.1986)

5.2. Format

Organizers of MI permitted Stage Rally event may, at their discretion, incorporate a Demonstration Run over closed roads. The maximum number of cars to be included shall be 20. The maximum number of stages to be traversed shall be 5 per day. The stages being traversed will be agreed in advance and no other stages may be traversed. Demonstration Runs cannot be organised without prior authorisation from Motorsport Ireland.

5.3. Application.

Full details, including route, scheduled timing, nominated officials, etc. of the proposed Demonstration Run must be submitted to Motorsport Ireland for approval no later than 12 weeks prior to the date for the holding of the Demonstration Run. The Rallies Committee will consider each application on its merits and may impose conditions, or may refuse the application in its entirety on the grounds of safety, or other issues.

5.4. Organisation.

When approved, the details of the route and timing of the Demonstration Run must be stated in the Event Safety Plan and also the Supplementary Regulations for the event and must also be included in the official programme for the event.

The Demonstration Run must be accommodated in the event time schedule preceded and followed by an Official Car. Both official car's must contain suitably experienced officials under the radio control of the Clerk of the Course. A member of one of the official cars must conduct a briefing with the participants prior to each event.

Timing is forbidden and the cars must not be given a start count down. Overtaking is forbidden, except in the case of "force majeure". Cars must not bear any competition numbers except for cars that are historically associated with a particular number. Other means of identification of the cars may be used by the organisers eg. 'B1' or 'Demo1' on side panels. Such identification must be removed when the car leaves the location of the event.

5.5. Scrutiny.

All cars must pass a safety check at Mechanical Scrutiny per the following guideline "Demonstration vehicles traversing closed road sections of rallies will be subject to safety checking prior to each individual event. Checks will be of a general safety nature and will take into account the age and period of the vehicle and the requirements of that time. Checking can take place at main scrutiny or at an alternate venue to the main scrutiny. An event scrutiny sheet will be completed in respect of each vehicle and a record maintained."

Safety clothing and helmets of an FIA standard must be worn by both occupants and HANS devices are highly recommended but not compulsory.

5.6. Documentation.

Both occupants must possess a current competition licence of the following grade. Drivers must hold a minimum of National B (or ASN equivalent). Co Drivers must hold a Navigators licence.

Participants must complete an event registration form and sign the indemnity (as per sign-on form attached), and must produce a valid IRDS / BRDS letter and pay the full premium or any additional loading as outlined on letter.

Evidence of any driver who is observed driving in a dangerous manner, or is involved in an accident, will be reported to the Stewards of the event. Penalties may be imposed as per GCRs.

Only those cars who are passed at pre event scrutiny as those involved in the Demonstration Run may take part in it. Cars that retire from any part of the main rally are not permitted to join demonstration run.

The number of cars participating in the demonstration run along with their names must be made available to the Stewards of the event at Pre Event Stewards meeting.



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APPENDIX 25

EVENT NAME AND DATE

RALLY CAR DEMONSTRATION RUN SIGN ON

(a) I have read the supplementary regulations issued for this event and agree to be bound by them and by the General Competition Rules and Regulations of MI including the guidelines and regulations contained in Motorsport Ireland's Code of Conduct for Children's Sport. In consideration of the acceptance of this entry or of my being permitted to take part in this event I agree to save harmless and keep indemnified the (Organising Club), Irish Automobile Club Ltd. t/a Royal Irish Automobile Club, Irish Motorsport Federation Ltd. t/a Motorsport Ireland and their respective officials, servants, representatives and agents from and against all actions, claims, costs, expenses and demands in respect of death, injury, loss of or damage to the person or property of myself, my driver(s), passenger(s) or mechanic(s) (as the case may be) howsoever caused arising out of or in connection with this entry or my taking part in this event and notwithstanding that the same may have been contributed to or occasioned by the negligence of the said bodies, their officials, servants representatives or agents. Furthermore, in respect of any parts of this event on ground where Third Party Insurance is not required by law, this Agreement shall in addition to the parties named above extend to all and any other competitor(s) and their servants and agents and to all actions, claims, costs, expenses and demands in respect of loss of or damage to the person or property of myself, my driver(s), passenger(s) or mechanic(s).

My age (driver) is (If applicable, state "over 18 years").

My age (co-driver) is (If applicable, state "over 18 years").

(b) I declare that to the best of my belief the driver(s), passenger(s) possess the standard of competence necessary for an event of the type to which this entry relates and that the car entered is suitable and roadworthy for the event having regard to the course and the speeds which will be reached.

I declare that the use of the car hereby entered is covered by insurance as required by the Road Traffic Act, which is valid for such part of this event as shall take place on roads as defined in the Act.

(c) I understand that should I at the time of this event be suffering from any disability whether permanent, temporary or otherwise which is likely to affect prejudicially my normal control of my automobile, I may not take part unless I have declared such disability to MI, who have, following such declaration issued licence which permits me to do so.

(d) Any indemnity and/or declaration as prescribed by sub-paragraphs (a) and (b) above which is signed by a person under the age of 18 years shall be countersigned by that person's parent or guardian, whose full names and address shall be given.

First Driver:

Address:

Telephone No.:

Second Driver:

Address:

Telephone No.:

Details of Car:

Make: Model:

Registration Number: Colour:

Please Sign:

Frist Driver

Second Driver

Parent / Guardian (if applicable):

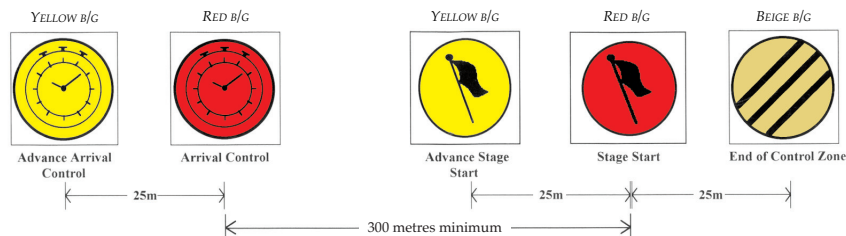


WARNING SIGNS

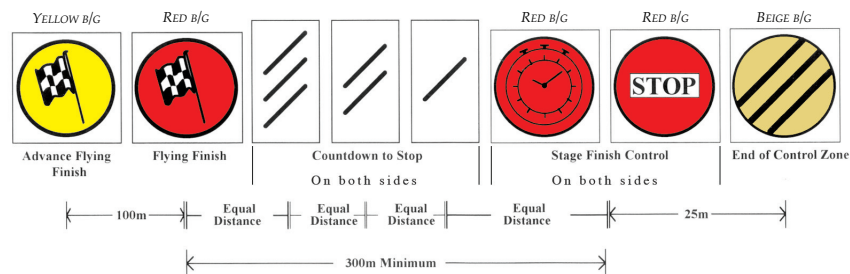
APPENDIX 26

WARNING SIGNS - Boards and Distances

Special Stage Start - Boards and Distances

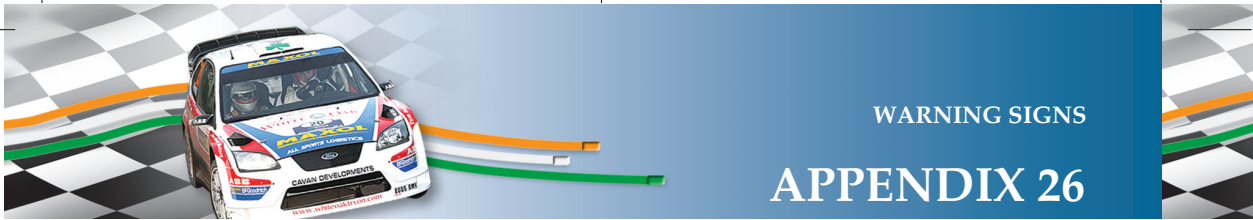


Special Stage Finish - Boards and Distances



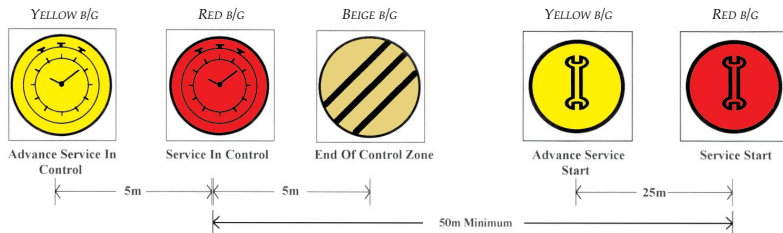
Stage Safety - Boards and Distances



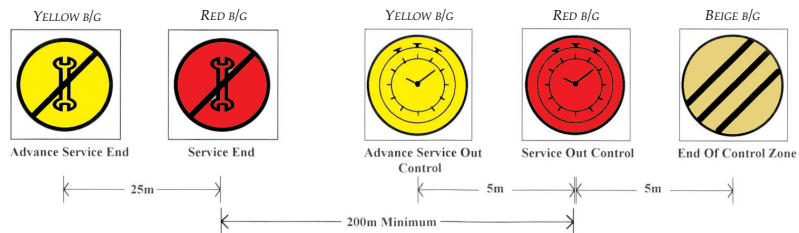


WARNING SIGNS - Boards and Distances

Service In - Boards and Distances



Service Out - Boards and Distances



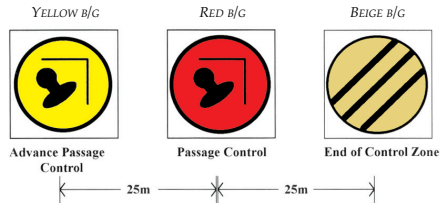


WARNING SIGNS

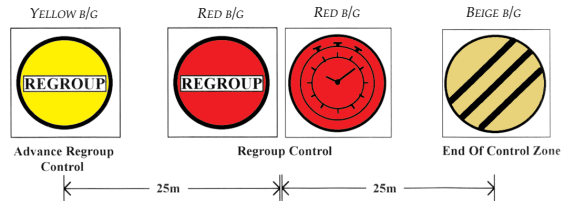
APPENDIX 26

WARNING SIGNS - Boards and Distances

Passage Control - Boards and Distances

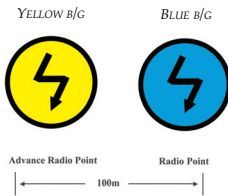


Regroup Control - Boards and Distances



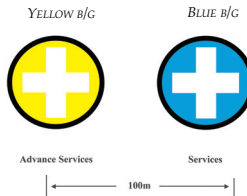
Radio Points

- Boards and Distances



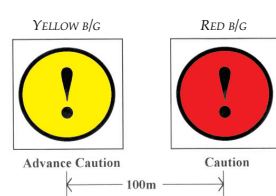
Services

- Boards and Distances

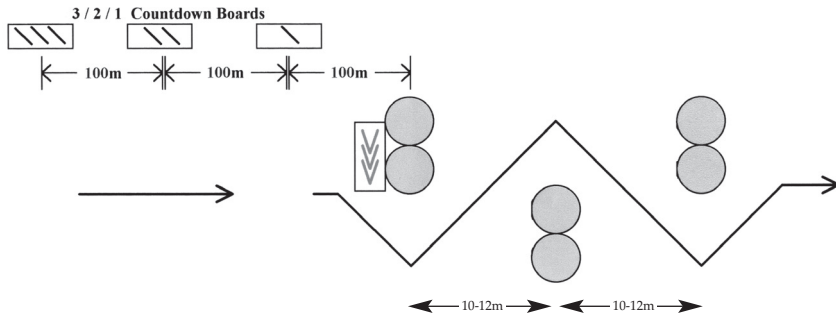


Hazards

- Boards and Distances



Chicanes - Boards and Distances





SAFER SPECTATING

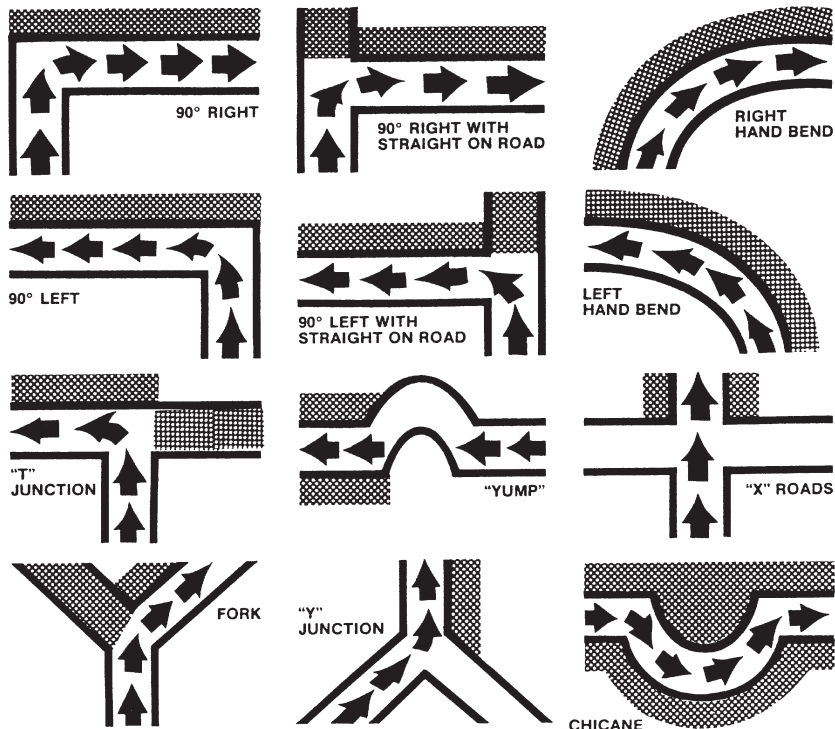
APPENDIX 27

GUIDE TO SAFER SPECTATING

To Be Included In All Event Programmes

DO NOT - BLOCK ESCAPE ROAD
 STAND ON THE OUTSIDE OF BENDS
 OBSCURE STAGE ARROWS OR SIGNS

WHEN SPECTATING THERE IS NOWHERE THAT IS TOTALLY SAFE BUT THE DIAGRAMS BELOW SHOW THE MOST DANGEROUS AREAS SHADED



**REMEMBER - MOTORSPORT CAN BE DANGEROUS
 AND YOU SPECTATE AT YOUR OWN RISK
 PLEASE OBEY THE MARSHALS AT ALL TIMES -
 IF THEY ASK YOU TO MOVE IT IS FOR YOUR SAFETY**



IRDS

APPENDIX 28

IRISH RALLY DRIVERS' SCHEME (IRDS) MOTOR INSURANCE FOR THE RALLY DRIVER

Validity of Cover:

The cover will apply to any Driver, who must hold a valid Competition Licence, and who can PRODUCE to the Organising Club a "Letter of Acceptance" issued by Marsh Ltd., (Brokers) identified by the prefix "IRDS" followed by a number, and who has paid the required premium to the Organising Club before the starting time of the Rally / Navigation Trial.

Cover will commence as the Competitor leaves the "Start" control, and will cease on arrival at "Finish" control, or at the point where he/she retires from the Event.

In this context the Scrutineering Site will constitute the "Start" Control, given that in circumstances where scrutineering takes place on the evening previous to an Event, competing cars will be retained in Parc Ferme until the Event commences.

In other words, cover only exists while travelling the official route (i.e. the Road Book) during the course of the competition and while under the control of the event organisers.

Scope of Cover:

Third Party Only, including Legal Liability to Passengers.

Premiums:

- (i) Navigation Trials
€ 20 per Driver per event.
- (ii) Special Stage Rallies
€ 20 per Driver per event. (including Government Levy)

Note: Young Drivers under 25, or Drivers with a record of accidents or convictions, may have special terms applied. These will be noted on the "Letter of Acceptance".

Premium Loadings for age are as follows based on the age of the applicant on January 1st of the year in question:

- 17 -20 inclusive: +75%
- 21 - 24 inclusive: +50%

Letter holders should check their letters on receipt to ensure that the correct premium is shown. It must also be borne in mind that driving convictions and claims history can result in loadings.

METHOD OF OPERATION:

1. PROPOSAL FORMS.

Are available to Competitors from Motorsport Ireland, and these should be returned to Dawson Street, completed in ALL respects. Motorsport

Ireland will forward these to the Brokers. Under no circumstances can the Brokers issue Proposal Forms direct to applicants, nor receive these direct. Proposal forms must be received by the Brokers a **minimum of 14 days** before cover is required, in order that "Letters of Acceptance" may be issued by the Brokers direct to applicants, and confirmation passed to Motorsport Ireland.

Before passing Proposal Forms on to the Brokers, Motorsport Ireland must check - 'Blanks' and 'Dashes' as answers to questions as these are NOT acceptable, and will result in disappoinment.

1.1. MSA Licence Holders can avail of this scheme if competing in MI permitted evens and upon completion of the above proposal form will be issued with a BRDS (British Rally Drivers Scheme) "Letter of Acceptance".

There is an annual charge of €20 per letter for BRDS applicants to cover administration costs and this should be paid by credit card, along with the proposal form each year. Application forms are available from Motorsport Ireland.

2. ORGANISING CLUBS.

Must apply to Motorsport Ireland, on the prescribed Form for the issue of a Blanket Certificate. This form is printed on the reverse of the Permit application, and must have with it a **post-dated cheque, made payable to Motorsport Ireland.**

The Certificate Application Form **MUST** be forwarded to the Brokers by Motorsport Ireland, to arrive a **minimum of 14 days** before the Event date. The Blanket Certificate will be sent to the Club by the Insurers, addressed to the Official handling the Scheme.

3. DOCUMENTATION.

3.1. At Documentation the organising Club will collect from the Competitor the appropriate premium, as indicated above. Any variation from these standard rates will be detailed on the "Letter of Acceptance".

3.2. The Declaration Form will be completed by the Club in respect of the details of, and signed by, each Driver at the time of payment of premium. It must then be forwarded by the organising club **within 7 days**, to Motorsport Ireland.

Motorsport Ireland will then immediately forward a copy of the Declaration Form to the Brokers.



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3.3. While the organising Club may accept premium from any Driver, who can produce a "Letter of Acceptance", at any time up to the Start of the Event, it should be especially noted that the Club should in no circumstances confirm cover or accept premium from a Driver unless:

3.3.1. The Club holds a valid R.T.A. Certificate.

3.3.2. The Driver has actually produced a current "Letter of Acceptance" bearing the IRDS number.

3.4. Any Navigator, or co-driver, who wishes to share the driving during an Event must hold and produce, his own "Letter of Acceptance" and in such cases, the Co-driver must also pay 50% of the appropriate premium.

3.5. Zero cars must be included in the Irish Rally Drivers Scheme. Insurance and premiums will be charged at 50% of the competitors rate. Drivers of zero cars must also be holders of a suitable current competition licence, medically approved, and the car which they intend to use must pass the same safety and mechanical scrutiny which all competition cars are subject to prior to taking part in any event. Navigators / Co-Drivers must also possess a current valid competition licence.

3.6. Third party Motor Insurance within this scheme is available, on a voluntary basis, to those officials on Special Stage Rallies who, as part of their duties, are listed on the event time schedule and drive the same route as competitors. "Letters of Acceptance" must be obtained in the usual way, and premiums will be charged at 50% of the competitors rate.

3.7. The organising Club must appoint a responsible Official, to take sole control of the handling of the Scheme. This official should take particular care in examining "Letters of Acceptance" produced by Drivers, since those will contain details of any variation from standard rates of premium and terms. Declaration Forms will be checked retrospectively on these points.

IMPORTANT 4. PLEASE NOTE THE FOLLOWING ESSENTIAL REQUIREMENTS:

4.1. The Club MUST hold a valid RTA Certificate for the Event.

4.2. The Club MUST NOT accept a premium unless a Driver can actually produce a "Letter of Acceptance".

4.3. Should a Club wrongly accept a premium from a Driver who cannot PRODUCE a "Letter of Acceptance", NO COVER IS OPERATIVE, and the Driver will face possible prosecution by the Gardai. It should also be noted that this applies to any Club Official who amends, alters or retrospectively accepts cover.

4.4. All new Proposals will be checked by Motorsport Ireland for missing details, since incomplete Forms CANNOT be accepted.

4.5. The Brokers cannot correspond, nor deal, direct with Drivers except in the following circumstances:

4.5.1. The issue to Drivers of the "Letters of Acceptance"

4.5.2. Notification to the Brokers of changes of Address

4.5.3. Notification to Brokers of Accidents which have occurred

All other correspondence should be directed through Motorsport Ireland.



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REGULATIONS FOR ALL STAGE RALLIES

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The information in SECTION 1 of this Appendix 29 is the minimum that must be included in the Supplementary Regulations which are provided by the Promoters to all entrants in the competition. The Standard Regulations in SECTION 2 of Appendix 29 may be reproduced in part or in their entirety.

Amendments to the Standard Regulations including the proposed Classes of Cars permitted in the competition are allowed. Motorsport Ireland (MI) must approve such amendments prior to the publication of the Regulations. Any proposed amendments must be highlighted when the draft regulations are submitted for approval. Amendments that are not approved in this manner will result in the amendment not having effect for the competition and cannot be relied upon by the Promoters. All amendments to the Standard Regulations must be published and made available to all entrants prior to the promoters accepting any entries.

SECTION 1 Supplementary Regulations

RALLY HEADQUARTERS.

Rally Headquarters will be at:

ORGANISATION.

1. The Rally is promoted and organised by the MOTORCLUB LTD., hereinafter called the Promoters.

2. The Rally is a qualifying round of the following Championships:

- a)
- b)
- c)

and will take place over a route of approximately kilometres.

3. These Regulations have been submitted to and approved by MI, and the necessary Permit for holding of the Competition has been granted.

MI PERMIT NO.

The Competition will be held under the International Sporting Code of the FIA, the General Competition Rules of MI and these Supplementary Regulations including Appendix 25 (Rally Code) and Appendix 29 (Standard Regulations for all Stage Rallies) of the current Motorsport Ireland Yearbook. All Competitors who forward completed Entry Forms agree to be bound by the rules.

EVENT and/or ENTRIES SECRETARY.

Include Name, Address and contact numbers.

OFFICIALS

Motorsport Ireland Stewards, Motorsport Ireland Safety Officer, Motorsport Ireland Chief Scrutineer, Club Appointed Steward, Clerk of the course, Deputy and/or Assistant Clerks of the Course. Secretary of the Event, Press Officer, Chief Marshal, Chief Medical Officer, Chief Rescue Officer, Radio Controller, Competitors' Liaison Officer.

CLASSES.

The event will include the following classes:

- Class 1. Group N up to 1400 cc.
- Class 2. Group N 1401cc to 1600 cc.
- Class 3. Group N 1601 cc to 2000 cc.
- Class 4. Group N over 2000 cc.
- Class 5. Group A up to 1400 cc.
- Class 6. Group A 1401 cc to 1600 cc.
- Class 7. Group A 1601 cc to 2000 cc.
- Class 8. Group A over 2000 cc.
- Class 9. Modified cars up to 1450cc - 2 wheel drive, normally aspirated.
- Class 10. Modified cars 1451cc to 1650cc not more than 2 valves per cylinder, 2 wheel drive, normally aspirated.
- Class 11. Modified cars 1451cc to 1650cc more than 2 valves per cylinder, 2 wheel drive, normally aspirated.
- Class 12. Modified cars 1651cc to 2100cc not more than 2 valves per cylinder, 2 wheel drive, normally aspirated.
- Class 13. Modified cars 1651cc to 2100cc more than 2 valves per cylinder, 2 wheel drive, normally aspirated.
- Class 14. Modified cars 2101cc to 3500cc 2 wheel drive, normally aspirated or **forced induction** - see note 2.
- Class 15. Four-wheel drive cars whose homologation has expired and other specified cars. - see notes 3,4,5.

NOTES ON CLASSES.

1. It should be clearly understood that if the following text does not clearly specify that you can do it, you should work on the principle that you cannot. The interpretation of any matters concerning the above classes or these notes on classes is a matter for MI. Queries should be submitted in writing to MI.

2. Cars in Class 14 may only be fitted with a **forced induction** if the car was originally homologated with one fitted in two-wheel drive format.



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3. Cars in Class 15 must comply with their last published homologation papers, FIA appendix J. Further modifications may be granted by application to TAG via MI/Rallies Committee and such permitted modifications will be listed with the vehicle logbook. The modifications set out below in articles 3.1 to 14 are also permitted.

3.1. Metro 6R4s as per Group B-2777 homologation papers and the following: Maximum 2800cc, with 65mm plenum restrictor or 2500cc with trumpeted induction and must run with MSA certified engine only. The engine certificate must be presented to each event scrutineer.

3.2. Ford Escort RS Cosworth, homologation No. A5466 and Ford Sierra Cosworth 4x4 homologation number A5414, Subaru Legacy A/N 5399 and Subaru Impreza A/N 5480 may enter class 15 if fitted with non-homologated gearbox. No other modifications are permitted and the car must comply with the homologation papers.

3.3. Subaru Legacy Homologation Nos. A5399 and N5399 may enter for Class 15 if fitted with a Turbo charger fitted normally to Subaru Impreza and complying with Homologation No. A5480 or N5480.

4. Additional cars authorised at the discretion of MI in advance may also be allowed to enter the class but must not exceed 3500cc.

5. Homologation papers will be required for all cars in Groups N and A and relevant cars in class 15. If these papers are not produced at scrutiny the car shall be refused a start. Unless there is an indication to the contrary on the homologation form excluding certain evolutions, Group A and Group N cars **plus any Group A and Group N cars whose FIA permission to run expired after 31.12.07, which are eligible for an additional 4 years.**

These cars may take the start and be classified on an equal footing with the currently homologated cars. Such non-homologated cars are as defined above may only be accepted for participation if the duly completed, though expired, homologation papers are produced at documentation and at scrutineering and the cars remain in total conformity with the original technical specifications and are in a sound condition to participate at the discretion of the scrutineers. The size of turbo restrictors used on these cars and the minimum weight must be those currently valid. **This also applies to National Permit Events.**

6. Cars in classes 9-14 may be modified to an extent not permitted for cars homologated in groups N and A with the following exceptions. Motorcycle / motorcycle derived engines are not permitted. Bodysell must be of a monocoque

construction i.e. no space frame types allowed. Furthermore cars in these classes with logbooks issued from 01.01.04 and any existing logged cars applying for a change of engine size/type must comply with the following in addition to complying with notes 2, 7, 8, 9.

ENGINE

Engine to remain in original location.

Engine cc may not exceed that of the vehicle model as supplied by the manufacturer as original equipment (or homologated by FIA for the vehicle) by more than 25%, to an absolute maximum of 3500 cc.

Engine block must be of a type fitted by any manufacturer as original equipment in a passenger car, or an engine block derived therefrom. Motor cycle/motorcycle derived engines are not permitted.

DRIVETRAIN

Driving wheels to remain as manufacturer, e.g. front wheel drive to remain front wheel drive, rear wheel drive to remain rear wheel drive. It is permitted to cut out a part of the bulkhead situated in the engine compartment for the fitting of engine and transmission parts including the position and hole for the passage of the gearbox control in the bulkhead and transmission tunnel. However, such cut-outs must be strictly limited to those parts necessary for this installation (see drawing Appendix J 279-1 and 279-2). Chain drive not permitted.

Cars which are "professionally" converted from front wheel drive (FWD) to rear wheel drive (RWD), **in accordance with the guidelines in notes on classes section 15 appendix 29 may apply (using the form available on the MI website) for permission to run in their appropriate "cc" class, once approved MI will issue a permission to run letter giving the car details which cannot be altered unless approved by MI and new letter issued.**

BODYSHELL.

The bodysell must be of a monocoque construction as per the manufacturers original specification (or homologated by FIA for the vehicle.) i.e. no space frames allowed. For the purposes of these regulations bodysell comprises rolling shell / car frame complete, including front and rear wings / rear quarter panels*, together with outer door panels and door frames. Bonnet and boot lid may be fibreglass / composite but must have internals/bracing similar to original bonnet/boot lid and must be fastened by way of four bonnet pins (two front and two rear), or by original type hinges and two bonnet pins.

If door trims are removed they must be replaced with sheeting made from metal at least 0.5 mm thick, or from carbon fibre at least 1 mm thick or from another solid and non-combustible material at least 2 mm thick.



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*(Front wings and rear wings / rear quarter panels may be modified solely for the purposes of fitting wheel arches.)

Bonnet vents/scoops are permitted provided they are homologated by any manufacturer or were/are fitted as standard by any volume motor vehicle manufacturer on type approved.

The requirements shown above for bonnets and boot lids will also apply to cars which have logbooks already issued before 31.12.03.

7. To determine the cubic capacity of **forced induction** cars a multiplication factor of 1.7 will be used **for Petrol and 1.5 for Diesel**.

8. To determine the cubic capacity of a rotary engine a multiplication factor of 1.5 will be used and the car will run in either Class 11 or 13 (more than 2 valves per cylinder).

9. All **force induction** cars must be fitted with an FIA specification turbo intake restrictor as Appendix 2 Art. 28.10.

10. A car may only be entered for one class.

11. The responsibility rests with the competitor to ensure that the car is entered in the correct class.

12. WRC Cars.

In the context of the location of the steering, cars manufactured after 01.01.05 must comply with FIA Appendix J, Article 7.3 in all MI permitted events.

13. National rallies run in conjunction with International events may include one class for homologated cars whose drivers do not hold an International competition licence. These drivers must hold a National A or National B licence, as appropriate to the engine size of the car. WRC cars are not eligible for this class.

14. A list of cars homologated by the FIA and cars with permission to run, granted by the FIA are available on MI website.

15. Front to rearwheel drive conversion guidelines.

15.1. Engine capacity/type to be 2500cc, 16V maximum (i.e. eligible for Classes 9-14). Engine capacity may not exceed that of the vehicle model supplied by the manufacturer as original equipment (or FIA homologated for the vehicle), by more than 25%.

15.2. Alterations from the bulkhead rearwards being limited to those detailed in FIA Appendix J, Article 279.3.2.2 and shown in FIA Appendix J drawings 279-1 and 279-2.

15.3. Engine to remain forward of bulkhead / windscreen line.

15.4. All materials added must be ferrous and must also be welded to the chassis / bodywork / unitary construction.

15.5. Standard roof skin must be retained, but a roof vent may be fitted.

15.6. No titanium, magnesium or alloy suspension parts allowed, unless standard or previously homologated on donor car.

15.7. Doors and windscreens must remain in the manufacturer's original material.

In all other respects, car to comply with MI Appendix 2 & Appendix 29.

15.8. Intending competitors are reminded that if they are undertaking a project like this it would be advisable to talk to an MI Technical Advisory Group member before and during the build to avoid any misunderstandings. The bodyshell should be inspected by an MI Scrutineer when alterations are completed and before painting to avoid any problems which could prove costly to rectify in a completed car.

TIMETABLE

(Here list dates and times as appropriate, opposite each entry)

Publication of Regulations:

Entries open:

Entries close of N.I. & U.K. competitors.:

Entries close at reduced rate.:

Entries close:

Final Instructions posted not later than:

Pace Notes (if allowed) may be made during the following period(s):

Scrutiny:

Drivers Briefing (Time and Location):

First Car Away:

First Car Finishes:

Full Provisional Results posted:

Prizegiving.:

ENTRIES

Closing date:

Amount: €

Entries should be sent to:

Telephone entries WILL NOT be accepted, nor will entries received after

The number of Starters is restricted 131 or 151. If the number of entries received exceeds this, the following procedure will be used to determine the starters

(State the procedure to be used)

The remaining entries will be placed on a reserve list.



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AWARDS - suggested**Overall Awards**

1st Overall	2 trophies & Perpetual Trophy
2nd Overall	2 trophies
3rd Overall	2 trophies

Class Awards

1st in Class	2 trophies
2nd in Class	2 trophies
3rd in Class	2 trophies

Crews who finish, but do not qualify for the above awards, may receive special finishers awards. If less than 5 entries are received in any Class, the number of awards may be reduced.

No monetary awards will be presented.

In order to promote its use, organisers are requested to present a Biofuel award.

SUNDAY RUN

A Sunday Run will (will not) be allowed.

**SECTION 2
Standard Regulations**

The promoters of a Competition may reproduce all or parts of this Section.

ARTICLE 1. JURISDICTION.

1.1. The Promoters reserve the right to introduce in these Regulations any modifications directed by major or safety reasons and to postpone the event in case of unforeseen circumstances.

1.2. The Promoters decline liability in any accident caused to or by a Competitor or Competitors. The Promoters also decline any liability for Breach of Laws, Regulations, Ordinances or Codes in Force, the consequences of which are the entire responsibility of the Competitors.

1.3. The Promoters reserve the right to issue mandatory written instructions, Road Books and time cards for the conduct of the competition. The interpretation of these Regulations and of any other such written notifications to Competitors shall rest entirely with the Stewards of the Meeting subject to the General Competition Rules of MI.

1.4. The event will have an Official Notice Board. It will be located in Rally Headquarters or if not located at Rally Headquarters then the location will be specified in the Supplementary Regulations or the Final Instructions which will be sent by post to each entrant or driver.

1.5. Official Instructions, including amendments to previous instructions must be displayed on the Official Notice Board or at some other place which shall be designated in the final instructions. Every competitor shall be deemed to

have read all such instructions, and if applicable such instructions shall have the force of these Regulations and shall be binding on all Competitors.

1.6. The onus is on a Competitor to keep informed of any additional Instructions.

ARTICLE 2 ENTRIES & STARTERS

2.1. Entries may be submitted by all persons holding a valid F.I.A. Competition Licence or Commercial Entrants Licence but will be valid for acceptance only if made on the Official Entry Form, properly completed, signed by the entrant or driver and navigator, and accompanied by the appropriate fee.

2.2. The Licence No. for Entrants, Drivers and Navigators must be included on the entry form.

2.3. The Promoters reserve the right to cancel the event if insufficient entries are received.

2.4. False statements made on the Official Entry Form relating to the Entrant, Driver or Navigator or in respect of the vehicle's characteristics imply the immediate exclusion of the participants, independently of any additional penalties that may be applied in accordance with the GCR 69.

2.5. Entries open on the publication of the Supplementary Regulations or on a date specified in the regulations and should be forwarded, to the Secretary of the Event or to another person as included in the Supplementary Regulations.

2.6. Single Stage and Mini Stage Rallies.

Thirty (30) places will be reserved on Single Stage and Mini Stage events in **2012** for "first time" National B Driver Licence Holder with licences issued in **2011/12**.

A further fifty (50) places will be reserved on Single Stage and Mini Stage events for National B Driver licence holders not catered for above.

Cars in classes 4, 8, 14 and 15 are not eligible to compete on Restricted Mini Stage Rallies.

Currently homologated WRC cars are not permitted on any Single Stage or Mini-Stage Rally. WRC cars which are out of homologation may compete on Non-Restricted Mini-Stage Rallies, in Class 15.

2.6.1. Supplementary Regulations.

Entry Fees may only be refunded if:

2.6.1.1. The Entry is not accepted.

2.6.1.2. The Competition is cancelled or postponed for more than 24 hours.

2.7. The Promoters may refuse an Entry without having to disclose a reason for this refusal, in accordance with the GCR 70.



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2.8. Drivers must have in force a Road Traffic Insurance, i.e. IRDS/BRDS, which is available through MI covering them against third party liability on the road sections, whilst taking part in the event.

2.9. Entry Fees (less a deduction of amount to be specified) will be refunded to drivers/entrants, who withdraw their entry, in writing, before a stipulated date.

2.10. The Stewards of the Meeting may permit a change of nominated driver on written application being made by the entrant who is the holder of a Commercial Entrants Licence in writing, a minimum of two (2) hrs prior to the scheduled starting time for drivers briefing.

2.11. A change of driver cannot be requested by anyone other than an 'entrant', provided the entrant is the holder of a current Entrants Licence and the application to take part in the meeting was by the entrant.

2.12. The number of starters is limited to 130 (or 150) plus one as allowed for in Appendix 16.

ARTICLE 3. COMPETITORS AND DRIVERS.

3.1. Two Competitors shall be nominated for and travel in each competing car. If the entrant is not the driver, the driver nominated on the Entry Form shall be deemed to be his agent. The driver shall hold a valid Competition Licence of the appropriate grade and these must be produced at Documentation Scrutiny. The nominated driver must drive all special stages of the event. Navigators must have either a current Competition Licence or a special Navigator's Licence, as per MI Rules of Entry.

3.2. A car may only carry the nominated competitors, whose names are stated on the Entry Form, under pain of Exclusion from the Finishers List, and such persons shall be carried throughout the event. Competitors are required to display their names, national flags and blood groups on the front wing or door of the car.

3.3. During the period allowed for the making of Pace Notes and throughout the rally, competitors, their service crews and others assisting them must strictly observe the traffic laws of the Republic of Ireland. They shall always drive and conduct themselves in a manner, which shall not discredit the event or arouse adverse public opinion. Failure to do so, or any instance of a competitor or others associated with the competitor driving at an excessive speed, or in a negligent manner, or failure to comply with local or national traffic regulations and laws, or being accused of any driving offence will result in the competitor being penalised as follows:

First Offence	2 mins
Second Offence	Exclusion

Judges of the Fact will report to the Clerk of Course all competitors, which are noted as having failed to stop at "Yield Right of Way" and/or "Stop" signs on all road sections. Penalties will be imposed as follows:

First Offence	2 mins
Second Offence	Exclusion

3.4. Competitors are required to inform the Organisers each day of any alleged offence or contravention, which is to be the subject of a Garda report. Failing to do this will be penalised as follows:

First Occasion	2 mins
Second Occasion	Exclusion

3.4.1. In the case of a breach of the traffic laws, officials, having noted the infringement, will inform the offending competitor.

3.4.2. The Organisers will also apply penalties as in 3.4 above if asked to do so by the Gardai.

This will be done subject to the following:

3.4.2.1. That the notification of the infringement reaches the Organisers through official channels, and in writing, before the posting of the official final results.

3.4.2.2. That the information in the notification will establish beyond any doubt the identity of the offending competitor, the location and time of the infringement. That the facts are not liable to various interpretations.

3.4.2.3. In such circumstances the written report will be sufficient to allow both the Organisers in the first instance to decide on the penalty to impose if any and subsequently to allow the Stewards of the Meeting to hear an Appeal. The appellant may, if he/she so wishes, call the reporting Garda as a witness. The responsibility for the attendance of the witnesses is a matter for the appellant. The non-attendance of the Garda witness will not prevent the Stewards from hearing the appeal.

3.5.1. In the event of:

3.5.1.1. A competitor who crashes on a Familiarisation Run.

3.5.1.2. A competitor failing to carry or to display the OK sign/board in a clearly visible manner on the car and who fails to ensure that the sign/board remains in position until the car is removed from the stage (At least one member of the crew must remain with the car for the remainder of the stage to ensure that the OK sign/board continues to be displayed).



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3.5.1.3. A competitor who fails to stop and investigate when encountering, on a Special Stage, another competing car, on which an OK sign/board is NOT displayed.

3.5.1.4. A competitor who is reported by an Official/Judge of Fact or a written report being submitted by a Garda for bad driving standards on the public road – Penalties as per GCR 139.

The COC or Steward may impose a penalty of exclusion. In addition, the Steward may report the incident to MI and recommend a start be refused on MI Permitted events for a period of 30 days.

3.5.2. Competitors who stop or are slowed as a result of the displaying of an SOS board, or the non displaying of an OK board, may have the stage interruption rule applied as per Appendix 29, Article 8.13 /8.14.

COC's are encouraged to fully investigate such incidents, and having established the facts, to act accordingly.

3.6. Prior to start of stages clubs should appoint an authorised official to inspect the readiness of competitors safety equipment (helmet fastening, balaclava, belts and Head Restraint System device, gloves, suits, fire extinguisher etc.). ***This official should not be part of emergency or rescue crews. This official should not attempt to adjust or interfere in any way with the equipment but should request that the crew do so in his/her presence. This checks must be carried out at random and at a reasonable distance (6 to 7 cars) back from the stage start line to ensure that stage start procedures are not compromised. CoC's must make competitors aware of this procedure at drivers briefing and should emphasise strongly to competitors the need for safety equipment to be used/adjusted correctly.***

3.6.1. The Stage Commander, in consultation with the CoC, may refuse entry to the stage to competitors not in compliance with all safety requirements.

3.7. As per Appendix 2, Article 29, Competitors must carry an SOS/OK board. The SOS board to be displayed when it is necessary to summon medical help in the shortest possible time. In the event of an incident where someone requires urgent medical help, display SOS board to next 8/10 competitors – they will carry the message to the next Radio Point where the medical services can be alerted.

The OK board is to be displayed by competitors stopped on a stage for any reason (off, broken down, etc.) and is used to indicate that medical help is not needed.

If no OK board is displayed, the following competitors must act as if an SOS board were being displayed. They must stop and establish what are the circumstances of the crew in question and act accordingly (i.e. report to the next radio point if medical assistance is required, or simply continue if not).

ARTICLE 4. RECONNAISSANCE REGULATIONS FOR RECONNAISSANCE AND THE MAKING OF PACE NOTES

4.1. Pace notes can only be made at the times and days specified in the Supplementary Regulations.

4.2. Before commencing reconnaissance competitors must register with the organisers.

4.2.1. Competitors must sign on at the start of (each) day.

4.2.2. Competitors must declare the make, model and registration number of the car that they will be using for reconnaissance.

4.2.3. Competitors must carry identification at all times, including an "entrant" decal on the windscreen.

4.2.4. Any competitor found on a Special Stage without having registered as above, or in breach of these registrations, may not be permitted to start the event.

4.3. Only standard cars may be used for reconnaissance. Rally cars will not be allowed to be used for reconnaissance. Modified suspensions and altered seats and wheels are permitted. No advertising may be carried on the cars used for reconnaissance. Cars may be scrutinised before the start of reconnaissance to see that they are in compliance with these regulations. The organisers have the power not to allow the use of any particular car.

4.4. The Organisers will issue a Road Book to entrants with lists of Special Stages and speed limits, with supplementary regulations which will apply during the reconnaissance period and which will have the same force as Regulations of the Rally.

4.5. The maximum speed allowed on special stages is 60 Kph, during reconnaissance unless a lower limit is indicated by a traffic sign or a written instruction issued by the Organisers.

4.6. During the reconnaissance period the Gardai and the Organisers will arrange speed checks and general patrols of special stages, and:

4.6.1. Competitors found in cars exceeding speed limits during reconnaissance will be penalise as follows:

1st Offence	2 minutes penalty
2nd Offence	start refused.



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4.6.2. Competitors driving or being driven at an excessive speed or in a manner which in the opinion of the organisers causes or could cause danger or inconvenience to other road users or local residents or could bring the sport into disrepute will be penalised as follows:

1st Offence	2 minutes penalty
2nd Offence	start refused

4.6.3. Reconnaissance is limited to a maximum of three passes per stage or any part thereof. Competitors who exceed this limit will be penalised by exclusion from the event.

4.7. The organisers will appoint Judges of Fact, whose decision will be final.

4.8. Any competition licence holder who practices, reconnoitres or otherwise examines a Special Stage outside the specified reconnaissance period, or causes any person to do so on his behalf will incur the following penalties:

4.8.1. A fine of €380 which will apply to each competition licence holder and payable to the club organising the event at which the breach of regulation was committed, and

4.8.2. Start refused, and

4.8.3. Entry Fee forfeit, and

4.8.4. Start refused for all MI permitted events until any or all monies or fines are paid in full.

4.9. In addition to 4.8 above, if a competition licence holder is observed either by a Judge of Fact or any other person in close proximity to the location of a Special Stage and no reasonable explanation is given to the promoters for his/her presence there then the promoters may refuse to accept an entry or if an entry has already been received, such an entry may be returned. In such circumstances as defined in 4.8 and 4.9 the observations made by the Judge of Fact, or the decision of the promoters cannot be the subject of a Protest or Appeal.

4.10. Reports of Competitors found to be in breach of these regulations will also be submitted to the CEO of Motorsport Ireland for consideration by a Tribunal of Inquiry (Chapter 16).

4.11. Competition licence holders who use the roads, which may be used as special stages, for social, domestic or business use must contact the Organisers for Official Permission to use such roads.

4.12. The use or carrying of Pace Notes when specifically forbidden or any other information concerning Special Stages, except official information, will be reported to MI to consider imposition of penalties. Pace Notes are defined as anything in writing or material on video or audio tape which could be considered as giving the crew knowledge of the course. Annotations to the Road Book will be considered as 'Pace Notes'.

A DVD of the stages may be made available at reconnaissance sign-on for tarmac multi-stages and mini-stage rallies. Clubs must not, under any circumstances, provide, supply or be seen to endorse DVD's prepared by a third party. Clubs must obtain a Letter of Indemnity from any person supplying DVD's for use in the event, indemnifying the club and MI from any claim arising out of the use of the said DVD's.

4.13. Safety Notes.

4.13.1. Safety notes are allowed on Forestry rallies and Single stage rallies. During the **reconnaissance** run over Single Stage rallies Competitors are allowed to **make amendments to the original safety notes**.

4.13.2. On Forestry events, a DVD which may be available for sale up to one week before the event, can be viewed in conjunction with the safety notes for the event, and edits to the event safety notes may be made.

4.14. In this regard, crews who enter the event will agree, to the following Search Procedures, if they are approached by Officials acting on behalf of the organisers during the event.

ARTICLE 5. SEARCH PROCEDURES.

5.1. On approaching the car the Officials will identify themselves to the crew and both members may be asked to vacate the car. Failure to do so immediately. - Penalty 100 minutes.

5.2. Both crew members may then be requested to allow a frisk search. Failure to give permission for such a search - Penalty of 100 minutes.

5.3. Should the Officials believe that documents or other items are being concealed in pockets or underclothing, crew members will be asked to produce the documents or items. Failure to do so - Penalty of 100 minutes. (The onus is on the competitor to satisfy the Officials that he is not carrying Pace Notes).

5.4. Permission to search the car, crew members bags, and co-drivers documents may be requested. Failure to give permission - Penalty of 100 minutes.

5.5. Should anything resembling Pace Notes be found the crew members will be requested to surrender these to the Officials, who will retain any documents they feel might be classified as Pace Notes, for subsequent examination and adjudication. Any such documents may be subsequently returned to the competitor.

5.6. Crew Members will be held responsible for documents found on themselves or in the car.



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5.7. An Official will be entitled to hold a car for as long as necessary to carry out a search. "Permitted time" will be given for any such delays incurred.

5.8. In the event of any of the above offences occurring a report will be made to the Stewards of the meeting who will report the matter to MI to consider the imposition of further penalties. The imposition of any of the above mentioned Penalties is subject to the Competitor's right of appeal to MI.

ARTICLE 6. CARS.

6.1. The Event will be open to any type of Motor Vehicle (except Commercial Vehicles), including station wagons (taxed as 'Private') provided the vehicle complies with the requirements of MI. Competitors are reminded that their cars must comply with the Road Traffic Act at all times and that they must make all reasonable efforts to ensure this. Cars deemed not to be roadworthy may be excluded from the event. All vehicles must have a valid competition logbook.

6.2. An entrant shall be deemed to have full knowledge of his/her car and to vouch for its eligibility by the act of his/her car being presented for Scrutiny at the Start. Any car found to be ineligible for the class entered during the event or at post event scrutiny will be excluded from the finishers list. The onus is on the entrant to ensure that their car is entered for the correct class and that the class entered is the same as that shown on the starters list. The promoters may subsequently make corrections to the starters list.

6.3. Before the Start, and elsewhere at the discretion of the Promoters, cars will be scrutinised in order to identify them as being in accordance with the details of entry, eligibility and compliance with the Road Traffic Act, (see Art 6.1 above) and the Regulations of MI.

6.4. Cars presented without a valid logbook will not be permitted to start. Log books may be obtained by writing to The Secretary, MI Technical Advisory Group, 34 Dawson Street, Dublin 2. Log books issued by the MSA in accordance with Appendix 2 Article 27.8.1. are acceptable.

6.5. Any Competitor who fails to present or have their car presented at Scrutiny will be deemed a non-starter.

6.6. Failure to pass Scrutiny will entail a non-start.

6.7. When vehicles are submitted at Scrutiny, the details on the top of the Scrutiny Card must be filled in by the Competitors, and the Competition Numbers affixed to the car.

6.8. All cars will be held in Parc Ferme at the conclusion of the event. Competing cars must remain in Parc Ferme at the conclusion of the event for a period of 30 minutes after their recorded time at Parc Ferme In Control. Where competing cars have earlier been diverted to a Ceremonial Finish Control, the 30 minute period may run from the recorded time at that control. A protest (See GCR's Chapter 11) regarding eligibility must be lodged, at the latest, at the time of check in at the Final Service "Out" Control. It may be lodged with the CLO or the timekeeper at Final Service "Out" Control.

6.9. In the case of 2/3 day events cars must remain in Parc Ferme during overnight halts.

6.10. Homologation Papers for all Group A and N cars will be required and presented at both mechanical and documentation Scrutiny. Failure to present them will result in a non-start.

6.11. Two or more identification markings showing Competitors Numbers will be provided by the Promoters, and must be displayed on each of the front doors and elsewhere as stipulated by the promoters.

6.12. Additional markings, incorporating, publicity for the Promoter and/or Sponsor of the Event, may be supplied and these must be carried on all competing cars.

6.13. Official markings must NOT be obscured or altered in any way, and must be maintained in a legible condition throughout the competition.

6.14. Official markings must be removed immediately after the event, or on retiring from the competition.

6.15. Cars may be impounded at the conclusion of the event to check eligibility. The Organisers may require any car to be dismantled in order to check eligibility. The Entrant shall be responsible and liable for any work involved and shall do this as and when directed, making available to the Organisers any components or samples required to determine the eligibility of the car. There shall be no claim against the Promoters for any cost involved in this work.

6.16. The entire event shall be completed by each car under its own power, or by gravity, under penalty of exclusion from the Finishers List, except the manual assistance by the occupants of the car will not incur penalty. A car which has left the road may receive assistance to regain it.

6.17. Competing cars will be permitted to carry advertising as per MI Regulations, but no additions are permitted after passing Scrutiny without the permission of the organisers.



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ARTICLE 7. RALLY ROUTE AND TIMING.

7.1. Road Books are issued prior to the event. These will include a list of Route Checks, Controls and Starts of Special Stages, in the sequence in which they shall be visited, and the distances between them. All distances shown in the Road Book shall be deemed to be correct. The alternative Road Book will also form part of the official route and competitors will be instructed when to make use of the alternative route. If circumstances require, the Promoters may issue amendments to the Road Book. These Amendments shall form part of the Official Route. Details of the amendments will be posted on the Official Notice Board or may be handed or notified to the competitors at a location along the route. There is no requirement that the notification of an amendment be in writing.

7.2. Competitors shall at all times follow the Official Route, except that having left it involuntarily, must regain it as soon as possible.

7.3. The position of all Controls and the entire Route will be indicated in the Road Book by Tulip Diagrams.

7.4. The entire Route will be contained on the O.S. Maps, half inch to 1 mile or O.S. Maps 1 to 50 000 on - sheet Numbers.....

7.5. Controls extend 25 metres in all directions from the point indicated on the Road Book. Once inside these boundaries a Competitor is deemed to be at a Control., providing both the Driver, Navigator and the car are within the Control area. This area will be deemed as Parc Ferme and will carry the same restrictions. If access to the control area is blocked and the competing cars are unable to enter the control area the Official at the Control may make a record of the time on the time card.

7.6. Controls open 5 minutes before the due time of the first car, and close 15 minutes after the scheduled time of the last car, extended if necessary in the case of official delays. Official time will be based on R.T.E. time (National Broadcasting Service).

7.7. If a Competitor is held for any reason by an authorised Official, then a "Permitted Time" may be entered on the Time Card.

7.8. Any Competitor who passes through a Control at any time, in a direction other than that defined, in the Road Book, will be penalised as in Article 9.1 (j). - 1 min. time penalty.

7.9. Competitors must visit all controls and have a time recorded on their Control or Time card. Penalty as per Article 9.2(b) - Exclusion.

7.10. Time cards will be issued to Competitors during the event. Competitors will be responsible for handing in each Time Card and recording their times at the last Control shown on that Card, collecting a new card at the same place or otherwise as directed, if necessary.

7.11. The Organisers may establish observation points anywhere without giving notice, to observe any of the following:

7.11.1. Driving manners, and/or compliance with the Highway Code.

7.11.2. Vehicle noise, condition or eligibility.

7.11.3. Average speed in accordance with Article 9.1.11.

7.11.4. Breach of servicing regulations.

ARTICLE 8. SPECIAL STAGES.

8.1. Special Stages will be held during the course of the Competition on roads closed to the public. The Route for these Stages will be defined by arrows. A standing start and a flying finish will apply.

8.2. Competitors must be fully equipped (crash helmets, safety belts, etc) and be ready to start following the Marshal's instructions, at 30/60 second intervals.

8.3. The Starting Marshal shall have the sole right to specify a Competitor's starting time for a Special Stage and rule as to whether or not a Competitor has jumped the Start.

8.4. The Stage Finish Timekeepers will be situated some distance from the finish sign. All Competitors must have the appropriate Time Card completed by this Timekeeper.

8.5. A Stage Time (set at not more than 130kph (80.78 mph) average speed) will be given for each special stage, in minutes and seconds.

8.6. A stage maximum will be given for each special stage, in whole minutes.

8.7. The method of assessing time taken on a Special Stage will be as follows:

8.7.1. A competitor recording less than the Stage Time will be debited with that Stage Time.

8.7.2. A competitor exceeding the Stage Time will be debited with his recorded time.

8.7.3. Additionally, each whole minute in excess of the Stage Maximum will also attract penalties as per Article 9.1.4, - lateness at a Control, and count as lateness towards exclusion as per Article 9.2.3. - maximum lateness. A special Stage Finish is considered a Time Control, with the Stage Maximum being the Target Time from the Stage Start.

8.8. No Case of Force Majeure or baulking will be taken into consideration after a Competitor's scheduled time from the Start Control.



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Nevertheless, the Promoters have the right to penalise any Competitor deemed to have unreasonably delayed another Competitor.

8.9. Each Special Stage will be open to a Competitor from his due time, until 15 minutes after his due time, extended if necessary in the case of Official delay.

8.10. Any Competitor whose car blocks the passage of other competitors, will be assumed to be acting against the interests of the sport, unless the contrary can be proven to the satisfaction of the promoters. Such Competitors, if any, may be excluded from the event.

8.11. Where it can be established that a competitor has incurred accumulated penalties in excess of the 15 minutes lateness permitted and the organisers are satisfied that their continued participation in the event poses a safety risk then the competitor may be excluded from the competition by the Clerk of the Course. The competitor should be informed of the decision of the Clerk of the Course upon his/her arrival at the next control. i.e. stage arrival, stage start, stage finish or other control.

In these circumstances the decision of the Clerk of the Course will be that of a Judge of Fact and as such cannot be the subject of a Protest or Appeal.

8.12. It is absolutely forbidden for Competitors, or their Service Crews, to double park, to park on the right hand side of the road, to form a queue other than in single file, to park on double yellow lines, or otherwise to block or congest an open road - Reference Article 9.1.15. -1 min. penalty.

8.13. Interruption of a timed stage. When a Special Stage has to be stopped for any reason whatsoever before the last crew has covered it, a classification for the Stage may however be established by allocating to each crew, which has been unable to complete the stage because of the interruption, the time of the last competitor to complete the stage before the interruption. This classification may be drawn up by the Clerk of the Course even if only one crew has been able to cover the stage in normal competitive conditions. Should the Clerk of the Course consider the time set by the last competitor to complete the stage as abnormal, he/she may choose as the scratch time the one among the four previous competitors to complete the stage which seems to be the most suitable. However, no crew which is totally or partially responsible for stopping a stage may benefit from this measure. It will be given the time which it might have eventually set if this is greater than the scratch time awarded to the other crews. Following consultation with the Safety Officer a stage may be re-started and the stage interrupted time will only be applied to those

crews affected by the stoppage. Crews completing the stage after it has been re-started will be credited with their actual time. If 10 minutes has elapsed since the start time of the last competing car that started the stage, then the stage may only be restarted in the following specific circumstances:

A stage may be restarted up to 16 minutes after the start time of the last competing car, provided that a 00 car is available at the stage start and has entered the stage no more than 11 minutes after the start time of the last competing car and the 00 car has passed the second radio point before the next competing car starts the stage. This re start procedure will be under the control of the CoC and the Radio Controller. A report (to include start and finish times together with times 00 reached the second radio point) for all re starts involving 00's must be submitted to the appointed MI Stewards of the event.

8.14. If the promoters became aware that a number of competitors were delayed on a Special Stage due to a blockage and the nature of the blockage can be verified, or that they had stopped to comply with Appendix 29, Article 3.5.1.3., then, the Clerk of the Course may apply the stage interrupted rule to the competitors affected. Alternatively the promoters may apply the Force Majeure rule.

8.15. If because of a stage blockage, no car receives a competitive time for that stage, the offending competitor will receive their actual time while the balance of competitors will receive the stage minimum time.

8.16. The point of interruption will be defined as the time at which an incident takes place. The stage interruption time will apply to ALL competitors who start the stage after the competitor who causes the interruption and prior to the stage restart.

8.17. Yellow Flag. The marshal in charge or areas of particular risk will be provided with a YELLOW FLAG to be used as an additional hazard warning of potential danger, if an incident occurs or spectators encroach in this area. The use of these flags will NOT involve stopping the stage.

Use of Yellow Flag:

Yellow Flag - Stationary:

Danger - hazard ahead, slow down, (no overtaking).

Yellow Flag - Waved:

Great danger, slow down considerably, be prepared to suddenly change from the projected racing line or take other evasive action, be prepared to stop if necessary (no overtaking).

8.18. Stage Stopped Board Flag. The Clerk of the Course and Safety Officer will have decided in



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advance where this board/flag will be deployed and it will be under the control of the Senior Marshall, on the junction or at a hazard point. A stage stopped board/flag will be issued to each mid-point and it will be demonstrated to competitors at drivers briefing.

8.19. If spectators refuse to move from what the Organisers have agreed is to be a prohibited area the marshal has the authority to walk a hundred metres back into the stage and display the stage stopped board/flag to the drivers. This will have the immediate effect of telling the drivers that the Stage Timing is being stopped and they must then slow to fifty kilometres per hour or less while driving through the remainder of the stage. The board/flag measuring 600 x 600 or larger will comprise a White background with a Red clock face crossed by a black diagonal bar.

Where a Stage Stop board/flag is shown competitor will be given a time as for an interruption of a Timed Stage.

ARTICLE 9. PENALTIES.

9.1. Penalties will be applied as follows:

9.1.1. Stage Time See Article 8.7

9.1.2. Actual Stage Time See Article 8.7

9.1.3. Jumping the Start of the Special Stage (each Occurrence) **30 seconds**

In the case of Forestry events the Penalty will be **60 seconds**

9.1.4. Each minute late at Control: **15 seconds**

9.1.5. Each minute recorded before scheduled time at any Control: **1 minute**

9.1.6. Each minute recorded over 3 minutes between the "Stage Arrival Control" and the "Stage Start" except in the case of an official delay. **1 minute**

9.1.7. Delay at checking in or leaving any timed point, when instructed to do so by an Official: **1 minute**

9.1.8. Obscuring or altering the Official identification markings (on either Rally or Service Car) or failure to maintain these in a legible or correct condition: **1 minute**

9.1.9. Creating excessive noise: **1 minute**

9.1.10. Non-compliance at any time with each specified direction of travel through any Control: **1 minute**

9.1.11. Each occasion on which a Competitor's average speed on a road exceeds an average of 60 kph: **1 minute**

9.1.12. Non-compliance with a reasonable instruction of an Official: **1 minute**

9.1.13. Each breach of a provision of the Road Traffic Act by a Competitor or by his Service Crew: **See Article 3.3**

9.1.14. Competitor or Service Crew entering a forbidden area defined in the Road Book: **1 minute**

9.1.15. Infringement of Article 8.12 or Article 15.1: **1 minute**

9.1.16. Non-compliance with any official requirement for which a penalty is not specified: **2 minutes**

9.1.17. Breach of Article 4.8 or 4.9: Please refer to penalties stated in 4.8 or 4.9

9.1.18. Breach of Article 15.4

1st offence :

2 minutes

2nd offence :

addit. 5 minutes

3rd offence :

Exclusion

9.2. The following will all carry the Penalty of EXCLUSION from the Finishers List:

9.2.1. Loss of Control or Time Card.

9.2.2. Absence of a recorded Time on a Control or Time Card.

9.2.3. Over 15 minutes late over a scheduled time at a Control (with adjustment in the case of an Official delay).

9.2.4. Failure to produce or surrender any Time Card on request.

9.2.5. Altering a Time Card.

9.2.6. Failure to complete any special stage.

9.2.7. Failure to wear an MI approved helmet, clothing, or seatbelt, properly fastened, on a special stage.

9.2.8. Smoking on a special stage.

9.2.9. Servicing outside specified areas. As per Art 11.1.

9.2.10. Failure to attend or pass Scrutiny or re-Scrutiny.

9.2.11. Infringement of Articles 2.4 - false statements, 3.3 - driving standards, 6.16 - outside help, and 11.10 - service crews entering Special Stages.

9.2.12. Any Competitor who introduces to his vehicle during the Competition any modification that will change the characteristics shown on the entry form and on the respective forms of recognition or who makes a wrong declaration as to the class entered. (see 9.2.17. below).

9.2.13. Any Competitor who does not produce their competition licence when requested during competition.

9.2.14. Any Competitor who does not follow the official itinerary.

9.2.15. Carrying or using Pace Note or Stage Information in competing cars or Service vehicles when prohibited.

9.2.16. Competitors or Service Crews entering Results Caravans/Rooms without invitation.

9.2.17. Making a wrong declaration as to the Class for which the vehicle is eligible. (see 9.2.12. above).

9.2.18. Breach of article 15.4 - 3rd offence

ARTICLE 10. TIES.

10.1. In the event of any tie in the Finishers List, or for any Award, precedence will be given to the Competitor having least Penalty points in the last Special Stage of the Competition. If this fails to resolve the tie, the Penalty points for the



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penultimate Special Stage will be taken into consideration, and so on until the tie is resolved.

ARTICLE 11. SERVICING.

11.1. Servicing of a competition car may only be carried out in specific areas designated by the organisers (Service Areas) and from vehicles bearing an official plate supplied by the organisers of the event. The official plate will include the competitor's competition number and must be affixed to the front of the service vehicle throughout the event.

11.2. Service Areas where servicing is permitted will be identified in the event Road Book.

11.3. A competitor shall be responsible for ensuring that any officially plated vehicle associated with his entry complies with all appropriate regulations and official instructions. A penalty of €200 may be applied if the users of such plates are observed acting in a manner contrary to the interests of the rally or contravening any specific instructions from any official.

11.4. Crew members, using solely the equipment on board and with no external physical assistance, may perform service on the car at any time, other than where specifically prohibited.

11.5. IN ANY AREA WHERE SERVICING IS NOT PERMITTED THE PARKING OF AN ASSOCIATED VEHICLE, THE PRESENCE OF TEAM PERSONNEL, AND/OR THE SETTING UP OF EQUIPMENT MAY BE DEEMED TO BE SERVICING.

11.6. Tyre monitoring.

11.6.1. Tyre changes may only be carried out in service areas or any other area specified in event regulations for that purpose.

11.6.2. Tyre marking zones and/or bar code reading zones may be established at the exit of above. Penalties as per 9.2.11.

11.7. Event organisers will appoint at least one named Judge of Fact to observe compliance with Service Regulations.

11.8. Penalties. The parking of an associated vehicle and/or the setting up of equipment on the rally route or within one mile of an area where servicing is not permitted.

1st Offence: 5 minutes
2nd Offence: Exclusion

Servicing (other than as permitted by crew Members) outside of designated Service Area

1st Offence: Exclusion

Competitors will be supplied with one service number, which must be carried in the service vehicle, and affixed in a forward facing position.

11.9. Service Crews must at all times, drive and conduct themselves in a quiet and reasonable manner. Penalties may be incurred if a Service Crew is observed:

11.9.1. Speeding.

11.9.2. Driving in an inconsiderate manner.

11.9.3. Contravening specific official instruction.

11.9.4. Leaving litter.

11.10. Under no circumstances may service vehicles enter Special Stages during the road closing time of that stage. This will be penalised as per Article 9.2.11.

ARTICLE 12. RESULTS AND PROTESTS.

12.1. A competitor wishing to raise a query concerning any matter connected with the event should do so by means of a Query Form, obtainable from the Competitor Liaison Officer or the Rally Office.

12.3. Protests or Appeals must be made in accordance with the General Competition rules of MI. Protests and Appeals must be accompanied by the appropriate Protest Fee, or Appeal Fee, as per the Schedule of Fees contained in the General Competition Rules. All protests must be lodged with the Clerk of the Course.

12.4. If the Protest requires the dismantling and reassembling of a competing car, the claimant must make an additional deposit as decided by the C.O.C. (GCR 163.2). This deposit will only be refunded if the Protest is upheld.

12.5. Any expenses incurred by work on a car shall be borne by the claimant if the Protest is unfounded, or by the competitor, against whom the Protest is made, if the protest is upheld.

12.6. Any Protests must be submitted within the following time limits:

12.6.1. Against a decision of a Scrutineer - must be lodged immediately following its notification to the Competitor concerned. (GCR 163)

12.6.2. Against the results of classification - within 30 minutes of their posting on the Official Notice Board, or other place as specified in Final Instructions.

12.6.3. If a Competitor considers during the Event, that an irregularity has occurred justifying a Protest, he must submit, in writing, at the first Time Control after the incident in question, a statement of his intention to lodge such a protest.

12.6.4. Against Special Stage times - must be lodged within 30 minutes of their posting on the Official Notice Board, or other place as specified in Final Instructions, or at any Rest halt or Overnight halt as 'Provisional'. Failure to comply with these directions means forfeiture of the right to protest.

12.6.5. Vehicle eligibility - Post event See GCR 163.



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12.7. Only the entrant of a competing car has the right to protest.

12.8. After re-posting results, following Protests or Queries, Appeals or Queries will only be accepted regarding original Protests or Queries. No Protests or Queries on new matters will be entertained.

12.9. All competitors, as appropriate, have the right of appeal against the decision of the C.O.C.

ARTICLE 13. SUNDAY RUN.

13.1. Competitors who fail to finish the event on Saturday, may compete in the Sunday Run if applicable, but they must inform the Promoters of their intention to do so in writing before 22.00 hrs. on Saturday.

13.2. In a two day event, 'Sunday Run' Competitors will be obliged to present their car for Re-Scrutiny on the Sunday morning at a time specified in instructions posted on Saturday. Failure to present the car for Re-Scrutiny will incur Penalty as per Article 9.2.10.

13.3. Sunday runners may be reseeded in the main field in accordance with their expected performance.

ARTICLE 14. PRIZE GIVING.

14.1. It is a condition of receiving a Perpetual Trophy, that it cannot be won outright, and that it be returned (in similar condition as when received), and that the recipient signs, at prize-giving, for its receipt, safe-keeping and return.

14.2. The overall winning Crew will be ineligible for all other awards other than Nationality, Manufacturers and Team Awards. Crews placed second and third overall will be ineligible for Class Awards.

14.3. The Promoters shall decide on the eligibility of Crews for the Novice Award.

14.4. Additional Awards may be presented, details of which will appear in the Final Instructions.

14.5. A Novice is one who has never won an Award on a Special Stage Rally.

14.6. Prize-winners must be present at Prize-giving Ceremony, otherwise they will forfeit their Awards. However, if a Prize-winner cannot attend the Prize-giving, he must nominate a person to collect the Award, and the Promoters must be informed of the Nominee, in writing, one hour before the scheduled Prize-giving time.

ARTICLE 15. GENERAL.

15.1. The Spirit of Rallying must be seen to be observed at all times. The Promoters shall be the sole judges of this.

15.2. Judges of Fact will be appointed to patrol the route, to determine breaches of the Regulations and their decisions shall be final.

15.3. The Promoters/Organisers will be the sole Judges of Fact (see 15.5).

15.4. Judges of fact will report to the clerk of the course all competing vehicles which are noted as having failed to stop at 'Yield Right of Way' and/or 'Stop' signs on all road sections.

15.5. All named officials of the event plus additional persons as nominated by the C.O.C. shall be Judges of Fact. They will be deemed to be Judges of Fact of the following:

15.5.1. Any matter which is the subject of a penalty under Article 9.1.1. to 9.1.8. including 9.1.10., 9.1.14., 9.1.17. and 9.1.18.. or 9.2.4., 9.2.8., 9.2.9., 9.2.15., 9.2.16., 9.2.17.

15.5.2. Any other matter which the C.O.C. shall specify in the supplementary regulations or final instructions.

15.6. The Promoters may impose penalties on any competitor upon the receipt of a complaint from a member of the Public or any other person, including a competitor, provided that the complaint is in writing and the person making the complaint is willing to make themselves available at any subsequent hearing of a Protest or Appeal against the decision of the Clerk of the Course. The Clerk of the Course must first satisfy himself/herself that the motives for the complainant are not malicious or vexatious. The procedure outlined above does not refer to a member of An Garda Síochána whose report in writing is acceptable.

ARTICLE 16. COMPETITOR LIAISON OFFICER (CLO).

16.1. The function of a CLO is to liaise with competitors and play the role of mediator at all times.

This post is entrusted to an official who has a knowledge of the regulations and the GCR's, and the CLO may be invited to attend meetings of the panel of the Steward's in order to keep informed of all decisions taken. He/she must be easily identifiable to the competitors. To this end it is advisable that he/she be introduced to the competitors at drivers briefing.

The Competitor Liaison Officer shall refrain from saying anything or taking any action which may give rise to a protest.



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ARTICLE 17. HISTORIC CARS: INTRODUCTION.

17.1. Historic cars may be used for competition under a set of rules that preserve the integrity of the vehicle in period. To comply with this principle Motorsport Ireland has adopted the technical rules, regulations and guidelines of the MSA (British) Championship including the Historic Rally Car Register (HRCR) with effect from 1.1.2012.

17.2. Through the HRCR, the website www.hrcr.co.uk provides technical regulations and information for those wishing to participate in categories 1, 2 & 3.

17.3. In addition all cars must comply with the requirements of Appendix 2 of the current Motorsport Ireland Yearbook.

ARTICLE 18. HISTORIC VEHICLE CATEGORIES AND CLASSES.

18.1. For sealed surface tarmac events classes may be divided into the following Categories and Classes. Awards may be made to each Category, but no overall winner of the Historic section in a multi discipline sealed surface rally may be declared.

18.2. All Stage Rallies may combine Categories 1, 2 & 3 as one for the purpose of declaring results and issuing awards with Category 4 and FIA Appendix K cars treated as separate category.

18.3. CATEGORY 1: HISTORIC CARS - Vehicles first registered from 1.1.1960 to 31.12.1967.

Class B1: Up to 1000cc
 Class B2: 1001cc up to 1300cc
 Class B3: 1301cc up to 1600cc
 Class B4: 1601cc up to 2000cc excluding all Porsche 911/2
 Class B5: Over 2000cc including all Porsche 911 and 912

18.4. CATEGORY 2: POST HISTORIC CARS - Vehicles first registered from 1.1.1968 to 31.12.1974.

Class C1: Up to 1300cc
 Class C2: 1301cc up to 1600cc
 Class C3: 1601cc up to 2000cc
 Class C4: Over 2000cc
 Class C5: All cars having engines with both more than one camshaft and more than 2 valves per cylinder and all cars with rotary engines.

18.5. CATEGORY 3: CLASSIC RALLY CARS - Vehicles first registered from 1.1.1975 to 31.12.1981.

Class D1: Up to 1300cc
 Class D2: 1301cc up to 1600cc
 Class D3: 1601cc up to 2000cc
 Class D4: Over 2000cc
 Class D5: All cars having engines with more than one camshaft and more than 2 valves per cylinder and all cars with rotary engines.

During the period 1.1.2012 to 31.12.2012 cars may compete in categories 1, 2, & 3 without a Historic Rally Vehicle Identity Form (HRVIF) but must have this completed in order to compete in these categories from 1.1.2013.

HRVIF forms may be obtained from a registered eligibility Scrutineer (MI Yearbook Appendix 2).

ARTICLE 19. FIA APPENDIX K CARS.

In addition to the above classes a separate category may be included in all events for vehicles that comply with the current FIA Appendix K regulations and which are in possession of a valid FIA Historic Technical Passport (HTP).

ARTICLE 20. CATEGORY 4 HISTORIC CARS.

All cars that do not comply with the above or FIA Appendix K and were registered no later 31.12.1981.

(This category will be discontinued on 31.12.2012)

ARTICLE 21. NOISE LIMITS.

For MI permitted events only, the noise limits as specified in Appendix 2 Article 9 shall apply.



RALLY SPRINTS

APPENDIX 30

STANDARD REGULATIONS FOR ALL RALLY SPRINTS

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Event Name
Organising Club
Date of Event
Permit Number

To be run under the International Sporting Code of the FIA, the General Competition Rules of Motorsport Ireland and these Supplementary Regulations

Motorsport Ireland Steward :
Club Nominated Steward :
Clerk of the Course :
Assistant Clerk of the Course :
Safety Officer (if applicable) :
Secretary of the Event :
Medical Officer :

Organisation

The event is organised and promoted by name of Club, hereinafter called the promoters.

1. TIMETABLE.

Entries Open :
 Entries Close :
 Final Instructions Posted :
 Scrutiny :
 Drivers Briefing :
 First Car Start :
 First Car Finishes :
 Provisional Results :

2. ENTRIES.

Entries on the completed entry form accompanied by the entry fee of € state amount should be sent to: Name and Address of Secretary. The entry fee does / does not include Competitors' Personal Accident Insurance.

3. IRDS.

IRDS does / does not apply to this event.

4. EVENT.

Approximate Length :
 Surface :
 Tarmac / Loose or a Combination of both :
 Number of Runs :
 Runs to Count :
 Timing :
 Clocks :
 or Transponders

5. RESULTS.

The results of the competition will be based on each competitor's cumulative time for the total number of runs completed and any penalties incurred.

6. TIMING.

Timing will be to the nearest one hundredth / tenth of a second from a standing start to a flying finish. The maximum time allowed to complete the stage is 10 minutes - max may be varied but must be posted on the notice board

Times will be posted on the official notice board or other place specified by the organisers. The location of the official notice board will be "...state location...", or the location will be notified to competitors in final instructions.

7. TIES.

Ties will be resolved by reference to the fastest time on the last run and so on until the tie is resolved.

8. COMPETITORS.

Double driving may be permitted and will be deemed to be separate entries. Separate entry forms must be completed and submitted. Passengers may be carried subject to their holding the appropriate competition licence, signing an indemnity form and paying the Competitors Personal Accident Insurance fee. A driver must carry the same passenger for all the timed runs.

For events promoted by Motorsport Ireland, different co-drivers may be carried for each run, provided that the Rallies Committee has been asked for and has given approval in advance. They must meet the standard licence requirements set out in the previous paragraph.

9. CLASSES.

Class 1: 2WD Rally Cars up to 1450cc normally aspirated

A Cars up to 1450cc with only 2 valves per cylinder
B Cars up to 1450cc with more than 2 valves per cylinder

Class 2: 2WD Rally Cars from 1451cc up to 1650cc normally aspirated

A Cars from 1451cc up to 1650cc with only 2 valves per cylinder
B Cars from 1451cc up to 1650cc with more than 2 valves per cylinder

Class 3: 2WD Rally Cars from 1651cc up to 2100cc normally aspirated

A Cars from 1651cc up to 2100cc with only 2 valves per cylinder
B Cars from 1651cc up to 2100cc with more than 2 valves per cylinder



RALLY SPRINTS

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Class 4: 2WD Rally Cars from 2101cc up to 3500cc normally aspirated

Class 5: All 4WD Cars
A 4WD Group N complying with their last published homologation papers as per FIA Appendix J
B 4WD Rally Cars including all WRC complying with their last published homologation papers as per FIA Appendix J and cars with MI permission to run letters (National Tarmac Class 15)

Class 6: Rallycross Cars up to 1650cc

Class 7: Rallycross Cars 1651cc to 2150cc

Class 8: Rallycross Cars over 2150cc

Class 9: Rallycross Supercars

Class 10: 14 to 16 years old up to 1000cc, Junior Speed licence holders, no co-driver

For Junior Speed Licence details - see Appendix 1.

Class 11: 14 to 16 years old up to 1400cc, Junior Speed licence holders, no co-driver

Class 12: 16 to 18 years old up to 1600cc, Junior Speed licence holders, no co-driver

Upgrade from Class 10 may only be made after 6 finishes in Class 10.

Organisers may use any or all of class's 1 to 12 and may sub divide as indicated if required.

Notes on Classes 10, 11 and 12:
In order to run these Junior classes the following procedure must be followed:

1. A trained children's officer must be available to liaise with these competitors for the entire running of the event.
2. A day-glo X sticker 200mm x 200mm must be fitted to the rear of each of these cars to identify them as juniors to other competitors.
3. Clubs are reminded to refer to GCR 108.2.3 with regard to accepting entries for these classes.

10. PROTESTS.

All protests must be as per the General Competition Rules, Chapter 11.

11. PENALTIES.

In addition to the penalties contained in the FIA International Sporting Code, the General Competition Rules, and the Motorsport Ireland Yearbook the following regulations will also apply.

11.1. Jump Start 30 seconds

11.2. The following will carry the penalty of exclusion from the event:

11.2.1. Failing to report to the start when instructed to do so.

11.2.2. Not wearing safety clothing, helmet, or other equipment on the stage.

11.2.3. Overtaking between the flying finish and the stop point.

11.2.4. Driving in the opposite direction on the stage.

11.2.5. Servicing in a prohibited area.

11.2.6. Driving in a dangerous or reckless manner in the service/paddock area.

11.2.7. Carrying a passenger other than that stated on the entry form.

11.3. Maximum Time will be given for:

11.3.1. Failing to complete the stage.

11.3.2. Missing a timed run.

12. AWARDS.

May be varied by each organiser

1st Overall

2nd Overall

3rd Overall

1st, 2nd and 3rd in each class



DUNLOP NATIONAL RALLY CHAMPIONSHIP

APPENDIX 31

2012 DUNLOP NATIONAL RALLY CHAMPIONSHIP REGULATIONS

ARTICLE 1.

1.1. FORMAT.

COUNTING EVENTS IN THE 2012 CHAMPIONSHIP

26th February	Birr Stages Rally
11th March	Mayo Stages Rally
1st April	Circuit of Kerry Rally
13th May	Carlow Stages Rally
3rd June	Limerick Circuit of Munster Rally
12th August	Tipperary Stonethrowers Rally
16th September	Clare Stages Rally
13th October	Donegal Harvest Stages Rally

1.2. Best performance in **6** out of **8** events will count. Juniors: Best performance in 4 out of 6 events only will count.

1.3. Events are arranged into 2 regions. To qualify for a championship award competitors must register for and start a minimum of four events, at least one of which must be in each region. A fee of €7.50 per crew per event will be charged and will be included in the entry fee for each event.

1.4. The following will be regarded as the two regions:

Region One: *Birr, Mayo, Donegal, Carlow.*
Region Two: *Kerry, Limerick, Tipperary, Clare.*

ARTICLE 2.

2.1. CLASSES.

The Championship will include the following classes:

Class 1.	Group N up to 1400cc
Class 2.	Group N 1401cc to 1600cc.
Class 3.	Group N 1601cc to 2000cc.
Class 4.	Group N over 2000cc.
Class 5.	Group A up to 1400cc.
Class 6.	Group A 1401cc to 1600cc.
Class 7.	Group A 1601cc to 2000cc.
Class 8.	Group A over 2000cc.
Class 9.	Modified cars up to 1450cc - 2 wheel drive, normally aspirated.
Class 10.	Modified cars 1451cc to 1650cc, 2 wheel drive, normally aspirated with not more than 2 valves per cylinder.

Class 11. Modified cars 1451cc to 1650cc,
2 wheel drive, normally aspirated
with more than 2 valves per cylinder.

Class 12. Modified cars 1651cc to 2100cc,
2 wheel drive, normally aspirated
with not more than 2 valves per
cylinder.

Class 13. Modified cars 1651cc to 2100cc,
2 wheel drive, normally aspirated
with more than 2 valves per cylinder.

Class 14. Modified cars 2101cc to 3500cc,
2 wheel drive, normally aspirated
or **forced induction**.

Class 15. Four wheel drive whose
homologation has expired
and other specified cars,
as per Appendix 29.

Class 16. Cars up to 1650cc, 2 wheel drive,
normally aspirated. Drivers to be
under 26 on the 1st of January 2010
and contested over a limited
number of stages.

2.2. Notes on Classes.

2.2.1. These classes must be included in all Championship events but Clubs may subdivide them if they so wish and/or include additional classes subject to the overall limitation that Clubmans cars are restricted to a maximum engine capacity of 3500 cc.

2.2.2. See also 'Notes on Classes', Appendix 29 standard regulations for all Stage Rallies Rule Nos. (1) to (8) inclusive

ARTICLE 3.

3.1. AWARDS.

1st Overall

Driver: Cecil Vard Trophy (perpetual)
and replica.

Co-driver: Noel Smith Trophy (perpetual)
and replica.

2nd Overall

Driver: Trophy. Co-driver: Trophy.

3rd Overall

Driver: Trophy. Co-driver: Trophy.

To the driver achieving the highest overall placing in a two wheel drive car, the best **6** scores from **8** to count.

Winners of the above award are **eligible for overall but not for class awards**.



DUNLOP NATIONAL RALLY CHAMPIONSHIP

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GROUP N AWARD

To the Driver achieving the highest placing in the overall classification using only points scored in a Group N car. Only the best eight scores out of ten to count.

JUNIOR CLASS

The Winner of Class 16, the Junior Class will qualify for the selection process for the Billy Coleman Award.

MICHAEL FARRELL MEMORIAL TROPHY

To the Class Winner with the greatest number of Class Points from his/her eight eligible rounds, excluding those classes from which 1st, 2nd & 3rd overall are derived.

3.2. Class Awards.**1st in each Class**

Driver: Trophy. Co-driver: Trophy.

2nd in each Class

Driver: Trophy. Co-driver: Trophy.

3rd in each Class

Driver: Trophy. Co-driver: Trophy.

3.3. The number of awards in any class may be reduced if entries during the year are insufficient.

ARTICLE 4. GENERAL RULES.

4.1. Substitution of events, or alteration of dates will be permitted in the case of 'force majeure' at the discretion of the Motor Sport Commission (MSC). (GCR 12)

4.2. The Championship will be open to the following categories of licence holders:

4.2.1. Motorsport Ireland National 'A' and International Licence holders.

4.2.2. Motorsport Ireland National 'B' Licence holders who may only drive in cars up to 1650cc.

4.2.3. MSA Licence holders, of a grade not lower than National "B", who may be resident in either Ireland or Britain with Medical Certificates.

4.2.4. Co-drivers must hold Driver or Navigators Licences.

4.3. All cars complying with the classes as per Article 2 of these regulations are eligible for the Championship. Commercial vehicles are not eligible.

4.4. Promoting Clubs Responsibilities:

4.4.1. Accommodation on night prior to event in Rally Headquarters for Championship Registrar for two people, plus evening meal.

4.4.2. The club must provide a suitable location, to be agreed with Registrar, for registration and information which will take place during the times specified in the event regulations for documentation on the evening before the event.

4.4.3. The club will provide the Registrar with a class-based list of entries and reserves prior to the publication of the entry list.

4.4.3.1. Note: This will allow the Registrar to assist clubs in determining which drivers are entitled to priority. In so doing clubs must afford the Registrar a reasonable period within which to revert to them with comments. Minimum 48 hours.

4.4.4. The travelling expenses will be paid directly by Motorsport Ireland to the Registrar.

4.4.5. The Club must also provide sufficient copies of the up to date Championship Results for distribution on the event.

4.4.6. Once the results have been declared final the organisers must provide to the Registrar the following information:

1. Full list of starters.
2. Full list of Overall Results.
3. Full list of Class Results.

4.5. The interpretation of these rules in case of doubt, shall rest with the Registrar in consultation where necessary with Motorsport Ireland National Stages sub committee. This decision will be subject to the approval of the Motor Sport Commission of the RIAC, subject to the overriding right of appeal under the GCRs of MI.

4.6. The Championship Registrar is:

Laura Fuery
c/o Motorsport Ireland
34 Dawson St
Dublin 2
Telephone: 087 6646002
e-mail: nrcregistrar@hotmail.com

The National Championship
Sub Committee Chairman is
Richard Talbot
Talbyrn House, Caherleheen, Tralee, Co. Kerry
Telephone 087 2680732
E-mail: rtalbot@eircom.net

4.7. Scrutiny Requirements.

See Appendix 2.

ARTICLE 5. POINTS SCORING.

5.1. Points will be awarded as follows on the basis of results from each round. In addition, each class will be marked separately as per 5.1.2.

5.1.1. Overall Points:

20/18/16/14/13/12/11/10/9/8/7/6/5/4/3

5.1.2. Class Points:

10/8/7/6/5/4/3/2/1

5.2. In addition one point will be awarded to each registered finisher not receiving points from 5.1.1 or 5.1.2 above.

5.3. Championship points will be awarded on the basis of the finishing order of Registered Competitors (i.e. 1st placed registered competitor 20 pts, 2nd placed registered competitor 18pts



DUNLOP NATIONAL RALLY CHAMPIONSHIP

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and so on. Class points will be awarded on the same basis).

5.4. If the number of counting rounds is reduced, for any reason, the Registrar of the championship will in consultation with the Motorsport Ireland National Stages Sub-Committee, make a recommendation to the Motorsport Ireland Motor Sport Commission as to what amendments should be made to the regulations for the championship. This will take into account at what point during the championship the number of counting rounds is reduced. All registered competitors will be notified in writing within 14 days of the MSC reaching a decision. Ties in either the overall or class placing will be decided in favour of the competitor with the greatest number of wins, and failing this, the greatest number of second places etc, until the tie is resolved. If further ties remain they will ultimately be decided on the basis of performance in the sequential order of events, i.e. the first event will count first, and so on. (See Appendix 15 No. 6.10.

5.5. Registration for the championship will close immediately after it no longer becomes possible for a competitor to comply with the regulations in accordance with Art. 1.3. Competitors who have registered prior to this but cannot comply with Art. 1.3 will not be awarded any further points.

5.6. The following rules will apply to handling of entries for each championship round.

5.6.1. Entry forms to be available 10 days before the official opening date. Official entry forms should be made available to all registered drivers. Labels are available on request from championship registrar. The onus will be on the competitor to ensure that he is in receipt of the regulations prior to the relevant priority dates.

5.6.2. Regulations for National Championship events must not open earlier than a date calculated as Monday, five weeks prior to the last Monday before the event. e.g. (Sunday event 42 Days).

5.6.3. There will be a common date for the closing of entries, including priority entries as per Art 7.1. In the event of the closing date being extended priority entries must be secured by the original closing date.

5.6.4. Cheques must not be lodged before closing date for all entrants who are awarded an entry. A reasonable number of reserve entries may also have their cheques lodged at closing date. Clubs are encouraged to state on regulations that cash should not be sent with entry.

5.6.5. Where a club starts a maximum entry, no money can be withheld from the non-starters.

5.6.6. Where a club does not start a maximum entry due to the withdrawal of entries, the club may impose financial penalties to the last cars that withdraw from the starters' list.

5.6.7. All non starters are to be refunded their cheques not later than fourteen days after the event.

5.6.8. National Championship Events will be allowed to start 140 cars on a 3 x 3 stage format or 160 cars on a 4 x 2 stage format provided there are 60 priority places afforded to National Championship Registered Competitors as per Art 7.1.

ARTICLE 6. REGISTRATION.

6.1. Competitors are required to register for the championship at documentation of the first qualifying round that they enter.

Once registered it is not a requirement that you sign on at subsequent rounds, Drivers and Co Drivers register and score points on an individual basis. On registering competitors agree to be bound by these regulations and any amendments subsequently issued. Competitors who wish to change class during the championship must re register in the class they are moving to.

6.2. The nominated driver must drive all competitive sections of each event.

6.3. To be eligible as a counting round each event's route must comprise not less than 104 stage kilometres as defined in the published road book.

ARTICLE 7. ENTRY PRIORITY FOR CHAMPIONSHIP REGISTERED COMPETITORS.

7.1. The First Five. Registered drivers in the Overall standings.

The First Two. Registered drivers in Classes 1 and 5.

The First Three. Registered drivers in Classes 2, 3, 7, 9, 10, 12, 14, and 15.

The First Four. Registered drivers in Classes 11 and 13.

The First Five. Registered drivers in Classes 6 and 8.

The First Nine. Registered drivers in Class 4 will be entitled to priority provided that their entries are in order on the closing date as defined below. In the event of the registered drivers entitled to priority in each of the classes above not having a valid entry in by the closing date, the priority will pass to the next registered driver in that class who has a valid entry submitted. This will facilitate a maximum of 60 Registered Drivers receiving priority entries for the events.

Event	Closing Date for ALL entries
Birr Stages Rally	13th February
Mayo Stages Rally	28th February
Circuit of Kerry Rally	20th March
Carlow Stages Rally	30th April
Circuit of Munster Rally	21st May
Tipperary Stonethrowers Rally	30th July
Clare Stages Rally	3rd September
Donegal Harvest Stages Rally	1st October



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7.2. Priority for Championship Registered Drivers for Round 1 and 2 will be the Award Winners from the **2011** Championship. On Round 2 should the award winners from **2011** not make up the 60 places provided for the national championship the shortfall will be taken from the class positions after Round 1. (For example if 40 places on round 2 are taken by **2011** awards winners the remaining 20 places will be made up from the final results after round 1.) These remaining places will be decided by class winners from Round One first, followed by 2nd and 3rd in class.

Priority for the remaining eight rounds will be based on a Registered Driver's Championship position at the closing date for entries for that event, provided that their entry is in order and subject to the following:

7.3. Drivers who cannot improve their awards position by competing in additional rounds will not be granted priority for those rounds. Priority will then pass to the next registered driver in that class and so on. A listing showing competitors entitled to priority will be supplied to the Clerk of the Course of the events by the Registrar.

7.4. Where a registered driver, by virtue of the fact that he has won his class, is not entitled to priority but is in contention for the Michael Farrell Memorial Trophy, he will also be entitled to priority, provided that his entry is in order on the closing date as defined above.

ARTICLE 8.

8.1. All events must be run in accordance with the General Competition Rules and Appendix 25, 29 and 31 of this Yearbook.

8.2. Competitors must provide an area under the windows on each front door of the car for the Official championship logo, as supplied. Competitors should also provide an area on both sides of car for any Advertising proposed by championship event organisers.

8.3. Each Club must appoint a Public Relations Officer to promote the interests of their rally, who should work in close liaison with the Promoters and the Championship Media Co-Ordinator. Each club to furnish the Media Co-ordinator with event information as soon as it is available. Full seeded entry list and Time and Distance Schedule must be issued to the Media Co-ordinator no later than the Thursday week prior to the event, Media Co-ordinator to be advised of any withdrawals.

Media Co-ordinator contact details:
Martin Walsh,
Mobile 086 8212023,
e-mail: mwmsport@yahoo.co.uk

8.4. The championship logo must be included on the front of all event regulations. Each club must ensure that the sponsor is given advertising space in the event programme, as follows: full A4 colour page and Championship logo on the front cover (artwork to be supplied by the sponsor).

ARTICLE 9. RESULTS.

9.1. Interim results will be available on the MI website, www.motorsportireland.com, following each round. They will also be available via email. Competitors wishing to avail of this service should furnish the Registrar with their email address when they register.

9.2. Provisional and final results will also be posted on the Motorsport Ireland website. Competitors wishing to receive a hard copy through the Post should inform the Registrar.

ARTICLE 10.

10.1. Two Championship decals will be provided by the Registrar to each registered competitor. These must be fixed to the competing car in a position approved by the Registrar. Championship decals must remain affixed to the competing car for the duration of each counting round. Breach of this regulation will result in no points being awarded to the relevant competitors.

10.2. The Top 3 overall in the final classification of the event (driver and navigator), along with the driver and navigator winning the Group N and the 2 wheel drive category, shall wear the caps provided by the championship organisers for any interviews, photographs, video recording, or any other publicity or promotional purpose required by the sponsor. The winner's caps will be provided by the sponsor. Failure to do so may result in a fine of €500 being imposed by the Registrar, and/or the loss of Championship points for that event. Before deducting championship points from any competitor the Registrar of the championship will consult with the Motorsport Ireland National Stages Subcommittee.

10.3. Regional Championships consisting of rounds of the National Championship and Events forming part of the National Championship shall not have a conflicting Title Sponsor to that of the National Championship Sponsor.

10.4. Events who wish opt out of the National Rally Championship will do so for a period of 3 years and will be reviewed on year 3 for inclusion the following year.



FOREST RALLY CHAMPIONSHIP

APPENDIX 32

2012 NATIONAL FOREST RALLY CHAMPIONSHIP REGULATIONS

ARTICLE 1. CHAMPIONSHIP EVENTS.

28 January	Donegal Forestry Rally
3 March	Carrick-on-Suir Wm. Loughman Forestry Rally
15 April	Munster CC Moonraker Forestry Rally
19 May	Cork Jim Walsh Forestry Rally
8 July	Tipperary Sean Conlon Memorial Forestry Rally
4 August	Maiden City Dogleap Forestry Rally
22 September	Omagh Bushwhacker Forestry Rally

ARTICLE 2. ANNOUNCEMENTS.

2.1. The National Forest Rally Championship is promoted by Motorsport Ireland and the National Forestry Sub Committee hereafter referred to as the 'Promoters'.

2.2. The Championship will at all times be run under the International Sporting Code of the FIA, the General Competition Rules of the Motorsport Ireland year book, these Regulations and the Supplementary Regulations issued for each event in the series.

2.3. Organisers of qualifying events retain the sole right to select and accept entries for the event and the Championship cannot be held responsible for the refusal of an entry. It is a competitor's responsibility to obtain event regulations and make his/her entries.

2.4. For event award purposes the organisers of each qualifying round are required to provide results for first second and third over all and for all classes, (article 5.1) for each Championship round. Organizers may sub-divide classes if they wish, or include additional classes.

2.5. From time to time the Promoters may issue a Competitors Bulletin **by email** to all Registered Competitors. **Such Bulletins will also be included in the monthly MI Bulletin.** This will have the same force as these Regulations.

2.6. These regulations supersede all previously published regulations and have been approved by the Motorsport Ireland Rallies Committee and the Motorsport Commission.

ARTICLE 3. ELIGIBILITY.

3.1. Eligibility for the Championship is reserved to those who hold suitable Competition Licenses issued by MI or the MSA.

ARTICLE 4. REGISTRATION.

4.1. Registration for the Championship **may be** free. Registration forms must be completed by both driver and navigator. Competitors may register for the Championship up to and including the **5th** round.

4.2. Deleted

4.3. Competitors must display championship or sponsors decals, as required by the promoters, clearly on **specified areas** of the car in **a** location above the wheel line. If sponsors decals are not on competitors cars or competitors refuse to carry decals championship points cannot be scored for that event.

4.3.1. Door plate to be placed at the top of each door panel. To measure no less than 500 mm x 130 mm.

Championship sponsor details and Logo to be placed on the top half of each plate.

Car number in Day Glo Red to be no less than 290 mm x 190 mm to be placed on each rear quarter glass.

4.4. Organising Clubs Responsibilities.

4.4.1. Accommodation on the night prior to the event in Rally Headquarters for the Championship Registrar and Chairman for two people plus evening meal (i.e. 2 rooms) and rooms for sponsors and PRO.

4.4.2. The club must issue Final Instructions to Competitors in the week leading up to the event.

4.4.3. The club must provide a suitable location for registration to be agreed with the Registrar. Registration will take place between 6.30p.m. and 9.00p.m. at rally HQ on the evening prior to the event and for one hour finishing at the actual time of Drivers Briefing on the morning of the event at rally HQ.

4.4.4. The club must provide sufficient copies of the up to date championship results for distribution on the event.

4.4.5. Once the results are finalised the organisers must provide to the registrar the following information:

1. A full list of starters
2. A full list of overall results
3. A full list of class results

4.5. The interpretation of these rules in case of doubt, shall rest with the Registrar in consultation



FOREST RALLY CHAMPIONSHIP

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where necessary with the Motorsport Ireland National Forestry Rally Sub Committee. This decision will be subject to the approval of The Motorsport Commission of MI, subject to the overriding right of appeal under the GCR's of Motorsport Ireland.

4.6. The Championship Registrar is:
Trish Hogan
Ardsallagh
Youghal
Co. Cork
Mobile: 086 1233742
Email: trisean@eircom.net

The Chairman of the Promoters is:
John Quill
2 Patricks Terrace
Blarney, Co. Cork
Mobile: 087 929 9590
Email: johnsquill@gmail.com

4.7. Scrutiny Requirements.

Scrutiny for all competing cars as per Appendix 2 MI Yearbook. All zero cars for forestry rallies must be rally cars (only) and must comply with Appendix 2 for scrutiny and Appendix 25 Article 11. Zero Car.

4.8. Priority Entry 7 days prior to the closing date for entries to the event for championship registered competitors.

4.9. To be eligible as a counting round, the route of each event must comprise of not less than 60 stage km and not more than 115 km per day as defined in the published roadbook. No more than 6 stages should be used and no stage to be used more than twice. All stages over 13 km must have a doctor, Rescue, Ambulance, located at a mid point with a deputy or assistant stage commander who must be in radio contact. The maximum required speed for loose surface stages shall not be more than 113 km. Speed limitations Appendix 25 MI yearbook.

4.9.1. The minimum stage distance may only be reduced in unforeseen circumstances and in consultation with the MI appointed Safety Officer.

4.10. Class **1, 2, 9 and 10** to be seeded together as per perceived performance and run in reverse order ahead of the main field.

A licenced Official in a 4x4 must then clear the stage to run ahead of the 00 cars and main field.

ARTICLE 5. CLASSES.

5.1. The Championship will be divided into **eleven** classes as follows:

Class 1: Cars up to 1450cc. (See 5.2.1. below)

Class 2: Cars from 1451cc to 1650cc. with only 2 valves per cylinder (See 5.2.1. below)

Class 3: Cars from 1451cc to 1650cc. with more than 2 valves per cylinder (See 5.2.1. below)

Class 4: Cars from 1651cc to **2100cc** with 2 valves only per cylinder (See 5.2.1. below)

Class 5: Cars **2101cc** to 3500cc including all two wheel drive cars with more than 2 valves per cylinder over 1651cc, V8's normally aspirated. (See 5.2.1. below)

Class 6: 4wd Four wheel drive cars excluding all WRC cars in this class must comply with their last published homologation papers. FIA Appendix J Including: Metro 6R4's as per Group B. 2777 homologation papers and the following: Maximum 2800cc, with 65mm plenum restrictor or 2500cc with trumpeted induction and must run with MSA certified engine only. The engine certificate must be presented to each event scrutineer.

Subaru Legacy Homologation Nos.A5399 and N5399 may be fitted with a Turbo charger fitted normally to Subaru Impreza and complying with Homologation No. A5480 or N5480.

Ford Escort RS Cosworth, homologation No. A5466 and Ford Sierra Cosworth 4x4 homologation number A5414 may run with nonhomologated gearbox. No other modifications are permitted and the car must comply with the homologation papers.

Additional cars authorised at the discretion of Motorsport Ireland in advance may also be allowed to enter this class but must not exceed 3500cc.

Class 7: All WRC cars only must comply with their last published Group A homologation papers as per FIA Appendix J.

Class 8: Group N Four wheel drive complying with their last published homologation papers as per FIA Appendix J.

Class 9: Junior Class: This class is open to drivers born on or after 01.01.85, driving cars up to 1650 cc, 2 wheel drive, normally aspirated.

Class 10: *R2 homologated cars.*



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Class 11: Historic cars see Appendix 29 Articles 17 & 21.**5.2. Notes on Classes.**

5.2.1. It should be clearly understood that if the following text does not clearly specify that you can do it you should work on the principle that you cannot, please check with MI 34 Dawson Street Dublin 2. Tel: 01/6775628.

5.2.2. Cars in classes 1 to 5 and class 9 may be modified to an extent not permitted for cars homologated in groups N and A. Furthermore cars with Logbooks issued after 31/12/2002 must comply with the following in addition to complying with notes 1 to 6:

Engine to remain in original location

Engine cc may not exceed that of the vehicle model as supplied by the manufacturers as original equipment (or homologated by FIA for the vehicle) by more than 25%, to an absolute maximum of 3500cc.

Engine block must be of a type fitted by any manufacturer as original equipment in a passenger car, or an engine block derived therefrom.

Motor cycle/motor cycle derived engines are not permitted.

The bodysell must comply with manufacturer's specification (or homologated by FIA for the vehicle).

For the purposes of these regulations bodysell comprises rolling shell / car frame complete, including front and rear wings / rear quarter panel, together with outer door panels and doorframes. Bonnet and boot lid may be fibreglass/composite but must have internals/bracing similar to original bonnet/boot lid and must be fastened by way of four bonnet pins (two front and two rear), or by original type hinges and with two bonnet pins.

If door trims are removed they must be replaced with sheeting made from metal at least 0.5mm thick, or from carbon fibre at least 1mm thick or from another solid and non-combustible material at least 2mm thick.

Front wings and rear wings / rear quarter panels may be modified solely for the purposes of fitting wheel arches.

Bonnet vents/scoops are permitted provided they are homologated by any manufacturer or were/are fitted as standard by any volume motor vehicle manufacturer on type approved vehicles.

Drive wheels to remain as manufacturers. e.g. front wheel drive to remain front wheel drive rear wheel to remain rear wheel drive. It is permitted to cut out a part of the bulkhead

situated in the engine compartment for the fitting of engine and transmission parts including the position and hole for the passage of the gear box the gear box control in the bulkhead and transmission tunnel. However such cut outs must be strictly limited to those parts necessary for this installation (see drawing appendix J 255-6 and 255-7. Chain drive not permitted.

Additional cars authorised at the discretion of Motorsport Ireland in advance may also be allowed to enter classes 1 to 5.

5.2.3. All cars must comply with MI vehicle and safety regulations as published in the current Yearbook, including the requirement for all cars to have a competition Logbook.

5.2.4. To determine the Class for cars fitted with **forced induction** a multiplication factor of 1.7 **for petrol engines and 1.5 for diesel engines will be applied to determine engine capacity.**

5.2.5. The original method of induction as specified by the manufacturer must be retained (normally aspirated or forced induction).

5.2.6. The onus is at all times on the Entrant/Competitor to enter the correct class.

5.2.7. A driver may nominate one vehicle and class for scoring on each event. Points will not be transferable from class to class.

ARTICLE 6. PENALTIES.

6.1. Should any car be found ineligible for the Class entered on any Championship round, all points scored by both crew members to date shall be forfeited and that event has to be used as a counting round for Final Results purposes.

ARTICLE 7. SCORING.

7.1. The marking system shall be as follows:

Each Starter will be awarded 1 point in both overall and class allocation.

Points awarded only for overall finishers, positions 1 to 18 as follows: 20, 18, 17, 16, 15, 14, 13, 12, 11, 10, 9, 8, 7, 6, 5, 4, 3, 2.

Each finisher thereafter will be awarded 1 point.

Any competitor who contests 5 rounds will receive 5 bonus points in class and overall scoring. These points will be included as and when the competitors have reached five events.

7.2. Competitors in classes 1, 2, 3, 4, 5, **9, 10 and 11** will in addition, receive a separate set of points based on the overall structure (7.1).

This is a 2 wheel drive category.



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7.3. Points will be awarded to registered competitors in all classes with a minimum of FIVE championships registered starters as follows:

1st	10 points	6th	5 points
2nd	9 points	7th	4 points
3rd	8 points	8th	3 points
4th	7 points	9th	2 points
5th	6 points		

Where in any event there are less than five championships registered starters for any class the points will be allocated according to the number registered for that class in the championship, subject to a minimum of five registered in any class.

7.4. Overall ties will be decided in favour of the competitor with the greatest number of maximum scores; failing this the greatest number of second points finishes, and so on down to eighteenth points scored until the tie is resolved. If any further ties remain they will ultimately be decided on the basis of performance in the sequential order of rounds.

7.5. Class ties will be decided in a similar fashion by considering points down to tenth registered Competitor in Class.

7.6. For the purposes of both Overall and Class Championship points, competitors best **five** scores will count.

7.7. When results for the championship are posted provisional competitors have 7 days to lodge queries with the championship register. Results will become final 7 days from the date they are posted.

7.8. In the event of any round not running the Promoters will issue a bulletin.

ARTICLE 8. AWARDS.

8.1.

	Driver	Co-driver
1st Overall	Dr. Jackson Memorial Trophy	Ger Fahy Memorial Trophy
2nd Overall	Trophy	Trophy
3rd Overall	Trophy	Trophy
1st 2 Wheel Drive	Trophy	Trophy
2nd 2 Wheel Drive	Trophy	Trophy

3rd

2 Wheel Drive

Trophy Trophy

1st in

Each Class

Trophy Trophy

2st in

Each Class

Trophy Trophy

3st in

Each Class

Trophy Trophy

Best

Newcomer

Trophy Trophy

8.2. No Competitor shall be eligible for more than one of the Championship awards listed above. Overall awards shall take priority and Class awards shall then be awarded to the next eligible competitor.

8.3. To qualify for an award competitors must start a minimum of three rounds.

8.4. Additional awards may be presented at the discretion of the Promoters.

8.5. Best Newcomer shall be decided at the organisers discretion.

8.5.1. Deleted.

8.6. 2 Wheel Drive Awards. The top three in the 2 wheel drive category (7.2) will receive first, second and third 2 wheel drive awards, competitors are reminded that article (8.2) will apply.

8.7. Championship prize giving is on **3rd November 2012** at a venue to be confirmed.



NAVIGATION TRIALS

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STANDARD REGULATIONS FOR NAVIGATION TRIALS

1. THE EVENT.

The event is an open Navigation Trial, held under the International Sporting Code of the FIA, the General Competition Rules of Motorsport Ireland (MI), these supplementary regulations and any final instructions before the start of the event.

2. ENTRIES.

2.1. Only fully completed entry forms will be accepted. No telephone, late or unpaid entries will be accepted.

2.2. Deleted.

2.3. The number of starters is limited to 50.

3. ROUTE.

3.1. Competitive navigation may not start before 21.01 hours. There will be no competitive navigation sections during daylight hours (Daylight is the period between: half an hour before official sunrise, up to half an hour after official sunset).

3.2. Tulip diagrams must be provided by the organisers covering the section from the first Control (start venue) to the first Time Point. Competitive navigation to commence at the first Time Point.

3.3. A minimum additional time allowance of 10 minutes, in addition to the 50kph average, must be given between the first Control and the first Time Point and between the final Time Point and the final Control.

3.4. Where the route passes through 50kph / 60kph zone the average speed for the entire section cannot be more than 32kph / 20mph.

3.5. The first Time Point must be located at least one mile from a built up area and not within a 50kph / 60kph zone.

3.6. The use of National Primary and National Secondary roads is discouraged. Where a National Primary or National Secondary road is used, the entry must be by left turn only. The exit from a National Primary or National Secondary road must be clearly marked and manned by a marshal / Time Point.

These requirements shall not apply in the case of sections not involving competitive navigation.

3.7. Organisers may only use 1:50,000 scale maps when setting out a route card but competitors have the option of using other maps during the event.

3.7.1. Average Speed is to be the average speed calculated from the time allowed to competitors to cover a certain distance. This distance shall be determined from 1:50,000 Ordnance Survey maps along the route specified by the organisers, or if no route is shown by the OS map, it is to be measured physically on the Ground.

3.8. All map references will be given to six figures on the National Grid system (eastings followed by northings). The Edition and Year of the Ordnance Survey maps and the make of Romer used will be declared by the organisers. Grid references given to competitors must be derived using the official Romer for the event. GPS co-ordinates may not be used to calculate such references. **Grid References should be given to 6 figures, with increments of no less than 0.5 if deemed necessary. Where greater accuracy is required, a clear diagram must be provided.**

3.9. There will be Controls, Time Points, ITC's and Vias, through which the competitor must pass in the order in which they appear on the route card. A control, Time Point, ITC or Via shall be deemed to extend for a radius of 50m around the actual point specified in the route instructions. A competitor is deemed to be at the point if within this radius. Alternative routes within this radius are not valid for the purpose of determining correct approaches and/or departures. **Where direction changes occur within this 50m radius, which might cause confusion, either a clear diagram must be provided, or alternatively the approach /departure direction may be quoted as, for example "N>NW", meaning North approach initially (I.E. @50m), and then NW nearer the point, or, in case of departure, meaning North Departure initially, then NW at 50m. Where there is a requirement to pass at an earlier or later stage of an event through a different junction less than 50m away, then a clear diagram must be provided. This diagram shall provide clarity as regards limits within the 50 m radius in relation to boundaries for double visits.** The official must be sited on the approach or departure road to or from each point. If officials are not present at a scheduled manned point during the whole period when competitors may report, all performances at such point will be ignored in compiling the results. Performances at a point not sited at the location specified will also be ignored.

3.10. Should "Plot and Bash" or Tulip diagrams be used on an event, organisers must ensure that the



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same be accurate and avoid ambiguity. Plots must not be chosen at locations where there are other junctions within **100** metre radius. The maximum number of "Plot and Bash" references to be used is 4 per half on long events (those with a mid-point halt) and 6 on shorter events (those without a mid-point halt). Where a route instruction which includes tulip diagrams incorporates unmapped roads, the end point of the route instruction must be given as a grid reference.

3.11. All Out of Bounds, Non-goers etc. must be given with the initial route instructions, and may not be given with "Plot and Bash" references. The issuing of grid references not relevant to the route is forbidden.

3.12. The start and finish of unmarked roads, which are part of the Trial route, **and roads shown on the map which indicate the shortest route but which are in fact non-goers must be listed on the preliminary Route Card. The only exception to this is where unmarked roads are used as part of a tulip diagram.** The direction of travel at any change of direction / junction where the intended route is unclear on an unmarked road must be indicated to competitors (eg. by Tulip diagram, arrows, etc.). Where an arrow is used, it must be placed on the approach to the change of direction / junction, and immediately before the change of direction / junction. The intended route across open ground must be clearly arrowed.

3.13. Deleted - see 3.17.

3.14. All fords falling within the route must be proven to be passable within 30 minutes before passage of the first car. A marshal must be positioned at the ford to re-route competitors if necessary.

3.15. Any map displayed by the organisers at the start venue should indicate only the location of the various Time Points / Via's etc and must not show the direction of travel or intermediate details of the route between points.

3.16. An official Course car must visit all Controls, Time Points, Vias and ITCs **approximately** 15 minutes before the due time of car number 1 for the purpose of checking that marshals are in position and their watches are set to the correct time. The course car crew must have a timecard completed by each marshal for practice and this time card must be displayed with competitor time cards on the clothes line system. **Organisers may share this duty between two "leapfrogging" course cars, but all points must be visited, and a timecard completed at each point.**

3.17. Each competitor will be issued with a routecard, which will contain details of the entire route (except as provided for at 3.10 above), at least 75 minutes before the due start time (90 minutes where the number of plots on the route card exceeds 50, 105 minutes where the number exceeds 60). Both the distance and the time allowed between the points and controls must be indicated on this route card. Any plots which do not define the route e.g. cautions, gates, rough road, NG's etc., must be given to competitors on a preliminary Route Card 60 minutes before any other route instructions are issued.

3.18. All route instructions should be legible, accurate, **and must be immediately apparent and visible once received. They must come complete with instructions as to how they are to be deciphered. Irrelevant information is not permitted. Route instructions must not be contained in envelopes.** Cryptic or unexplained instructions which require the competitor to stop in order to plot the correct route are forbidden. The only exception to this rule is that plotting using standard grid references can be issued at time points located at junctions, where the correct departure has to be determined. **Note that this departure direction does not count as one of the permitted plot and bash references in 3.10.** An experienced senior club official should ensure that any plot & bash used is compliant.

3.19. The length of Navigation Trials shall be: Minimum 96 km / 60 Miles, Maximum 224 km / 140 Miles.

3.20. A refuelling halt is to be provided for any event over 136 km / 85 Miles in length (a long event).

3.21. Organisers should use a 'NO' board to prevent competitors becoming unnecessarily bogged down on a non-goer. The 'NO' board should be placed such that the competitor can turn in safety.

3.22. Final instructions may not contravene GCRs. **However, clarifications and additional requirements in relation to event-specific issues may be issued as part of final instructions. Non-compliance with these requirements may be the subject of a penalty under 21.24 below but no other penalty.**

3.23. At no time during an event should competing cars be required to travel in opposite directions over the same piece of road unless the time schedule precludes any possibility of overlap, or unless the piece of road is of a broad two lane type adequate for two-way traffic. Full details of any two-way traffic must be given to



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competitors before the start of the event. *In addition, organisers must take steps to prevent, by means of arrows, "stop" signs or otherwise, competitors departing from the correct route at locations where this is likely to happen, and where such departure would result in two-way traffic on roads unsuited to same.*

3.24. *An official who is familiar with the route must be available until the last competitor has left the start control.*

3.25. *An Official Course car (2 wheel drive non jeep type) must prove the Trial route within 12 hours of the start and must report back to the start no later than 30 minutes before due departure time of the first car.*

3.26. *Likewise a sweeper car should traverse the Trial route after the last car, to check that all competitors have safely covered the course. Marshals should stay in position until the sweeper car has passed. If it is not possible to have a sweeper car, the Marshal should drive the route between him and the following point as soon as the last car has passed.*

3.27. *The last timed checkpoint before the final control (at the finish) must be designated as a control.*

4. ELIGIBILITY.

4.1. Standard road-going vehicles only, *including 4 wheel drive "jeep type" vehicles, will be eligible. All commercial vehicles are excluded.* It is the responsibility of the driver (not the scrutineer) to ensure that the vehicle complies in all respects with the Road Traffic Acts. Limited safety modifications are permitted as outlined in Article 23.

4.2. The maximum engine capacity must not exceed 2000cc. Petrol turbo cars are not permitted.

4.3. All nominated drivers must hold a valid FIA Driver's Competition Licence and all navigators/co-drivers must hold a valid FIA Driver's or Navigator's Competition Licence, which must be presented for inspection at the start of the event. Drivers must also produce a current RTA driver's licence.

5. PERSONNEL.

5.1. Each competing car shall carry only two persons; one, the nominated driver, and the other, the nominated navigator.

5.2. The nominated driver must drive throughout the event.

6. INSURANCE.

6.1. Drivers must possess valid Road Traffic Act Insurance covering their driving on the event. (GCR No. 78).

6.2. Drivers must produce a valid letter of acceptance from the insurers of the Irish Rally Drivers Scheme or BRDS equivalent.

7. INDEMNIFICATION.

Both driver and navigator must sign an indemnification clause on the entry form and may be required to sign a further indemnification form at the start of the event.

8. TIMING.

8.1. Timing will be by Marshals' watches which must be capable of displaying seconds. All due times for competitors at Controls and Time Points will be indicated on the Time Cards. *In all cases except during (but not start and finish of) Speed Regulations Sections and Regularity Sections, timing will be to the last elapsed minute.* "Timing on sight" is only permitted for Speed Regulation Sections and Regularity Sections (Retros).

8.2. The maximum average speed between any two consecutive timed points is 50kph.

8.3. Time can only be taken back, on the competitive route, at specified Time Recovery Sections (TRS) and the mid-point halt.

Organisers must include at least two TRSs per half on long events (those with a mid-point halt) and a total of at least two TRSs on shorter events (those without a mid-point halt). These must be spaced so as to ensure maximum benefit to competitors. Time Recovery Sections are to be c 0.4 Km (0.25 miles) in length with a minimum time allowance of 7 minutes, maximum 12 minutes and defined by Time Points.

Competitors may take back 75% of the time allowed (fractions rounded down). "Take back" time must be specified to competitors on the initial route card before the start. A TRS must immediately precede the start of each Speed Regulation Section (SRS) except where a SRS commences at the first Time Point of the event or immediately after a "mid-point halt" where Article 8.8. applies.

8.4. The Chief Marshal/Clerk of the Course must ensure that all marshals set their watches to the official event time.

8.5. A Control, Time Point, VIA or ITC is deemed to be open 15 minutes prior to the due time of car number 1.

8.6. Where a Time Point is unmanned, a competitor may leave that point at the beginning of his/her due minute (subject to App 33, Article 11.1).

8.7. Where competitors are deemed not to have followed the intended route, e.g. not visiting consecutive timed points or taking a wrong



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approach and/or departure, they can subsequently rejoin the intended route without penalty under App. 33, Article 21.2.3 to 21.2.5.

8.8. On events with a "mid point halt", competitors can leave on their original scheduled departure time without incurring any penalty under App. 33, Article 21.13. The COC should confirm this with competitors before the start of the event. Where the Mid-point halt is scheduled to last for 40 minutes or longer, the organisers may require that competitors leave on their original scheduled time. This must be included in final instructions.

9. ROUTE CARDS.

Deleted - see 3.7. and 3.24.

10. TIME CARDS.

10.1. A minimum of two emergency telephone numbers, one of which must be a land line, must be printed on all Time Cards.

10.2. Time cards **must be issued not later than with the main** route card. The onus is on the competitor to ensure that each time card is fully completed by the marshal and that all entries on the card are legible and authentic. Non-compliance may result in penalties (eg. for failing to visit a designated point). Clubs must issue clarification with final instructions as to whether time cards or marshals' sheets will take precedence in the event of a dispute. Where no clarification is issued, competitors' time cards will take precedence (except for instances where it is not possible for a judge of fact to record an infringement on the competitors' card, e.g. out of bounds, wrong departure, double visit etc.).

10.3. Times of arrival at Controls, Time Points and Intermediate Time Checks should be recorded on the Time Cards by the marshals.

10.4. No time will be recorded at Vias.

10.5. Noise Fails are to be recorded on the Time Cards by marshals.

10.6. A Stop/Yield sign Marshal must sign the time cards of each competitor in the space specified if they pass through the junction, indicating clearly whether or not they are being penalised.

11. MARSHALS.

11.1. Marshals, including Stop Sign Marshals, will be the sole judges of fact.

11.2. Any competitor reported to the Clerk of the Course by a Marshal as being abusive, discourteous or unco-operative will be excluded from the results.

11.3. All official cars at controls, time points, ITC's and Vias must be clearly identifiable (App.25, Artl. 4.3). Marshal's identification boards should be used.

12. NUMBERS.

12.1. Competitors must display numbers as, supplied by the organisers, on the leftside rear window.

12.2. Those competing in Championships may be required to carry decals on their car, and this should be included in the regulations for individual trials.

13. FORCE MAJEURE.

Force Majeure will apply to everything that is beyond the Organisers' control and will not be considered once a car has started the event. In the event of force majeure, the Organisers will make no alterations to penalties incurred by competitors. **However, if a point is subsequently scrubbed because of an incorrect route instruction on the part of the organisers, no time penalty will be incurred at the next time point but accumulated time penalties will be carried.** Distances shown by the organisers are deemed to be correct. **At the organisers option, provided that at least 66% of the route has been completed, results may be calculated on the basis of performance up to and including the timed point previous to a point for which incorrect route instructions were given, the remainder of the route being disregarded.**

14. STOP SIGNS.

14.1. The Clerk of the Course must man at least three stop or yield signs during the event, using senior experienced marshals. This rule is to be implemented regardless of the number of marshals available to the organisers.

14.2. Competitors must stop, ie. all forward motion must cease, at the point of entry to any road protected by a Stop, Yield, Stad or Geill Sli sign **or Road Marking. The point of entry is defined as a line, real or imaginary, which defines the border of the road which the competitor is about to join. The precise location at which this cessation of motion is required shall be at, or no more than 1m in advance of (i.e. before) the stop line, where one is provided, or shall otherwise be at, or no more than 1m in advance of, a clearly identifiable marker (such as traffic cone or a suitable sign mounted on a stake), which marker must be consistent throughout an event, and an example of which must be shown at drivers briefing.** The Stop Sign Marshal must be positioned such that he can observe the point of entry therefore he must be positioned at or near the point of entry onto the departure road.

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14.3. Any Stop or Yield sign which is manned must be CLEARLY visible to competitors from the correct direction of travel. **Where exiting from an unmapped road, a suitable advance marker shall be placed c. 50m in advance of any stop location.**

14.4. Before the start of an event the means of identification (e.g. Board, Jacket) etc. of a Stop Sign Marshal must be shown to competitors by the organisers.

14.5. The Onus is on the competitor to stop at a Stop Sign Marshal for the purpose of receiving a signature in the specified space on his/her timecard if requested.

14.6. Deleted - see 21.16.

14.7. Stop/Yield Sign Marshals may move from point to point along the route at his/her own discretion but, they must be present during the whole period when competitors may pass through the junction where the sign is located.

14.8. Deleted.

14.9. Deleted.

14.10. **At junctions not protected by signs as per 14.2, organisers may nonetheless require competitors to stop by placing a stop sign on the approach road or lane.** This stop sign must be shown to competitors at drivers briefing and may be manned for the purpose of stop sign penalty monitoring by an identifiable Stop Sign Marshal.

14.11. Should an organiser wish to marshal a Stop Sign on a SRS, this can be done only when combined with an ITC.

14.12. Deleted - see 14.2.

14.3. "Stop sign" penalties will not apply at roundabouts or on sections of road where the required average speed is 32 kph or less. However, Competitors are required at all times to comply with the Road Traffic Acts, and may be penalised for failure in this regard as per 21.24 below.

15. CLASSES.

15.1. There will be a total of three class divisions, as follows:

- A Expert
- B Semi Expert
- C Novice

Organising clubs may include a beginners class if desired.

15.2. The classification of navigators for Regional Championships will be determined by the

relevant Regional Championship Registrar in consultation with the Motorsport Ireland Navigation Sub-Committee.

16. RESULTS AND TIES.

16.1. The time and location at which results will be posted must be specified in the final instructions. Results should be available within 2 hours of the last competitor finishing the event. If results are not available at this time, an amended time must be notified to competitors. Prizegiving should not be unnecessarily delayed after the publication of results.

16.2. Ties will be resolved by the following principles:

16.2.1. The lesser experienced crew should take precedence (i.e. novice beats semi-expert beats expert).

16.2.2. Furthest cleanest.

16.2.3. Lowest penalty on Speed Regulation Section.

16.2.4. 2WD beats 4WD.

16.2.5. Lowest engine capacity.

17. SELECTIVES.

Selectives are prohibited on Navigation and Retrospective Trial events.

18. SPEED REGULATION SECTIONS. (Not regularity sections)

18.1. Speed Regulation Sections (SRS) will be set at 30 mph (0.5 mile/minute) unless otherwise specifically stated. They will be no more than 32km (20 Miles) and no less than 5km (3 Miles) in length. The Clerk of the Course is required to include at least two Speed Regulation Sections in the route for events greater than 85 miles in length and at least one SRS for event of shorter length. SRSs will be located on tarmac roads and will include the most PR sensitive sections of the route. National Primary roads **must** not be used for SRS.

18.2. Competitors must be supplied with the following pieces of information: length of section, time allowed, and average speed if different from 48kph / 30 mph. Where the average speed is other than 48kph / 30 mph, then the organisers must provide a speed table. Where the average speed is other than 48kph / 30mph, this information may be contained on a supplementary route card. The average speed should not be set at less than 32 kph / 20mph at any part within the SRS.

18.3. Timing will be to the second. Penalties will be applied for every 10 **whole** seconds early or late (ref: Article 21.3, 21.4).



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18.4. Types of time control are:

18.4.1. Time Points at the start and after the finish of the SRS, operate as for normal time points. Seconds are not to be recorded at these points

18.4.2. Intermediate Time Checks.

These are secret checks established by the organisers to check the competitors' average speed. The ITC is established at a precisely measured distance and timing is on sight. The distance from the start of the SRS to the ITC marshal will be a minimum of 5 km (3 miles) and a maximum 32 km (20 miles) in length. The exact point at which vehicles are timed must be clearly defined as demonstrated at drivers briefing. ITCs must be clearly indicated at the point of the check.

18.5. Each SRS will contain only one ITC. This ITC will denote the end of the SRS. The minimum distance between this ITC and the next time point is 3.2km (2 miles). **The required average speed must be maintained for the full length of a SRS.**

18.6. Where competitors are required to stop for the purposes of recording time, the time due at any Time Point or Intermediate Time check in that section shall be determined by the time recorded at the immediate preceding Time Point.

18.7. Previously carried lateness may not be made up during a SRS (up to and including the finish Time Point of that section) but may be made up outside SRS at TRS's and "midpoint" halts as per normal rules. If lateness is made up during a SRS (up to and including the finish Time Point of that section) penalties will be incurred.

18.8. Earliness must be carried during the SRS (up to and including the ITC of that section) but competitors must check into the next Time Point after the SRS at their due time.

18.9. Competitors are required to stop and have their time recorded and cards signed at Intermediate Time Checks.

18.10. The **entire** route between two time points containing a SRS must be given as either a pre-plot with the initial route card or on sequential ball and arrow Tulip diagrams. In each case intermediate distances (junction to junction) must be given.

18.11. Organisers are to establish a measured distance at the start of the event which competitors can use to check the calibration of tripmeters. This must be exactly 1 mile in length and the start and finish must be clearly marked on the ground or by boards. This measured distance may or may not be a part of the trial route but in any case, its location must be given to competitors at sign-on. The organisers distances are deemed correct.

19. DRIVING & NOISE.

19.1. Driving in a manner which would bring the Motor Club and Motor Sport into disrepute will result in exclusion. Strict observance of the law is required of each competitor and of those in any car which may be servicing him/her. Competitors are reminded that the Clerk of the Course must enforce compliance with this rule.

19.2. Competitors suffering an exhaust failure must retire from an event immediately.

19.3. Any marshal considering a competing vehicle to be too noisy should fail the vehicle at that particular Time Point / Via / ITC / Control by noting on the Time Card **and on the Marshals Master Sheet.**

19.4. Any competitor receiving three Noise Fails is automatically excluded and must retire from the event immediately. A competitor who is excluded, in this manner, from more than one event during a season may be subject to further penalty.

19.5. A drivers briefing must be held prior to the start of the event. COC's must highlight driving standards, compliance with the RTA, Cautions and PR sensitive areas.

20. SERVICING.

20.1. No service / chase vehicles are permitted, except in a designated service area.

20.2. A competitor will be excluded if, in the opinion of the organisers, any vehicle with a connection with a competitor is found to have interfered with the proper running of the event, or caused a nuisance to other competitors or members of the public. Any vehicle used as a pilot or shadow car will cause such an exclusion.

20.3. The use of all two way radios, mobile and portable cell type telephones is banned while competing on an event.

20.4. The use of any form of satellite navigation systems is forbidden.

21. PENALTIES.

21.1. Per minute late at a Control/Time Point - **1 mark.**

21.2.

21.2.1. Early arrival at a Control/Time Point (excluding Start Control, first control/time point after mid-point halt, Intermediate time checks, the Time Point at the end of any Speed Regulation Section and the time point at the end of TRS) each offence - **50 marks.**

21.2.2. Per minute early at the Time Point at the end of any SRS and the time point at the end of a TRS each offence - **2 marks.**

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21.2.3. Where previous time point/via missed or an incorrect departure at previous time point/via or an incorrect approach at time point in question, competitors can rejoin route without incurring - **50 marks penalty as in 21.2.1.**

21.2.4. Where previous time point/via missed or an incorrect departure at previous time point/via or an incorrect approach at time point in question and competitors arrive before their original due time: each offence: per minute early - **2 marks.**

21.2.5. At subsequent time points where competitors arrive before their original due time (after 21.2.4.) without exceeding the 30mph average: each offence: per minute early - **2 marks.**

21.3. Per 10 seconds late at an Intermediate Time Check - **1 mark.**

21.4. Per 10 seconds early at an Intermediate Time Check - **1 mark.**

21.5. Over 15 minutes late at a Time Point - **20 marks.**

21.6. Over 30 minutes late at/or missing a Control - **Exclusion.**

21.7. Missing a Time Point or Via - **20 marks.**

21.8. Missing or being over 15 minutes late at an Intermediate Time Check - **40 marks.**

21.9. Wrong approach at Control/Time Point/Via - **15 marks.**

21.10. Wrong departure at Control/Time Point/Via - **15 marks.**

21.11. Wrong approach OR wrong departure at Intermediate Time Check - **20 marks.**

21.12. Wrong approach AND wrong departure at Control/TP/Via/Intermediate Time Check - **20 marks.**

21.13. Deleted.

21.14. Traversing a Control/TimePoint/Via/Intermediate Time Check more than once each offence - **30 marks.**

21.15. Failure to stop at Stop/Yield Signs 1st offence - **50 marks**, 2nd offence - **Exclusion.**

21.16. Failure to stop at identifiable Stop Sign Marshal or other judge of fact - **50 marks.**

21.17. Visiting Out of Bounds area each offence - **100 marks.**

21.18. Altering a Time Card - **Exclusion.**

21.19. Removing any Time card from a clothes line/results display by a competitor without express permission of the CoC or results team - **Exclusion.**

21.20. Failure to complete entire event under own power - **Exclusion**
(except that vehicles that have left the route may receive assistance to regain the route or vehicles blocking the route may receive assistance, for the minimum distance necessary, to clear the route).

21.21. Failure to pass scrutiny at any time during the event - **Exclusion.**

21.22. Passing a NO board - **100 marks.**

21.23. Use of any form of satellite navigation systems - **Exclusion.**

21.24. Breach of any requirement issued in writing by the organisers and with the approval of the stewards, and not otherwise the subject of a specific penalty herein - 10 marks.

21.25. Failure to visit timepoints in correct sequence, each offence - 20 marks.

22. AWARDS.

22.1. 1st crew overall **2 Trophies**
1st, 2nd, 3rd in each class **2 Trophies**
(depending on the number of entries)

22.2. Additional awards may be given as per supplementary regulations.

23. SCRUTINY.

23.1. All vehicles must be examined for compliance with regulations before the start. The minimum basic standard is complete compliance with Road Traffic Act requirements including those for tyres. Seat belts must be worn at all times.

23.2. All vehicles must carry a luminous red warning triangle.

23.3. Fire extinguishers are recommended.

23.4. Standard headlights may only be supplemented by two additional lights. All such lights must extinguish on dipped beam. If additional lights are fitted, any supplementary manufacturer's lights must be removed. The wattage of all bulbs in forward facing lamps must comply with the RTA. Any additional lights must be fitted so that their horizontal centre-line is not above the top of the original manufacturers headlights.

23.5. Under body protection for vehicles is permitted.



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23.6. Competition brake linings / pads are permitted.

23.7. Roll cages (where fitted) must blend in with the appearance of the vehicle.

23.8. Safety harnesses are permitted.

23.9. Suspension may be altered provided mounting points remain as standard.

23.10. Wheels must not extend beyond the periphery of the bodywork.

23.11. All vehicles must present in a manufacturer's original colour scheme subject to a maximum of two colours (excluding vinyl roof where applicable), or single colour if original colour scheme changed by respraying.

23.12. Vehicles are not permitted to carry sign writing, stickers or competition numbers, except numbers supplied by the organisers (see Art.12) and championship sponsors decals which may be placed only on the periphery of the rear screen.

23.13. Noise.
Every effort should be made to reduce noise to a minimum.

23.13.1. Exhaust system.
The permitted noise level measured at 45 degrees to the exhaust and 0.5m from the exhaust may be no more than 100 dB(A) at 3500 RPM. (2500 RPM for diesel engines) (Ref. App. 2 Art. 9)

23.13.2. Induction.
Carburettors must have filters fitted. It is the responsibility of the COC to ensure that: a) the necessary equipment is available b) the required noise tests are carried out c) the appropriate penalties are applied. See also Articles 19.2 – 19.4.

23.14. All Navigation Trials must have a noise test at pre-event scrutineering, at the half way halt, and the finish. In addition the COC shall have the discretion to include a noise test at any other point he thinks appropriate.

23.15. A competitor who fails the noise test must be refused permission to start or be excluded if the noise level cannot be brought within the prescribed limits.

23.16. Route sheets may be handed out at the start of due plotting time without the car having gone through scrutiny, but under no circumstances will a car failing to pass scrutiny be allowed to start the event.

23.17. The Vehicle Licencing Certificate (VLC) or Registration Book must be produced as **evidence** of compliance with Article 4.2 above.

23.18. A Scrutiny Record Card will be issued to all drivers from 1 October, 2006. This card must be produced at scrutiny for each subsequent event. Failure to produce the card may result in start refusal.

24. OFFICIALS.

24.1. The Clerk of the Course, appointed by the organising Club(s), should ideally be an experienced navigator.

24.2. If a new Clerk of the Course is appointed then the organising Club(s) must provide an experienced mentor to assist the Clerk of the Course.

24.3. Organisers must ensure that all Officials, Marshals, etc. have signed on at the start of the event and have been provided with a copy of the benefits under the personal accident insurance policy for officials and that all entrants, drivers and passengers have signed an indemnity.

25. STEWARD.

25.1. The appointed Steward must drive a minimum of two competitive sections of the route checking the following:

25.1.1. The actual distance of the competitive sections.

25.1.2. The actual time allowed as per the Time Card/Route Card.

25.1.3. The accuracy of the marshals watches. The results of these checks are to be appended to the Stewards Report.

25.2. The appointed Steward must also inspect the letters issued by the organising Club(s), in relation to the event, to both the residents along the route and to the Gardai.

25.3. The appointed Steward must also check the use of "NO" boards.

25.4. Where an event receives a negative comment from the appointed Steward regarding the route and timing then the next event run by the promoting Club(s) will be required to have the route and timing inspected and approved by the appointed Steward for that next event. Such inspection to be carried out at least two weeks prior to the proposed date of the event.

26. PUBLIC RELATIONS.

26.1. Residents and other interested parties on and adjacent to the route who are likely to be disturbed or inconvenienced must be notified in advance about the event. Where at all possible this notice should be in writing and should enable residents and the Gardai to contact the

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organisers if necessary. Organisers should route events around towns and villages where possible, unless petrol or meal stops are planned.

26.2. All houses along the route and fringe roads must be visited.

26.3. Ideally, the resident should be spoken to but at the very least a PR Notice must be delivered at each house.

26.4. All Garda Stations in the area of the route or parts through which the route passes are to be advised, in writing, of the event at least one week prior to the event.

26.5. "NO" Boards are to be used by the organisers in sensitive areas. Competitors must not enter an area protected by a "NO" Board. Organisers must place "NO" board such that competitors can turn in safety.

26.6. The COC is required to satisfy the Steward that the foregoing has been complied with.

26.7. The Clerk of the Course and/or Stewards of the meeting **must** notify MI of any complaint of inconvenience made by members of the public and of any significant accidents, particularly those involving third parties. Such reports should be submitted within 48 hours of the complaint and should include the time and place and any other relevant details.

27. QUERIES.

27.1. Any competitor may raise a query concerning the event, with the Competitor Liaison Officer, if appointed, or with the Results Officer or Clerk of the Course as designated in the Supplementary Regulations.

27.2. Each query must be in writing, specify the nature of the query, and declaring the name, role and competition number of the person raising the query.

27.3. The time of receipt of each query shall be noted by the club official designated to receive queries.

27.4. A separate query must be raised for each individual matter being queried.

27.5. A query must be raised no later than 30 minutes following initial posting of the results. Where a query arises from reposted or revised results, this query must be raised within 15 minutes of such results.

27.6 When a query causes a change to results, the revised results must be posted on the official notice board with the time of posting noted. A period of 15 minutes must elapse

before results are declared final, unless all competitors unanimously agree to an earlier declaration.

27.7. When a query causes no change to results, the person raising the query must be informed in writing of such decision at least 5 minutes before results are declared final.

28. PROTESTS.

Protests must be made within 30 minutes of the posting of provisional or revised provisional results and must be in accordance with MI General Competition Rules Chapter 11.

29. 'RETROSPECTIVE' TRIAL - DEFINITION.

29.1. A Navigation type Trial normally for Historic Cars on Open Road with an average speed not exceeding 30 mph with penalties for being early or late.

29.2. It may have a single itinerary which must be followed by all cars or be several itineraries converging on the same Rallying Point fixed beforehand and followed or not by a common itinerary.

29.3. The Trial may incorporate Driving and Autotests, Regularity Sections.

29.4. The Trial must be run in accordance with the provisions of this Appendix generally, save where where modified by the provisions of this Article (Art 29).

29.5. Competitive navigation may not start before 21.01 hours. There will be no competitive navigation sections during daylight hours (daylight is the period between: half an hour before official sunrise, up to half an hour after official sunset).

29.6. At least 10% of the total route or 30 Miles (whichever is the greater) must consist of Regularity Sections.

29.7. The use of main / national roads is discouraged.

29.8. Dipped headlights must be used during daylight hours.

29.9. At least once in each section a Stop/Yield sign must be manned.

29.10. For daylight regularity a maximum average speed of 20mph is to apply for unsurfaced or single car width roads.

29.11. The average speed chosen for daylight regularity sections should reflect the quality / width of the road being used.



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29.12. The use of "Motorsport Event In Progress" signs is encouraged for daylight regularity sections.

29.13. All householders residing on competitive sections to be advised of the passage of the Trial – this to apply for daylight and night-time sections.

29.14. Public Relations – refer to App 33 Article 26.

29.15. Traditional "Classic Rally" style plates to be used on the front and rear of vehicles in place of door numbers.

29.16. Noise – refer to App 33 Article 23.13.

29.17. Time controls must be sited at Stop / Yield signs or more than 0.25 miles from such signs.

29.18. Classes.

Each organiser is free to choose its own classes if it so wishes, however the following are recommendations:

Class 1 - All cars manufactured before 31.12.45.

Class 2 - All Minis and Mini Derivatives first manufactured before 31.12.77*.

Class 3 - Saloon cars up to 31.12.66.

Class 4 - Sports Cars up to 31.12.66.

Class 5 - Saloon cars 1.1.67 to 31.12.77.

Class 6 - Sports Cars 1.1.67 to 31.12.77.

Class 7 - All other cars of a model that was in production before 1.1.85 to include cars not fully compliant with Classes 1-6 (at organisers' discretion).

*Specials or Cars specifically adapted for Autotests may be amalgamated with Class 2. This decision rests with the organisers.

29.19. The restriction of 2000cc and petrol turbos set out in Article 4 does not apply to Retro Trials.

29.20. It is recommended that Autotests be marked on a class basis. The fastest car in each class on any test scores 0 penalties and each competitor's penalty in his/her respective class will be the difference in time from the fastest in his/her class.

For example:

Fastest car in Class 2 on Test 1 - 40.4 secs

2nd fastest car in Class 2 on Test 1 - 43.2 secs

Fastest Car gets 0 penalties.

2nd fastest Car gets 2.8 penalties and so forth.

29.21. The nominated driver must drive all sections on the public highway. If another member of the crew wishes to drive on the public highway he/she must be in possession of a valid IRDS or BRDS letter and pay 50% of the appropriate premium before the event begins.

29.22. The imposition of penalties shall be in accordance with the following rules:

Road section:

Failure to visit a control/check - **30 marks.**

Wrong approach/departure at a control/check - **10 marks (each).**

Each minute early/late - **1 mark.**

Autotests:

Time taken per 1/10 second, ranked as 0;3;5;7;9;11;13;15 and so on Striking a pylon/marker - **5 marks.**

Line fault - **5 marks.**

Failure to perform/visit test - **MAX***

***MAX = 15 marks.**

NOTE: Tests are class based.

Retro Driving tests (all forward motion):

Each second taken slower than fastest in class - **1 mark.**

Line/pylon fault - **5 marks.**

Failure to perform/visit test - **MAX***

***MAX = 30 marks.**

NOTE: Tests are class based.

Navigation Test:

Each minute late at check point - **10 marks.**

Each minute early at check point - **20 marks.**

Failure to visit a check point or more than 15 mins late - **100 marks.**

Wrong approach/departure - **50 marks (each).**

Exceeding 30mph between any two TP

- **150 marks.**

Failure to stop at Stop/Yield sign - **300 marks.**

Maximum on Navigation Test (excluding exceeding 30mph or failure to stop penalties)

- **400 marks.**

Regularity sections:

Wrong approach/departure at check point - **10 marks.**

Failure to visit check point - **30 marks.**

Undue delay in checking in at checkpoint - **5 marks.**

Maximum in any one regularity - **200 marks.**

Maximum in any one ITC - **30 marks.**

Each second early or late at each point - **1 mark.**

Entire event:

Failure to stop at Stop/Yield sign - **300 marks.**

Second offence - **Exclusion.**

Failure to stop at a Stop Sign Marshal or Judge of Fact - **300 marks.**

Second offence - **Exclusion.**

Maximum lateness at any point - **15 minutes.**

30. NAVIGATION TRIALS RECOMMENDATIONS TO CLUBS

The following are recommendations, which have been arrived at over the years and Organising Clubs are asked to ensure that they are implemented to encourage and promote the future of Navigation Trials.



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30.1. Entry Fees should be standardised to comply with National Championship Regulations App.34.

30.2. In an attempt to encourage novices/beginners, clubs should organize navigation classes and provide for beginners awards on events. Excessively rough roads should not be used.

30.3. Beginners/Novices should be warned about the possibility that points may not be manned.

30.4. Canal banks and other dangerous areas should be shown on the route card. Caution boards should be used where necessary. Boyne Aqueduct and anything else as potentially lethal should not be used at all.

30.5. As far as possible, the same crew should not run at Number 1 more than once in a season, on successive Trials. Any crew who has demonstrated capability to win an event may run at Number 1, regardless of class. Seeding should be based on overall performance rather than class.

30.6. 1st Semi and 1st Novice to receive route cards at the same time as 1st Expert, etc.

30.7. When using plot and bash/tulip sections, a third party should be consulted, to cross check for errors.

30.8. Out-of bounds and "No" Boards should be used solely to avoid PR problems. They should not be used to define the route - Vias, even if unmanned, should be employed instead.

30.9. Deleted.

30.10. There should be a control at approximately halfway through the event.

30.11. Every effort should be made to arrange the availability of petrol at the finish. It is illegal and inadvisable to carry petrol in cans in the car, and many crews travel long distances to compete.

30.12. The 'Clothes-Peg System' of displaying every Competitor's time sheets is very efficient and recommended to all results teams. This is a standard requirement on National Championship events.

30.13. Instruction sheets should be issued to marshals, and it should be ensured that they know how to find their Time Point(s) or Via(s). Marshals should not have to rely on poor photocopies of maps to reach their destinations. They should be provided with the information, and given sufficient time to drive to the next time point when they close their current one. Naturally a sweeper car at the back would be preferable. Ideally marshals should be given the

full route, but not necessarily with all the time points marked.

30.14. All clubs should keep a list of marshals who attend their events, and send a copy to other clubs who request such information.

30.15. Competitors should be encouraged to introduce at least one marshal each, to ensure the continuance of the sport.

30.16. To encourage marshals, clubs should give a Marshal's award at the end of the event.

30.17. The route should be planned so that the majority of marshals can get to the finish in time for refreshments, etc.

30.18. It is strongly recommended that proper sheltered accommodation be provided for mechanical scrutineering at all Trial events.

30.19. Clubs are encouraged to try organizing an event with a complete route pre-plotted, to be given out at the first competitive time point at the start and mid-points.

30.20. Clubs should remind competitors at briefing of the importance, for the good of the sport, of reporting any/all damage caused during the event, Club should provide damage declaration forms to facilitate this.

30.21. To assist with the smooth running of an event it is recommended that clubs use a Competitor Liaison Officer (CLO). This person would ideally be someone who is known to competitors, who is respected by them and who has a good knowledge of the rules.



NAVIGATION TRIAL CHAMPIONSHIP

APPENDIX 34

MOTORSPORT IRELAND NAVIGATION TRIAL CHAMPIONSHIP 2011/2012

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1. GENERAL RULES.

1.1. All Navigation Trials must comply with the 'Motorsport Ireland Requirements for the Organisation and Safety of all Rallies incorporating 'Rally Code' (App.25). These requirements are designed to assist organisers and competitors, to increase the safety of the events and to keep inconvenience to non-competitors and to local residents to a minimum.

1.2. All Championship events must adhere to the "Standard Regulations for Navigation Trials" (App. 33). Any final instructions may not change the Standard Regulations, except in accordance with GCR No. 60.

1.3. The Championship is open to all holders of MI Licences and also to holders of MSA Licences of a grade not lower than Clubman.

1.4. The interpretation of these rules in case of doubt shall rest with the Registrar of the Championship, in consultation where necessary, with the MI Navigation Sub-Committee and the Rallies Committee subject to the rights of appeal to the championship Stewards as laid down in the GCR's.

2. EVENTS TO COUNT.

2.1. The Championship will be held over 6 months, from October to March and will include a total of eight events. A competitor counts points on their best performance in six events from the eight rounds towards his/her final score for the Championship. There will be a designated reserve event for each half of the the championship in the event that one of the preceding qualifying rounds does not run.

2.2. The events which qualify for the Championship are as follows:

8/9 October	Birr & District MC	2011
5/6 November	Cork MC, Startrek	
12/13 November	Co. Monaghan MC	
	Drumlin	
26/27 November	Midland MC	2012
10/11 December	Skibbereen Carbery	
	(Reserve Event)	
21/22 January	Cork MC, 1000 Shakes	2012
4/5 February	Skibbereen & District,	
	100 Isles	
11/12 February	Midland / Westmeath	
	(Reserve Event)	
3/4 March	Co. Cavan MC	
17/18 March	Co. Monaghan MC	

2.3. Should an event be cancelled or excluded for any reason and the reserve event is not run the number of events to count towards each competitor's final score will be reduced by one.

2.4. The awards for the Championship will be presented as follows:

1st, 2nd, 3rd in expert, semi-expert, novice and beginners classes - 1 trophy each.

2.5. Additional awards may be presented at the discretion of the Navigation Sub Committee.

2.6. In the Event of an over-subscribed entry on any counting round, the top four drivers and navigators in each class at the time will be entitled to priority. For the opening round, priority will be based on the final positions in the previous championship. This is subject to receipt of valid entries by the organising club within the closing date.

3. REGISTRATION.

3.1. All drivers and navigators competing in any event will be automatically registered for the Championship. (Registration forms will be available at all events, and must be completed and returned to the Registrar).

3.2. There will be a fee for registration of €3.00 per competitor per event. This registration is compulsory and will be collected by the Organising Club at 'Sign-On'.

3.3. The National Registrar is:
Stephen O'Neill, Rathnally, Trim, Co. Meath
Phone: 046 - 9437247
e-mail:stephenoneill6@gmail.com

4. MARKING SYSTEM.

4.1. Fifteen points will be awarded to the driver and navigator putting up the best overall performance in each event, with one point less going to competitors in each successive place, down to 15th place.

4.2. All competitors will gain one point for starting, and another point for finishing the trial, in addition to any points they gain due to their overall performance.

4.3. A competitor winning an event outright will be awarded one extra point.

4.4. A competitor starting a **minimum of six** counting rounds of the championship will be awarded an additional five bonus points both in class and overall. Bonus points will not be forfeited where a crew can verify that they did



NAVIGATION TRIAL CHAMPIONSHIP

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not start an event due to an oversubscribed entry list. Proof of entry must be provided by the crew in such cases.

4.5. In addition each class will be marked separately. The leading driver and navigator in each class will receive 10 points, with one point less going to each successive place down to 10th place.

4.6. A competitor or competitors whose retirement is directly due, in the opinion of the event Stewards, to an incorrect route instruction shall, at the end of the championship, be awarded points based on their average score over the course of the championship, disregarding the event in question. However the championship stewards may take into account the position of the competitor on the event in question immediately prior to the incorrect route instruction, and may accordingly reduce (but not increase) the points awarded.

5. ENTRY FEE.

5.1. The maximum entry fee chargeable for any Navigation Trial may not exceed €70 (which includes €6 Championship levy per crew). IRDS and Personal Accident insurance must be quoted separately in the supplementary regulations.

5.2. Should there be an increase in insurance fees during the season, the entry fee will be revised.

5.3. Deleted.

5.4. Competitors will be entitled to a refund of entry fee if they inform the Club of their intention not to compete before the start of the event. Fees do not include IRDS or P.A. Insurance.

6. CLASSIFICATION.

6.1. There will be a total of four Class divisions, as follows:

- 6.1.1. Expert
- 6.1.2. Semi Expert
- 6.1.3. Novice
- 6.1.4. Beginner

6.2. Final decision as to classification shall rest with the National Registrar, in conjunction with the Navigation Sub-Committee and the Rallies Committee.

6.3. Competitors in the Championship must be members of a Motor Club affiliated to MI, so that the organisers may be assisted in defining the classification of individual competitors.

6.4. The guiding criteria used shall be as follows:
BEGINNER - A navigator competing in their first season who has not competed in more than two Navigation / Retro trials as a navigator previously.

NOVICE - Any competitor, other than a Beginner, who has not navigated before **01.09.07** plus any competitor who has not been promoted.

SEMI-EXPERT - Any competitor not within the classification of a beginner, novice or expert.

EXPERT - Any navigator who has been placed in the top three in any Navigation Championship series in the past five years, plus any competitor who has been promoted or designated an expert by the Navigation Sub-Committee.

6.5. Based on the results of the previous season, the Navigation Sub-Committee will decide who is to be promoted. No Competitor may be upgraded by more than one class in one season. In each qualifying event, the class of the crew is determined by the classification of the navigator as per National Championship regulations. Once classified, a navigator (whether or not he/she is registered) may not compete in another class during the season. Re-classification may take place after each season.

6.6. It is possible for a navigator to be in a different class for a local championship to the class he/she is designated for the National Championship. This decision will rest with the Registrar in consultation with the Navigation Sub-Committee.

7. TIES.

7.1. The resolution of ties in the overall Championship will be decided on the following principles:

- 7.1.1. The lesser experienced crew should take precedence (ie. beginner beats novice beats semi-expert beats expert).
- 7.1.2. The number of wins.
- 7.1.3. The number of 2nd, 3rd, etc. placings.
- 7.1.4. The number of competitors beaten on the 1st, 2nd, etc. event.

7.2. The resolution of ties in an individual event for the purpose of allocating championship points will be decided on the following principles:

- 7.2.1. Furthest cleanest.

7.3. If a tie exists after using this criterion, then each tied competitor will receive equal points due to the highest placed tied score. The next placed untied competitor will receive points due to the place he/she would have received if there were no tie.

8. ROUTE.

8.1. As defined in "Standard Regulations for Navigation Trials" (App. 33) published in this Yearbook.

8.2. It is recommended that the minimum distance on an event be 135km. The maximum distance is 225km.



NAVIGATION TRIAL CHAMPIONSHIP

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9. COURSE AND SWEEPER CARS.

Deleted - see Appendix 33 Art 3.25, 3.26.

10. RESULTS.

10.1. Results must be posted as soon after each event as possible, at the finish. A full results board must be posted, giving detailed results.

10.2. Clubs should ensure that the finish is at a suitable venue, so that adequate time be available for the calculation of results. In order to facilitate the speedy calculation of results, it is recommended that at least 2 Time cards must be used, the first of which should be collected approximately half way through the event.

10.3. The "Clothes Line" system of showing all competitors time cards must be used.

10.4. Prizes must be presented on the night, provided that results are finalised. GCR No 166.

10.5. The promoting Club must send the results of a Trial Championship event to the Registrar not more than 8 DAYS after the event. A penalty of €15.00 per week will be incurred for any delay in forwarding results beyond the permitted period.

10.6. The promoting club must also send complete results, showing a full list of starters, finishers and details of penalties on the road, to each entrant. A penalty of €15.00 per week will be incurred for any delay in circulating results beyond the permitted period. GCR No 79.

10.7. If a registered competitor takes a prominent role in the running of a Championship event, he/she will receive 18 points (overall) and 10 points (class) as his/her score for this event, subject to a maximum of one event per Championship. Bonus points (Art. 4.4.) will also be awarded provided that the competitor starts **a minimum of five other** counting rounds. A "prominent role" has been defined as that of Clerk of the Course or Assistant Clerk of the Course. CoCs and ACoCs who are registered competitors must be agreed for the last 4 events prior to **31.12.2011** at the latest.

11. INFORMATION.

Anyone seeking further information about the Championship should contact:

Chairman of the Navigation Sub-Committee:

Paul Hughes,

Corbyfin,

Ballybay, Co. Monaghan.

Phone (042) 974 1175

e-mail: plhughes@iol.ie



ENDURANCE TRIALS

APPENDIX 35

STANDARD REGULATIONS FOR ENDURANCE TRIALS

1. THE EVENT.

The event is an open Endurance Trial, held under the International Sporting Code of the FIA, the General Competition Rules of Motorsport Ireland (MI), these supplementary regulations and any final instructions issued before the start of the event. Final instructions may not contradict / over-rule any of the above, they may only clarify / expand particularly if there is a situation unique to an event.

2. ENTRIES.

2.1. Only fully completed entry forms will be accepted. No telephone, late or unpaid entries will be accepted.

2.2. The number of entries is limited to 70.

3. ROUTE.

Competitors will be required to travel between selectives (as defined below) over sections of public road, on which the provisions of the Road Safety Acts apply.

3.1. There will be no competitive navigation sections on roads open to the public.

3.2. The competitive section of the event will take place over Selectives held on private ground. Selectives are sections of the route, timed to the second, on which the competitors are required to navigate at an average 50 kph (32 mph). The location of the Selectives and the route through them will not be made available to competitors until the start of the event.

Selectives may include:

- a) Passage controls, where the competitor has to stop to collect marshal's signature.
- b) Code Boards, where the competitor has to stop to record informations displayed on a board.

In addition, the following may be included on selectives to prevent excessive speed and to increase the element of driving skill required:

- I) Artificial chicanes, comprising two or more elements.
- II) Cones.
- III) Boxes, where the competitor is required to come to complete halt before proceeding again.

(Cones, boxes and chicanes are recommended on long straights, to keep the speed down or to slow the cars before any unduly hazardous obstacle).

3.3. The required average speed for any section of the route, including Selectives will not exceed 50 kph (32 mph). Selectives will have a minimum and maximum time.

3.4. Competitors recording a time equal to or less than the minimum (target time) required by the 50 kph average will be given the target time, which will incur no penalties but will gain no advantage.

3.5. Competitors recording a time greater than the target time but less than the maximum time will be given a penalty in seconds in excess of the target time.

3.6. Competitors recording a time greater than the maximum time will be given the maximum penalty in seconds for that Selective, i.e. the difference between the target time and the maximum time. The maximum time will be based on an average speed of 30 kph.

3.7. Tulip diagrams of the route will be provided by the Organisers.

3.8. A minimum additional time allowance of 10 minutes will be given, in addition to the 50 kph average, between the first Control and the start of the first Selective.

3.9. Organisers may use only 1:50,000 maps when setting out route card but competitors have the option of using other maps during the event (it is not envisaged that competitors will need maps, although they will be free to use them).

3.10. There will be Controls, Time Points, Passage Controls and Vias through which the competitor must pass in the order in which they appear on the Time Card.

3.11. Any map displayed by the organisers at the start venue should indicate the entire route.

3.12. A refuelling halt is to be provided for any event over 136 km (85 miles).

3.13. Organising clubs should ensure that the route is timed in such a way as to allow extra time where competitors are likely to be delayed, for example by heavy traffic or at level crossings, so that they do not have to drive at excessive speed on open roads.



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4. ELIGIBILITY.

4.1. Standard road going vehicles only, but not commercial vehicles, will be eligible. It is the responsibility of the driver (not the scrutineer) to ensure that the vehicle complies in all respects with the Road Traffic Acts. Limited safety modifications are permitted as outlined in Art. 21. Turbo and four-wheel drive vehicles are ineligible.

4.2. All nominated drivers must hold a valid FIA Driver's Competition Licence and all navigators must hold a valid Driver's or Navigator's Competition Licence, which must be presented for inspection at the start of the event. The nominated driver must hold a current RTA Driver's Licence.

4.3. For Technical Specification of Endurance Cars, please see Articles 27.1-27.35.

4.4. All other cars will be to Navigation Trial or Retro Trial specification.

5. PERSONNEL.

5.1. Each competing car shall carry only two persons, one being the nominated driver and the other being the nominated navigator. The nominated driver must drive throughout the event.

6. INSURANCE.

6.1. Driver must possess valid Road Traffic Act Insurance covering their driving on the event (GCR No. 78) or

6.2. Drivers must possess a valid letter of acceptance from the insurers of the Irish Rally Drivers Scheme or BRDS equivalent.

7. INDEMNIFICATION.

7.1. Both driver and navigator must sign an INDEMNIFICATION clause on the entry form and may be required to sign a further INDEMNIFICATION form at the start of the event.

8. TIMING.

8.1. Timing will be by marshals' watches. On Selectives, timing will be to the second. On the remainder of the route timing will be to the previous elapsed minute.

8.2. The Chef Marshal / Clerk of the Course must ensure that all marshals set their watches to the event time.

8.3. The maximum average speed required between any two consecutive timed points will be no more than 50 kph (32 mph), including Selectives.

8.4. Where a competitor is deemed not to have followed the intended route, e.g. not visiting consecutive timed points (or missing a Selective) they can subsequently rejoin the intended route without incurring penalty for early arrival, unless they check in before their original due time.

8.5. On events with a mid-point halt or rest halts, competitors can make back any lateness and leave on their original scheduled departure time without incurring any penalty.

9. ROUTE.

9.1. Each competitor will be issued with a Tulip Road Book for the entire event (including selectives) at the start of the event. This will be issued at least 75 minutes before their due start time.

9.2. An official who is familiar with the route must be available at the start until the last competitor has left the start control.

10. TIME CARDS.

10.1. A minimum of two emergency telephone numbers, one of which must be a landline, must be printed on all time cards.

10.2. Time cards will be issued with the route cards. The onus is on the competitor to ensure that each time card is fully completed by the marshal and that all entries on the card are legible and authentic. Non-compliance may result in penalties (e.g. competitors may be deemed not to have visited a designated point). Clubs must issue clarification with final instructions as to whether competitors' time cards or marshals' sheets will take precedence in the event of a dispute. Where no clarification is issued, competitors' time cards will take precedence (except for instances where it is not possible for a judge of fact to record an infringement on the competitor's card, such as wrong departure or double visit).

10.3. Times of arrival at Controls and Timed Points must be recorded on the Time Cards by the marshals.

10.4. No time will be recorded on vias, or passage controls on selectives.

10.5. Noise fails will be recorded on the time cards by the marshals.

10.6. A Stop / Yield sign marshal must sign the time cards of each competitor in the space specified if they pass through the junction, indicating clearly whether or not they are being penalised.



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11. MARSHALS.

11.1. Marshals, including Stop Sign Marshals, will be the sole judge of fact.

11.2. Any competitor reported to the Clerk of the Course by a marshal as being abusive, discourteous or uncooperative will be excluded from the event.

11.3. All official cars at Controls, Time Points, Passage Controls and Vias must be clearly identifiable. Marshal identification boards should be used.

12. NUMBERS.

12.1. Competitors must display numbers as supplied by the Organisers, on both rear windows.

13. FORCE MAJEURE.

Force Majeure will apply to everything that is beyond the Organisers control and will not be considered once a car has started the event. In the event of force majeure, the Organisers will make no alteration to penalties incurred by competitors. If a point is subsequently 'scrubbed' because of an incorrect route instruction on the part of the Organisers, no time penalty will be incurred at the next timed point but accumulated time penalties will be carried. Distances shown by the Organisers are deemed to be correct.

14. STOP SIGNS.

14.1. The Clerk of the Course MUST man at least three stop or yield signs during the event, using a senior experienced marshals. This rule is to be implemented regardless of the number of marshals available to the Organisers.

14.2. Competitors must stop, i.e. all forward motion must cease, at the point of entry to any road protected by a Stop, Yield, Stad or Geill Sli sign, whether a vertical sign or road marking.

14.3. Any such sign which is manned must be **CLEARLY** visible to competitors from the correct direction of travel.

14.4. Before the start of an event, the means of identification (e.g. board, jacket etc.) of a Stop Sign Marshal must be shown to competitors by the Organisers.

14.5. The onus is on the competitor to stop at a Stop Sign Marshal for the purpose of receiving a signature in the specified place on his / her time card if requested.

14.6. The penalty for not stopping at an identifiable Stop Sign Marshal shall be 300 seconds.

14.7. Stop Sign Marshals may move from point to point along the route at his / her own discretion but they must be present during the whole period when competitors may pass through the junction where the sign is located.

15. CLASSES.

15.1. The entry will be divided into five classes, as follows:

- a) Cars built to Endurance specification (for details see below,
- b) Navigation Trial cars up to 1400cc,
- c) Navigation Trial cars from 1401 to 2000cc,
- d) Front Wheel Retro type cars (retro cars as defined in appendix 33),
- e) Rear Wheel Retro type cars (retro cars as defined in appendix 33).

16. SELECTIVES.

16.1. A number of Selectives will be included in the route; they will be held on private ground.

16.2. Competitors will start Selectives at one minute intervals, unless instructed otherwise by the Organisers. Competitors will be given a countdown by the Selective Start Marshal.

16.3. The route through the Selective will be defined by Tulip Diagrams and / or any other means deemed necessary by the Organisers.

16.4. It is the competitor's responsibility to follow the correct route.

16.5. Apart from time penalties, penalties will also be incurred if the competitor strikes a cone, passes the wrong side of a cone, fails to negotiate a chicane as directed, fails to stop at a passage control or stop box or fails to correctly record a code board on their time card. Code information must be recorded on the time card at the point where the board is located. Missing code board information at the next manned point on the Selective or at the end of the Selective will be penalised.

16.6. At the end of the Selective, competitors must stop astride the finish line and then immediately move forward to receive a time from the Finnish Marshal. More detailed instructions regarding finish line procedure may be given in Final Instructions.

16.7. Failure to attempt a Selective, failure to complete a Selective or failure to follow the correct route through a Selective will incur the maximum penalty for that Selective.

16.8. At their discretion, Organisers may set out further penalties on certain Selectives, e.g. where leaving the road / cutting corners might cause damage to private property.



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16.9. To be classified as a finisher, competitors must start at least one third of the Selectives; this is to encourage beginners by making it easier for them to finish the event.

17. DRIVING AND NOISE.

17.1. Driving in a manner which would bring the Motor Club and Motorsport into disrepute will result in exclusion. Strict observance of the law is required of each competitor and any car associated with the competitor. Competitors are reminded that the Clerk of the Course must enforce compliance with this rule.

17.2. Competitors suffering an exhaust failure must retire from the event immediately.

17.3. Any marshal considering a competing vehicle to be too noisy should fail the vehicle at that particular timed point, via or passage control by noting on the time card.

17.4. Any competitor receiving three Noise Fails is automatically excluded and must retire from the event immediately. A competitor who is excluded in this manner from more than one event during a season may be subject to further penalty.

17.5. A Drivers' Briefing must be held prior to the start of the event. Clerk of the Course must highlight driving standards, compliance with the RTA, cautions and PR sensitive areas.

18. SERVICING.

18.1. No service / chase cars are allowed.

18.2. Cars may be worked on only by the crew themselves, other competitors, garages en route, motoring assistance organisations or assistance provided by the Organisers. Competitors are responsible for the cost of any work done.

18.3. Any tools / materials used to repair a car must be carried in the car or be provided by other competitors or purchased en route.

18.4. The Organisers, at their discretion, may arrange limited mechanical assistance at rest halts and may also arrange a Mechanical Assistance Vehicle to follow the last car through the route.

19. PENALTIES.

19.1. Per minute late at a Control or Time Point
- 10 seconds per minute

19.2. Early arrival at a Control or Time Point (excluding Start Control, first Control / Time Point after rest halt, at their discretion, the organisers may waive penalties for early arrival at the final control)
- 20 seconds per minute

19.3. Where a previous timed point/via is missed or an incorrect departure at previous point / via or an incorrect approach at timed point in question, competitors can rejoin the route without incurring penalties for early arrival, unless they arrive before their original due arrival time, in which case article 19.2 will apply. (In other words, a competitor who is running very late can cut out some of the route and get back on time without incurring penalties, as long as they don't check in before their original due time at the point).

19.4. At subsequent timed points where competitors arrive before their original due time, (after 19.3) without exceeding the 50 kph average, per minute early
- 20 seconds per minute.

19.5. Over 15 minutes late at or missing a Time Point
- 50 seconds.

19.6. Over 30 minutes late at or missing a Control
- Exclusion.

19.7. Wrong approach or departure at a Time Point/Control
- 50 seconds.

19.8. Wrong approach and wrong departure at a Control or Time Point
- 200 seconds.

19.9. Traversing a Control/Time Point/Via more than once (where not required to do so by the route instructions)
- 150 seconds.

19.10. Failure to stop at Stop/Yield signs,
first offence - 300 seconds
second offence - exclusion.

19.11. Failure to stop at identifiable Stop Sign Marshal or other judge of fact
- 300 marks.

19.12. Visiting an Out of Bounds area,
each offence - 30 seconds.
Second offence - Exclusion.

19.13. Altering a time card
- Exclusion.

19.14. Interfering with another competitor's time card
- Exclusion.

19.15. Failure to complete the entire event under own power (with the exception that vehicles that have left to route may receive assistance to regain the route or vehicles blocking the route may receive assistance for the minimum distance necessary to clear the route)
- Exclusion

19.16. Failure to pass scrutiny at any time
- Exclusion.



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19.17. Passing a 'NO' board - 300 seconds.

19.18. Use of any kind of Satellite Navigation System - Exclusion .

19.19. Taking less than target time for a Selective - No penalty.

19.20. Taking more than target time but less than maximum time for a selective
- 1 second per second in excess of the target time.

19.21. Taking more than the maximum time for the Selective - the penalty will be the difference between the target time and the maximum time for the Selective in seconds.

19.22. Failure to attempt or complete the Selective - the maximum penalty for the Selective will apply.

19.23. Jumping the start of a Selective - 60 seconds.

19.24. Striking/passing wrong side of a cone - 10 seconds.

19.25. Striking, missing or failure to negotiate a chicane as instructed - 10 seconds.

19.26. Failure to collect a signature at a Passage Control - 60 seconds.

19.27. Failure to correctly record code board information - 60 seconds.

19.28. Failure to come to a complete halt in a Stop Box - 20 seconds.

19.29. Failure to stop astride the finishing line of a Selective - 20 seconds.

19.30. Travelling in the reverse direction on a Selective - Exclusion.

19.31. Ties will be decided by the lowest penalty (in seconds) on the first selective. If this fails to resolve the tie, the penalty on each successive Selective will be used until the tie is broken.

19.32. Driving in a manner likely to bring motorsport or the organising club into disrepute - Exclusion.

19.33. Reconnoitering Selectives or possession of any kind of pace notes - Exclusion.

20. AWARDS.

20.1. First, second and third crews overall - two trophies.

Only those cars entered the Endurance Class will be eligible for the Overall Awards.

20.2. First, second and third in each class (depending on entries) - two trophies.

20.3. Those receiving awards for overall placings will not be eligible for class awards, i.e. there will be no separate class awards for Endurance cars.

20.4. Additional awards, including beginner's award, may be given as per supplementary regulations. A beginner is a driver who has not competed in more than two endurance trials, retro trials or night navigation trials combined.

21. SCRUTINY.

21.1. All vehicles must be examined for compliance with regulations before he start.

21.2. All vehicles must carry a luminous red triangle.

21.3. Fire extinguishers are recommended.

21.4. Hi-Vis jackets for both crew members must be carried in competing cars and worn on all occasions where competitors are on the route of the rally and out of the car, except at rest halts, petrol halts and awaiting the start of selectives.

21.5. Standard headlights may only be supplemented by two additional lights. All such light must extinguish on dipped beam. If additional lights are fitted, any supplementary manufacturer's lights must be removed, not just rendered inoperable.

21.6. The wattage of all bulbs in forward facing lights must comply with the RTA. Any additional lights must be fitted so that their horizontal centre-line is not above the top of the original manufacturer's headlights.

21.7. Under-body protection for vehicles is permitted.

21.8. Competition brake linings/pads are permitted.

21.9. Roll cages where fitted, must blend in with the appearance of the vehicle.



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21.10. Full safety harnesses are not mandatory (as per Navigation Trial regulations) but are recommended. These can be of four point 'Clubman' type or the quick release 'Aircraft' harnesses.

21.11. Suspension may be altered, providing that mounting points remain as standard.

21.12. Suspension systems which use remote reservoirs are not permitted in the Endurance Class.

21.13. Limited Slip Differentials are not permitted in the Endurance Class.

21.14. Wheels are free but must not extend beyond the periphery of the bodywork. In an attempt to keep costs down and to encourage the use of standard cars, no control tyre is specified. However, the use of tyres marked "For competition use only", "For Rally use", or "For Rally use only" is prohibited. Any competitor who is in any doubt about eligibility of their tyres should contact the organisers before entering. This rule will be strictly enforced, in order to prevent damage to the private roads over which selectives take place. Scrutineers may, at their discretion, exclude cars using tyres likely to damage private roads over which the event is being held.

21.15. Turbo charged cars are not permitted.

21.16. Four wheel drive cars are not permitted.

21.17. All vehicles must present in a manufacturer's original colour scheme subject to a maximum of two colours (excluding vinyl roof where applicable) or single colour if original colour scheme changed by re-spraying.

21.18. Vehicles are not permitted to carry sign writing stickers or competition numbers except numbers supplied by the Organisers (see Art. 12).

21.19. Noise – every effort should be made to reduce noise to a minimum. It is a responsibility of the Clerk of the Course to ensure that:

- a) the necessary equipment is available
- b) the required noise tests are carried out and
- c) the appropriate penalties are applied.

21.20. Exhaust system - the permitted noise level measured at 45 degrees to the exhaust and 0.5 metres from the exhaust may be no more than 100 db(A) at 3500 rpm (2500 for diesel engines) (App. 2, Art. 9).

21.21. Induction – Carburettors must have filters fitted.

21.22. All Endurance Trials must have a noise test at pre-event scrutineering, at rest halts and at the finish. In addition, the CoC shall have the discretion to include a noise test at any other point he thinks appropriate.

21.23. A competitor who fails the noise test must be refused permission to start or be excluded if the noise level cannot be brought within the prescribed limits.

21.24. Route instructions may be handed out prior to the car having gone through scrutiny, but under no circumstances will a car failing to pass scrutiny be allowed to start the event.

21.25. The Vehicle Licensing Certificate or Registration book must be produced as evidence of compliance with engine size. In the event of a protest concerning a car's eligibility, the organisers may request further documentation / information.

21.26. A Scrutiny Record will be issued to all drivers. This card must be produced at scrutiny for each subsequent event. Failure to produce the card may result in start refusal.

22. OFFICIALS.

22.1. The Clerk of the Course, appointed by the organising club, should ideally be an experienced competitor. If he/she does not have experience of Endurance events, he/she should have access to someone who has experience of competing on Endurance type events.

23. STEWARD.

23.1. The appointed Motorsport Ireland Steward must drive a minimum of two competitive sections of the route, checking the following:

23.2. The actual distances of the competitive sections.

23.3. The actual time allowed, as per the Time Card/Route card.

23.4. The accuracy of the marshals' watches. The result of these checks is to be appended to the Steward's Report.

23.5. The appointed Steward must also inspect the letters issued by the organising club, in relation to both the residents along the route and to the Gardaí.

23.6. The appointed Steward must also check the use of 'NO' boards. 'NO' boards are to be used to prevent competitors leaving the intended route, where to do so might cause annoyance / disruption to residents.



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23.7. Where an event receives a negative comment from the appointed Steward regarding the route and timing, then the next event run by the promoting club/s will be required to have the route and timing inspected and approved by the appointed Steward for the next event. Such inspection to be carried out at least two weeks prior to the proposed date of the event.

24. PUBLIC RELATIONS.

24.1. Residents and other interested parties on and adjacent to the route who are likely to be disturbed or inconvenienced must be notified in advance about the event.

24.2. All houses along the route and fringe roads must be visited (National Primaries excluded).

24.3. Ideally, residents should be spoken to but at the very least, a PR notice must be delivered at each house.

24.4. All Garda Stations in the area of the route or parts through which the route passes are to be advised, in writing, of the event at least one week prior to the event.

24.5. 'NO' boards are to be used by the Organisers in sensitive areas. Competitors must not enter an area protected by a 'NO' board. Organisers must place a 'NO' so that competitors can turn in safety.

24.6. The CoC is required to satisfy the Steward that the foregoing has been complied with.

25. PROTESTS.

Protests must be made within 30 minutes of the posting of provisional results and must be in accordance with MI regulations. provisional results will be available shortly after the finish.

26. ENDURANCE TRIALS RECOMMENDATION TO CLUBS.

As this is a new discipline, the following recommendations are made in order to ensure:

- a) smooth running of the event,*
- b) minimum inconvenience to other road users and residents and*
- c) respect for those land owners willing to allow their property to be used.*

26.1. Entry fees should be standardised, to comply with any Championship Regulations, should a championship be run.

26.2. In an attempt to encourage novices / beginners, clubs should organise classes for those crews entering an Endurance Trial for the first time and should have a beginner's award. Excessively rough roads should not be used.

26.3. Caution boards should be used where necessary. Canal banks, or anything as dangerous, even on private ground, should not be used at all.

26.4. As there is no competitive navigation on roads open to the public, and no requirement to plot the route, all route cards may be issued at the same time.

26.5. Out of bounds and 'NO' boards should be used solely to avoid PR problems. They should not be used to define the route; Vias, even if unmanned, should be used instead.

26.6 There should be rest halts at appropriate intervals.

26.7. Every effort should be made to arrange petrol en route and at the finish. The distance to the nearest available petrol station to the finish, and its location, should be given in the Final Instructions, as some crews travel long distances to compete. It is illegal and inadvisable to carry fuel in cans in the car.

26.8. Instruction sheets should be issued to marshals and it should be ensured that they know how to find their Time Points, Controls etc. Marshals should not have to rely on poor photocopies of maps to reach their destinations. They should be provided with the information and given sufficient time to drive to the next time point when they close their current one. A sweeper car, as distinct from a mechanical assistance vehicle, should traverse the route after the last car. Marshals should be given a copy of the entire route.

26.9. An official car must traverse the entire route approximately 15 minutes before the first competitor's car.

26.10. All clubs should keep a list of marshals who attend their events and second copy to other clubs who request such information.

26.11. Competitors should be encouraged to introduce at least one marshal each, to ensure the continuance of the sport.

26.12. To encourage marshals, clubs should give a Marshal's Award at the end of the event.

26.13. The event should be planned so that the majority of marshals can get to the finish in time for refreshments etc.



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26.14. It is strongly recommended that proper sheltered accommodation be provided for mechanical scrutineering at all Endurance Trial events.

26.15. An official course car must visit all points approximately 15 minutes before the due time of the car number one, completing a time card en route and checking that marshals are in position and their watches are set to the correct time. This time card must be available for inspection at the conclusion of the event.

27. TECHNICAL REGULATIONS FOR ENDURANCE SPECIFICATION CARS.

27.1. An Endurance Specification car is a fundamentally standard car under 1400cc.

27.2. All cars and their components e.g. engine numbers must conform to the Vehicle Licensing Certificate / Registration Book and to the RTA.

27.3. Cars must be in completely standard condition except for the specified modifications detailed in these regulations. All other modifications are prohibited and will be penalised up to exclusion from the event. All components and equipment and every measurement and dimension must conform precisely to the manufacturer's published standard specification for the particular model of car entered.

27.4. Entrants must supply the Organisers with any vehicle documentation as required. Every part or component of the car must be a standard production item identifiable as the manufacturer's listed and numbered part or an equivalent generic or 'spurious' part obtainable in the normal course from normal motor factors, except where alternative components are specifically permitted by these regulations. In the case of generic or spurious parts, the onus will be on the competitor to demonstrate that no performance advantage accrues.

27.5. No standard part may be machined, lightened or polished unless specifically permitted by these regulations.

Competing cars must have:

27.6. Four point seats belts, to at least 'Clubman' specification (i.e. Aircraft type buckles not mandatory). Roll cages are acceptable but not mandatory – this is to allow competitors to participate in events in the U.K., where the higher average speed means that roll cages are mandatory.

27.7. Front windscreen of laminated glass.

27.8. Mud-flaps for all four wheels.

27.9. Reflective red warning triangle, first aid kit, tow rope and SOS/OK board.

Bodywork:

27.10. Cars must have bodywork that represents the manufacturer's original profile, as in standard or manufacturer's optional extra form.

27.11. All bodywork must be of the material provided as standard by the car manufacturer.

27.12. Front and rear bumpers must be retained and be of the same material and dimensions as those fitted by the manufacturer. The fitting of additional wheel arch extensions is prohibited.

27.13. Cars may not be lightened from the manufacturer's standard production weight.

27.14. Strengthening of the bodyshell is permitted.

27.15. De-mountable strut brace/s may be fitted.

27.16. Detachable sump-guards, transmission, fuel tank and silencing system protection plates may be fitted.

27.17. Bonnet, door and rear door / hatch locks may be changed and additional catches may be fitted.

27.18. In the interests of safety, self locking door mechanisms should be disabled.

27.19. Sun-roofs must be taped over to the satisfaction of the scrutineer.

Interior:

27.20. All major internal trim must remain as originally supplied by the manufacturer. The exceptions are:

- a) full harness belts must be fitted
- b) the steering wheel may be replaced
- c) the front seats may be replaced
- d) rear seats may have their backs folded down and securely fastened (or cut only where necessary to allow the fitting of a roll cage or seat belts)
- e) parcel shelves may be removed
- f) trim and carpets behind the rear seats in the boot space may be removed
- g) inside door panels may be re-shaped to allow fitting of roll cage door bars or substituted by an alternative panel from a model variant
- h) safety air-bags may be removed or disabled
- i) additional instruments, switches and controls may be fitted providing the lay-out of the original components is not changed



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j) interior accessories may be added to improve comfort, convenience or safety, provided they do not affect the affect the performance of the car

k) the glass area of the car must not be rendered opaque except for the sun-strip area.

Engine and transmissions:

27.21. The engine must in all respects conform to that originally fitted by the manufacturer for the model of car.

27.22. No part may be machined, balanced, lightened, polished or otherwise modified in any way except for a service re-ground crankshaft and / or cylinder re-bore which does not exceed 1.5 mm (60 thou) or does not exceed the engine capacity class by more than 2.5 %.

27.23. Engine and gearbox mountings may be modified or replaced, provided there is no alteration to the engine or gearbox or their position within the car.

27.24. Additional engine braces are allowed, provided they utilise existing attachment points on the engine and a strengthening plate may be attached to the base of the sump-pan.

27.25. Dry sump lubrication is not permitted.

27.26. Forced induction is not permitted.

27.27. Carburettor engines may have only standard maximum two carburettor chokes (two single or one double).

27.28. Fuel injection engines must retain the standard injection system complete with plenum chambers and throttle body unmodified.

27.29. Air intake trunking, to the air filter, is free. Air filters and elements are free, but must be fitted to ensure that the vehicle complies with the noise regulations for Navigation Trials.

27.30 All component parts of the gearbox and transmissions must remain as standard for the model of car entered and be unmodified, except the clutch friction material which is free.

27.31. The use of a limited-slip or torque-biasing differential is prohibited.

Exhaust system:

27.32. The original exhaust manifold must be retained as standard but the dimensions and routing of the exhaust system after the exit from the manifold is free, provided any originally fitted catalytic converter is retained.

27.33. Additional straps and brackets may be fitted to the exhaust system.

27.34. Noise levels must comply with the regulations for Navigation Trials, as described above.

Electrical system:

27.35. The wiring loom and ignition and engine management system may be modified.

27.36. The battery may be changed and its mounting strengthened but its position must remain as standard.

28. INFORMATION.

Anyone seeking further information about Endurance Trials should contact:

Chairman of the Navigation Sub-Committee

Paul Hughes

e-mail plhughes@jol.ie



CIRCUIT RACING

APPENDIX 40

STANDARD SUPPLEMENTARY REGULATIONS APPLICABLE TO ALL CIRCUIT RACING EVENTS

(INSERT NAME OF CLUB HERE)

MOTOR CLUB LTD.

(Address)

Supplementary Regulations for the

(INSERT EVENT TITLE HERE)

RACES

at

(Date)

(Venue)

A qualifying round of the

(List Championships)

40

1. An Event to be held under the General Competition Rules of Motorsport Ireland MI (incorporating the provisions of the International Sporting Code of the FIA). MI Yearbook, these Supplementary Regulations and such Final Instructions as may be issued by the *(INSERT NAME)* Motor Club in accordance with these Supplementary Regulations, (add other rules if necessary).

2. MI Permit No.

3. Status - (Grade of Permit).

4. Officials of the Meeting:

MI Stewards:
(Club) Stewards:
Clerk of the Course:
Asst. Clerk of the Course:
Secretary of the Meeting:
Scrutineers:
Judges:
Timekeepers:
Medical Officer:
Childrens Officer:

5. The Meeting will be promoted by the *(INSERT NAME)* Motor Club Ltd., and held at *(venue)* on *(Date)* at a time to be advised in Final Instructions. The races will be run on the *(TYPE)* circuit of *(amount)* km length and in a clockwise/anticlockwise direction.

6. RIGHTS.

The Organisers reserve the right, subject to approval of the Stewards of the Meeting to cancel or declare void the races if any unforeseen

circumstances arise, and further reserve the right to divide or amalgamate any races, classes or heats, alter the length of any races and also without assigning a reason to refuse an entry or to debar any car/driver from competing. The Organisers shall assign entries to the various races and shall have the power to invite entrants to compete in any race.

7. ELIGIBLE COMPETITORS AND ENTRANTS.

Races are open to holders of current F.I.A. **Competition licences** of an appropriate grade issued by an EU member country, plus Norway, Monaco and Switzerland, who can produce a current Medical Certificate.

(Races No. *(Insert No.)* is open to holders of FIA INTERNATIONAL Licences, current, valid and issued by the appropriate A.S.N., who produce a current International Accident Control Card completed in respect of International Circuit Racing).

ENTRANTS other than the drivers concerned must be holders of an appropriate current Entrants Licence.

8. ELIGIBLE CATEGORIES OF CAR.

Races will be held for the following classes, in accordance with the current class Regulations governing eligibility incorporated in the MI Yearbook.

RACE A laps
RACE B
RACE C
RACE D
RACE E

laps
laps
laps
laps



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(Etc. and other Classes as desired under MI Yearbook Regulations).

9. AWARDS.

RACE A: 1st, 2nd, 3rd, etc.

QUALIFICATION FOR AWARDS:

1st if 3 or more starters in any class or race.

2nd if 5 or more starters in any class or race.

3rd if 7 or more starters in any class or race.

4th, where applicable, if 8 or more starters in any class or race.

The Organisers will have final discretion in interpretation of the above.

PERPETUAL TROPHIES - Winner of perpetual Trophies will, if presented with their Awards, be required to insure same for the value notified to them by the Organisers, and to sign Forms of Indemnity as required. (The Organisers regret that Perpetual Trophies may not be removed from the jurisdiction of the Republic of Ireland).

10. ENTRIES.

Entries must be made on the official forms provided and be accompanied by the relevant Entry Fee.

Entries open immediately and close at 12 noon on 10th day before the start of the event.

THIS CLOSING DATE IS FINAL.

LATE AND/OR UNPAID ENTRIES WILL BE DISREGARDED AS NULL AND VOID.

Entries to be addressed to:
Race Secretary
Motor Club. (Address etc.)

Entrants, other than Competitors, will be required to complete Official Forms in the manner described above. All entrants should answer all questions on the Official Form and sign clearly acknowledging the undertakings and indemnities thereon.

Entry Fee Club Member €
Non Member €

Limitation on Starters - Races will be limited to a maximum number of starters unless otherwise authorised by MI. If more than that number of entries are received for any race a Reserve list may be created. Priority will be given to MI Championship points holders and subject to entries being in order, the reserves will be nominated in strict rotation of receipt of entries.

Acceptance of Entries - Entrants shall be informed of the acceptance of their entry by the issue by the Organisers of Final Instructions after the closing date.

With the submission of a signed Official Entry Form, each individual entrant and competitor confirms his/her acceptance of these Supplementary Regulations.

The Organisers reserve the sole right, subject to the approval of the Stewards of the Meeting, to accept or reject a proposed change of driver or vehicle.

11. SCRUTINY AND SIGN ON.

11.1. Competitors must present their Racing competition licence, with a valid medical date, to the Race Secretary or Deputy Race Secretary at "sign-on" on a form prescribed by Motorsport Ireland.

11.2. Before signing or attempting to sign on for a race held under the G.C.R.'s of Motorsport Ireland (MI), an Entrant / Competitor must have submitted their vehicle for Safety Scrutiny at a place and time as agreed with the Class Scrutineer. On finding that the safety of the vehicle complies with regulations (as submitted to MI), the class scrutineer will issue the Entrant / Competitor with a "Passed Scrutiny" sticker.

11.2.1. On receipt of such sticker the entrant / competitor must ensure that it is displayed in a prominent position within the vehicle. This position must allow easy checking by officials with the driver sitting in the driving position of the vehicle, and must have been approved by the Class Scrutineer.

11.3. The future condition of such vehicle will be the responsibility of the Entrant / Competitor who must maintain the "Safe Status" of the vehicle for the relevant race season. All vehicles entered and presenting to race under the G.C.R.'s of MI will be subject to "spot check" safety / eligibility scrutiny by the relevant class scrutineer.

11.4. The ONUS is on competitors to prove that their cars comply with the regulations and the cost of any dismantling required by the organisers will be decided by Appendix 2 of the current MI Yearbook. The Club retains the right to impound any car to check eligibility for its class.

11.5. All Crash Helmets must comply with the current MI Regulations.

11.6. Each car must carry a Fire Extinguisher System as required by MI Regulations.

11.7. Fuel (as defined by the FIA and of the maximum Octane Rating available as Premium Pump Fuel) must not be carried on a car in any container except the fuel tank. The definition of petrol is contained in Appendix 2 of this current MI Yearbook.



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11.8. Competition Numbers must be displayed on each side of the car and to show forward so as to be clearly visible to the Timekeepers from the timing position. Numbers must be waterproof and in black figures on a white background. The figures must be 23 cms in height and 5 cms wide. Competition numbers are subject to the approval of the scrutineers. **WHILE BEING DRIVEN ON PUBLIC ROADS TO OR FROM THE CIRCUIT THESE COMPETITION NUMBERS MUST AT ALL TIMES BE COVERED.** Breach of these regulations may lead to a penalty as defined in GCR 142 & 150.

11.8.1. Novice drivers On any car driven by a person who has not received six car race signatures (or five signatures and a completed IMC course) on his National B licence, there shall be on the back of the car a yellow square, 18 cm x 18 cm, with a black diagonal cross, with strokes 15 cm long and 2.5 cm wide.

11.8.2. All competitors must provide timing transponders to enable each competing vehicle to be timed. They should ensure that it is securely fitted in the approved bracket fitted in the designated place as indicated by the class scrutineer. This must be done prior to any practice or race. Transponder should be of the following type AMB260 and may be direct or battery powered.

11.9. Where an on-board Television camera or other recording device is fitted to a car the following regulations must be followed:

11.9.1. Written notice that the equipment is being carried must be given to the CoC, before the start of practice or racing.

11.9.2. The fitting and the device must be examined by a Scrutineer to ensure that it is safe before the competitor takes part in practice or race.

12. OFFICIAL PRACTICE / QUALIFYING.

Will be held on at times to be advised in the Final Instructions. Each Competitor may be required to complete not less than two practice / qualifying laps.

Any intending competitor, who in the opinion of the Organisers, drives in any way likely to prejudice the interests of Motor Racing may be excluded and forfeit his Entry Fee. per GCR 142 and 150.

13. RACE PROCEDURE.

(Subject to Art. 6)

13.1. The Race will be run in a clockwise or anticlockwise direction and cars should keep to the right and **overtake** on the left in a clockwise circuit and vice-versa on an anticlockwise circuit.

13.2. Grid positions for scratch races will be allocated in accordance with official practice lap

times and the class regulations. If the size of entry requires more than one Practice Session in the same class the Organisers will adjudicate if inequality of Practice conditions should occur. **Pole position** will be on the right hand side for clockwise and left hand side for anti clockwise. When identical times are recorded by more than one driver in the official practice session for that race the driver first setting such a time will receive the better starting position. Where **no practice times** are provided grid positions will be decided by most up to date class championship positions, with non registered championship runners lining up behind in positions decided by ballot conducted by the Clerk of the Course, or in a formation agreed in writing by all competitors.

13.2.1. Grid Positions for 1 Qualifying & 2 Races.

In the case of classes racing twice on the day but with only one qualifying session, the competitors fastest lap will count towards the grid position for race 1 and the competitors second fastest lap will count towards the grid position for race 2. Both races will be scratch unless the class requests an alternative in writing and receives approval from Motorsport Ireland.

13.3. When a **starting grid** has been formed to the satisfaction of the Chief Grid Marshal, cars will come under **Starter's Orders** at the showing of the 1 Minute signal. The cars will proceed on one or more warming-up laps as and when directed by the Clerk of the Course. On return to the starting positions in the correct order the cars, with engines running, will prepare to start. Any car which fails to start, or falls to the back of the field behind the last car during the formation lap, and does not enter the pit road, must start the race behind, the last line of the grid, must be stationary when the 5 second board is displayed, and must not retake their original grid position.

Practice starts are not permitted during warm-up laps.

13.3.1. Competitors who qualify out of session or are penalised during qualifying, must start from the pit lane. This will be after the grid has cleared the start line, and only when directed to do so by the Starter or Pit Lane Marshal.

13.4. Where lights are used to give a standing start, the starter will display a '5 SECONDS' board when the last cars arrive at the back of the grid after the formation lap. This is to warn the front rows that there are 5 seconds to go before the red light is shown. Within 4 to 6 seconds the red light will be extinguished indicating the start of the race. The Start Flag maybe used optionally.

13.5. Any car having come under Starters Orders and then not being able to take its place on the final grid may also start when ready from its pit, but only after the remaining cars have left the



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final grid and when directed to do so by the Pit Lane Marshal or Starter. (GCR 96).

In the case of Mondello where the pits may be behind the Starting Line or a continuation of it the provision of the FIA Yearbook I.S.C. 90 on **Departure from the Pits** - Starting Line after the pits would then apply, i.e. the driver will be considered as having completed one lap the second time he passes the line.

14. PENALTY FOR FALSE START - NATIONAL.

14.1. In the case of a massed start the driver concerned shall be penalised by the addition of 10 seconds for races up to 50 Km, and 60 seconds for races exceeding 50 Km to the time taken by him to complete the course (see also 14.2). The penalty shall immediately be notified to the drivers pits, or his representative and the final classification amended accordingly. Where a race is run in more than one part false start penalties will not affect grid positions for any restart and will only be applied when the final classification is being prepared.

14.2. In any case where the Supplementary Regulations so provide, the Stewards of the Meeting shall have power to increase the amount of the above mentioned penalties or to impose other penalties within the limits prescribed in such Supplementary Regulations.

15. FLAG SIGNALS.

Official Signals will be conveyed to drivers by the following flag signals (for more specific explanation see Appendix H International Sporting Code).

All flags will be a minimum of 60cm x 80cm.

15.1. Start: Lights/ Start Flag.

Where lights are used to control the start the following applies:

WHEN GRID IS FORMED: "5 Second board" is displayed.

RED lights illuminated: Remain stationary and prepare to start racing.

RED lights extinguished: start racing.

In the event of the lights not working, the Race will commence on the drop of the Start Flag (national flag).

15.2. Blue flag - Stationary.

Another competitor is following you closely.

15.3. Blue flag - Waved.

Another competitor is following you closely and may be about to attempt a passing manoeuvre.

15.4. White flag - A service car or slow moving car is on the circuit.

The white flag should be waved to indicate the sector of the track that the slow moving vehicle is in, and held stationary whilst the vehicle is in the next sector.

15.5. Yellow flag - Stationary.

Danger, slow down, no overtaking.

15.6. Yellow flag - Waved.

Great danger, slow down considerably, no overtaking, be prepared suddenly to change from the projected racing line or take other evasive action, be prepared to stop, if necessary.

15.7. Yellow flag with Red stripes - Stationary.

Slippery surface ahead.

15.8. Yellow flag with Red stripes - Waved.

Slippery surface imminent.

15.9. Green flag.

All clear, at the end of a danger area controlled by yellow flags.

It can also be used to signal the start of a warm-up lap.

15.10. Red flag.

Immediately cease racing and proceed slowly and with maximum caution to start line or pits as instructed by marshals, being prepared to stop should the track be blocked. No Overtaking.

15.11. Red flag waved at individual marshal's posts.

The Race has been stopped. Proceed as 15.10 above. No Overtaking.

15.12. Black flag with Orange disc displayed with number.

A warning of apparent mechanical failure or of a fire which might not be obvious to the driver. The car concerned must call at its pit for repairs on the next lap.

15.13. Black and White Rectangular flag split diagonally and displayed with a number.

A warning, to the driver of the car bearing the number that his behaviour is suspect and that he may be Black-flagged on further reports.

15.14. Black flag displayed with a number.

The driver must stop at his pit within one lap of receiving the signal and report to the Clerk of the Course. A penalty of exclusion may be enforced by removing a car from the race by display of the Black flag.

15.15. Black and White Chequered flag.

End of Race or session.



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15.16. At an incident where the track is obstructed, or marshals are working at the trackside; the attention of drivers should be gained by the deployment of a Waved Yellow at the Flag Post preceding the incident.

15.17. This should be reinforced by deploying a Stationary Yellow at the post prior to where a Waved Yellow is shown.

NB. In very serious cases, this flag may be supplemented by a waved yellow flag at this post.

15.18. A Stationary Green should be deployed at the post immediately after the incident.

15.19. If the incident is well off the track and marshals are not working at the trackside, the incident may be indicated by a stationary yellow, followed by a stationary green.

15.20. The Clerk or Deputy Clerk of the Course has exclusive authority to deploy the following flags:

Start Flag (National Flag),
Chequered Flag.

15.21. Red Flag, Black & White Flag and Black Flag should only be deployed with the knowledge of the Stewards.

Note: The exception would be that in an emergency the decision to deploy the Red Flag may be taken by the Clerk of the Course, without initial recourse to the Stewards.

15.22. Coloured light signals operated by remote control should be employed at points where the safety of flag marshals is in the slightest doubt (See I.S.C. Appendix H.) 16.

16. SAFETY CAR.

16.1. The Safety Car will be brought into operation to neutralise a race upon the sole decision of the Clerk of the Course.

16.2. For the purpose of these regulations the Safety Car will be located in accordance with the current track licence for the circuit, will enter and exit the circuit from this position and Safety Car (SC) boards and a stationary yellow flag will be shown initially from the Start line. If for safety or other relevant reasons a change in regulations is required a specific drivers and observers briefing will be given at the event, detailing the exact procedure to be used.

16.3. On the order from the Clerk of the Course, the Safety Car will join the circuit with its revolving lights on, regardless of where the race leader is.

16.4. When the order is given to deploy the Safety Car a stationary yellow flag and SC board will be displayed at the start/finish line and at each flag post on either side of the start/finish line. Once

the yellow flag and SC board is displayed at a flag post, each preceding, or subsequent flag post, should also display a yellow flag and SC board.

16.5. Flashing yellow lights may also be used at the Startline and at other points around the circuit.

16.6. Each time the Safety Car passes a flag point the yellow flag will be waved continuously while the Safety Car and all competing cars following it remain in the section between this points and the next flag point.

16.7. All competing cars, when notified of the Safety Car intervention (by the flag signals, SC boards, or by any other means) will reduce speed and line up behind the Safety Car, no more than 5 car lengths apart, and maintaining the same speed as it. Overtaking or overlapping of any other competing car during a Safety Car intervention is forbidden. Overtaking of a Safety Car is forbidden unless the particular competitor concerned is signalled to overtake the Safety Car by the Observer in the Safety Car.

16.8. When ordered to do so by the Clerk of the Course, the observer in the Safety Car will wave past any cars between the Safety Car and the race leader. These cars will continue at reduced speed and without overtaking until they reach the line of cars behind the Safety Car.

16.9. While the Safety Car is in operation competing cars may stop at their Pit, but may not rejoin the track while the Safety Car and the line of cars following it are passing the Pit Exit. A car rejoining the track will proceed at reduced speed and without overtaking until it reaches the end of the line of cars behind the Safety Car.

16.10. The Safety Car will remain in operation until at least all the leading cars on the circuit are lined up behind it.

16.11. When the Clerk of the Course calls in the Safety Car it will extinguish the yellow flashing lights prior to exiting the circuit. (It is recommended that this is prior to the 2nd last corner on the circuit) and the lap should be completed with the lights extinguished.

16.12. Following the withdrawal of the Safety Car and prior to passing the Green Flag, the race leader will maintain the pace and should maintain a gap of no more than 5 car lengths.

16.13. When the Safety Car pulls off the circuit a green flag will be waved and/or the green light shown at the start line.

16.13.1. Overtaking remains strictly forbidden until the first competing car passes the green flag at the start /finish line.



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16.13.2. All flag posts will withdraw their yellow flag and SC board, in race direction rotation and replace them with a stationary green flag for one lap.

16.14. Should a Safety Car be deployed, the race distance may or may not be extended by a maximum of 3 additional laps.

All laps completed will be counted in deciding the final result. When a race is for a specific period of time the length of the race will not be extended.

16.15. The Clerk of the Course for the race may impose a penalty upon any competitor or team who they consider has gained an unfair advantage whether inadvertently or not from a breach of these Safety Car Regulations. In the event where one competitor breaches these Safety Car Regulations for the benefit of another competitor, both may be penalised.

17. OUTSIDE ASSISTANCE.

During the race, fuel, oil water, spare parts or outside assistance must not be obtained other than in the pits.

18. END OF A RACE.

In the case of a race over a set distance or duration the winner shall be the competitor who covers the distance in the least time and the end of the race signal will be displayed when the first competitor completes the set distance or duration. Competitors in each race will be flagged off after the winner has crossed the finishing line.

The race will normally be considered finished three minutes later. Placings will be decided by the number of laps completed by each competitor, and where there is equality in the number of laps, by the time taken, provide they have covered at least three quarters of race distance. If any competitor on his final lap is prevented by a stop signal from proceeding to the finish line, his placing shall be determined from his time at the start of the lap.

19. FINISHERS.

19.1. If a race is less than 50 Km's long, only cars that have covered at least 75% of the distance or duration covered by the class winner, and which cross the finishing line under their own power within 3 minutes of the overall winner, will be classified as a finisher.

If a race is 50 Km's or more in length only cars which have covered more than 75% of the race distance or duration covered by the class winner will be classified as a finisher.

If any competitor in their final lap is prevented by a stop signal from proceeding to the finish line, their placing shall be determined from their time at the start of the lap.

19.1.1. End of Race Procedure.

Should the end of race signal inadvertently be displayed before the leading car completes the scheduled number of laps or the prescribed race time has been completed, the race will nevertheless be deemed to end the moment the signal is deployed.

19.1.2. Should the end of race signal be inadvertently delayed, the race will nevertheless be deemed to finish at the published race distance or duration and the competition classified accordingly.

19.2. Protest Period.

Competitors must remain available at the event until any protest period (usually 30 MINUTES after posting of results) relating to their event has elapsed, failing which any judicial action against or relating to that competitor may be heard in their absence. If a competitor wishes to leave the circuit before the expiry of the protest period, permission must be sought from the COC.

20. RACE STOPS.

20.1.

20.1.1. Should the need arise to stop any race or practice, Red Lights will be switched on at the Start line and Red Flags will be displayed at the Start line and at all Marshal Signalling Points around the Circuit.

When red flags are deployed all competitors will immediately cease circulating at racing speed.

20.1.2. If during a practice session, all vehicles will return to Pit Lane.

20.1.3. If during a race, all vehicles will return to the Grid.

20.2.

20.2.1. Case A.

Less than 2 laps completed by race leader. The race will be null and void. The race will restart from the original grid positions. Competitors unable to take the restart will be replaced by reserves that will start from the back in reserve order. Gaps on the grid should not be closed up. The length of the restarted race shall be determined by the Clerk of the Course but under normal circumstances the race distance will be reduced by at least two laps.

20.2.2. Case B.

More than 2 laps completed by the race leader but less than 50% of the total distance or duration. The race will restart from a grid set out by the finishing order of part 1. The result of the race will be the finishing order of part 2. The final result will not be aggregated. The length of the restarted race will be determined by the Clerk of the Course but under normal circumstances the remaining race distance will be the original distance, less the first part, less at least two laps.



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20.2.3. Case C.

If the race leader has completed more than 50% of the race distance or duration at the time of the stoppage, the race shall not be restarted and the results will be declared as if the race had run its full distance.

20.3.

20.3.1. Only Cars running under their own power at the time the Red flag is shown will be allowed to restart the race, or be classified in the final results.

20.3.2. However the Clerk of the Course may reinstate a competitor not running under their own power when the race is stopped, if such situation was caused through no fault of said competitor.

20.4. The finishing order shall be based upon the order of crossing the finish line at 1 lap less than the number of laps completed by the race leader at the time of showing the red flag.

20.5. False start penalties will only be applied to the final results.

21. DRIVERS BRIEFING/MEETINGS.

Competitors must attend any meeting or briefing where this is required, in the Final Instructions, indicated on the official notice board, by the Clerk of the Course, or by the Stewards of the Meeting.

22. DRIVING STANDARDS.

A driver must at all times drive in a manner compatible with general safety and any penalty incurred under these regulations shall not prevent any appropriate action under GCR 139, in respect of careless, reckless or dangerous driving.

23. AUTHORITY.

The decision of the Clerk of the Course on all matters arising out of the Race and Regulations shall be final (GCR 132), subject to the usual channels of appeal. Protests, if any, must be lodged in writing on the correct form where possible with the Clerk of the Course in accordance with GCR 163 within 30 minutes of the posting of the Official Results on the day of the Race, accompanied by the appropriate fee (see GCR's Schedule of fees), which may be refunded if the protest is deemed 'bonafide'. The powers given to the Clerk of the Course may be delegated to one or more deputies.

The names and the functions of these deputies will be given in final instructions or posted on the official notice board before the event commences. The COC may specifically delegate his Disciplinary Duties and the hearing of protests to an official whose name and function shall be published in the Final Instructions or on the Official Notice Board.

Breaching any of the foregoing regulations may entail exclusion from awards, results and/or the meeting.

The Organisers reserve the right to amend these Regulations should conditions warrant this being done for the better conduct of the Meeting or in the interests of safety. Any such alterations must be approved by the Stewards of the Meeting and will be announced to competitors. The Judges will report to the Clerk of the Course in respect of False Starts (if any) and the order in which cars cross the finishing line. No protest may be made against the decision of a Judge, which shall be accepted as final.

24. ADVERTISING.

Advertising will be permitted in accordance with the current MI regulations. Parts of the meeting may be broadcast over Radio or T.V. for the sole purpose of promoting Motor Racing. Under no circumstances will appearance fees be paid to any individual or company. Race sponsors advertising material as supplied by the Organisers should be affixed to all competing cars. No fee will be payable. GCR 184 relating to advertising & publicity material on cars applies.

25. JUDGES OF FACT.

Judges and Timekeepers must be named in the official programme or supplementary regulations or final instructions. The Judges will report to the Clerk of the Course in respect of False Starts (if any) and the order in which cars cross the finishing line. No protest may be made against the decision of a Judge, which shall be accepted as final.

Starting Judge: To observe the start and declare any false starts or other infringements.

Finish Judge: To declare the order in which the cars cross the finish line when the end of race signal is given or the scheduled race distance or duration is completed by the leader.

Chief Timekeeper: To declare the individual lap times and the order in which the cars cross the timing line throughout the competition.

Weighting Scales Judge: Will record the actual weight of competing cars and report any breach of the minimum weight requirements to the Clerk of the Course. This function may also be performed by a licensed scrutineer.

26. "STOP-GO" PENALTIES.

The following procedure must be adopted when a Clerk of the Course (COC) receives a complaint and decides to impose a "Stop-Go" penalty:



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26.1. The COC shall immediately instruct the appropriate start line official to display the black flag and the competitor's number on a board clearly marked with the words Stop and Go. This board will be displayed to the entire field and for a maximum of 3 laps.

26.2. The officials or the COC will clearly display the board to the pit lane in order to inform all present of the decision. The COC shall also advise the pit crew in writing as soon as reasonably possible.

26.3. The relevant driver must then come to the designated area within three complete laps of the display of the Black Flag and must proceed to the designated area without stopping elsewhere in the pit lane and remain there for the period of the time penalty.

26.4. Upon the designated signal, the driver shall rejoin the race without calling at his or her pit.

26.5. At all times the driver will drive in the pit lane at a safe speed and manner, obeying all signals.

26.6. Failure to comply with this procedure, or stop within 3 laps of the flag being displayed may result in additional penalties being applied in accordance GCR's 142 & 150.

26.7. A time penalty of not less than 10 seconds to be added to the competitor's finishing time will replace the above procedure if there are 3 or less laps remaining in a race.

26.8. The offence giving rise to the Stop-Go penalty must be subject to a disciplinary hearing to determine if the offence is subject to penalty points. GCR 142 and 150 .

27. INDEMNIFICATION.

Entry of a car will be accepted only on condition that the Leinster Motor Club Ltd. (LMC), Carlow Car Club (CCC), Irish Motor Racing Club (IMRC), Motor Enthusiasts Club (MEC), County Kildare Motor Club (CKMC), Mondello Park Ltd., Mondello Park Sports Ltd., R.I.A.C./Motorsport Ireland and the Sponsors, will not, under any circumstances whatever, be liable for any damage to property or injury or loss of whatever kind, sustained by the driver or other persons driving or attending upon said car, whether caused during the Race or during Practice runs, or while the said car is on the road forming part of the Circuit, or any approach, or on any land adjacent thereto, by whatever means such damage, injury or loss may be caused, and even though the same may be caused by the wilful act, neglect, or default of any official, agent or servant of the Club, Sponsors, landowners or MI.

The ONUS of proving that the car entered is in accordance with the specification disclosed on this Entry Form lies with Entrant/Driver.



GENERAL RACING CLASS

APPENDIX 41

GENERAL RACING CLASS SPORTING REGULATIONS

1. GENERAL

The regulations applicable to Racing are as the current MI Yearbook including the GCR's, Appendix 15 (Championships), Appendix 40 (Circuit Racing), these General Class Sporting regulations and the specific class sporting regulations.

1.1. Class Championship Organisers.

The Class Championship Organisers reserve the right to amend or vary the individual Class Sporting Regulations at any time before or during the Championship and to further issue additional statements concerning the regulations from time to time, subject to MI approval. All such statements will be notified in the MI Bulletin, on the MI website and to all registered competitors by post or e-mail to the address notified on the Class Registration form.

1.2. Eligible Cars.

1.2.1. As per Class Technical Regulations issued by the class and approved by MI.

1.2.2. Changes to Class Technical Regulations will be notified in the MI Bulletin and the details will be notified to registered competitors by post or e-mail to the address notified on the Class Regulation form.

1.2.3. Competitors must be registered for a Class Championship with the Class Organisers and be in possession of a valid racing licence issued by MI or be a valid racing licence and medical issued by an ASN of a member country of the European Union. Specific Class Registration details are found in the Class Sporting Regulations

1.2.4. Only those cars complying with the current Class Technical Regulations (in compliance with MI regulations as notified per Article 1.2.2 above), will be allowed to compete in any round of a Class Championship.

1.3. Class Registration and Championships

1.3.1. In order to be eligible, score points, obtain prize money (if Applicable), championship decals, and championship awards, a driver must complete the appropriate class registration form and return, accompanied by the registration fee, to the Class Championship co-ordinator at least 2 weeks prior to their first race in the championship.

1.3.2. The number, date and place of Class Championship rounds will be notified by publication in the MI Bulletin and on the MI website. MI reserves the right to amend the composition of the Championship rounds at any stage of the season and will give as much notice as possible to all registered competitors by publication in the earliest available MI Bulletin and on the MI website.

1.3.3. Championship Points.

Unless stated otherwise in the individual Class Sporting Regulations, and subject to approval by MI, points will be allocated in accordance with appendix 15 of the current MI Yearbook. In the event of a tie, the championship position will be resolved in accordance with Appendix 15 Article 6.10.

1.3.4. Awards.

Unless stated otherwise in the individual Class Sporting Regulations, awards will be presented for 1st, 2nd and 3rd for each race for each class. In the event of any provisional results or revision of championship tables being amended after any provisional presentations and such revisions affect the distribution of any awards and prize money (if applicable) the competitors concerned must return any such awards and prize money to the Class Championship co-ordinator in good condition within 14 days.

2. ENTRIES.

In accordance with Appendix 40 Article 10 of the MI Yearbook

3. CAR PREPARATION, SAFETY AND SCRUTINY.

General requirements, including personal safety equipment are set out in Appendix 2 and 3 of the MI Yearbook. Helmet care and eligibility is detailed in Appendix 4 and 5. Event scrutiny and sign-on procedures are in Appendix 40 Article 11.

4. RACE PROCEDURE.

As per Appendix 40 Article 13 of the MI Yearbook.

5. STARTS / DELAYED STARTS.

As per Appendix 40.

6. JUDICIAL PROCEDURES.

In accordance with the GCR's of the MI Yearbook



GENERAL RACING CLASS

APPENDIX 41

7. RACE PENALTIES - BREACH OF RULES.

7.1. In Accordance with the GCR's of the MI Yearbook, in particular Chapter 10.

7.2. *Any breach of driving standards will be dealt with firmly. Video or similar equipment under the control of Circuit, Race or Class organisers can be used to facilitate decisions by the CoC or Stewards. Video or similar equipment fitted to competitors' vehicles can be used to facilitate decisions by the CoC or Stewards. Competitors carrying in-car video equipment must ensure that they are forward facing and must provide the CoC or Stewards with the equipment on request.*

8. RACE PENALTIES - INFRINGEMENT OF TECHNICAL REGULATIONS.**8.1. Post-qualifying / pre race.**

8.1.1. Any competitor whose car is subject to a technical reject report, or other judicial action affecting the technical eligibility of the car, must represent the car for scrutiny, together with the reject report, at the designated place of scrutiny no later than 40 minutes before the time notified for the race to start. Time is of the essence and force majeure does not apply. This will be enforced by the MI scrutineers.

8.1.2. Any competitor who does not submit their car for re-scrutiny, or whose car fails re-scrutiny, may be subject to penalties imposed by the Clerk of the Course and/or Stewards.

8.2. Ineligibility.

8.2.1. Any competitor whose car is deemed ineligible under the relevant current Technical Regulations will be subject to penalties as per GCR 142.

8.3. Sealing /Examination

8.3.1. If a vehicle/component is sealed for subsequent technical examination the competitor will be notified by the Eligibility /Safety scrutineer, within the applicable time limit, of the date and place of the examination of the vehicle/component. The sealed items must be available for examination at the notified time and place, unless otherwise agreed by the Eligibility/Safety scrutineer. If a vehicle component is not made available for examination as required by the Eligibility/Safety scrutineer, the clerk of the Course or the Stewards, it will be considered ineligible. This will be reported to the Clerk of the Course for the application of penalties in accordance with MI Yearbook and these Championship Regulations.

9. ADVERTISING RIGHTS.

All advertising will be in accordance with GCR's Chapter 15

10. ELIGIBILITY / SAFETY SCRUTINEER.

As appointed by the MI Technical Advisory Group.

11. TECHNICAL.

MI Race Committee will publish annually on the MI website the Technical Regulations for each approved Championship class. Any changes will be notified in the MI Bulletin as noted in Article 1.2.2 above.

12. TELEVISION.

12.1. By entering the Championship competitors and their teams are obliged to assist the Class Championship organisers in the promotion of the championship and in particular any television coverage. All competitors and their teams are obliged to assist the nominated TV production company in the recording of the championship through the granting of interviews when requested and any other reasonable requests of the TV production company.

12.2. Cars must carry an in-car or on-board camera at the sole request of the Championship co-ordinator and allow this equipment to be fitted at any reasonable time at the request of the TV production company. No other in-car video or film cameras will be permitted without the written agreement of the Class championship co-ordinator or designated representative and/or MI.

12.3. The fitting of cameras must satisfy the safety requirements of the scrutineers.

12.4. Competitors selected to carry an in-car camera must display a Championship sponsors logo in front of the camera. Other advertising in view of the camera is not permitted without specific agreement.

12.5. The competitor or anyone associated with them are prohibited from trying to influence the editorial decisions of the TV production company in connection with any broadcast material.

12.6. All television rights are the property of MI or the Class organizing Television coverage. Any broadcast, rebroadcast or reproduction without the express written permission of the Class organiser and/or MI is strictly forbidden.



GENERAL RACING CLASS

APPENDIX 41

12.7 Appearance.

At the start of each event, practice and race cars must be clean and well painted, rust free and have no un-repaired accident damage. All panels must be properly secured and sign writing must be of high quality. All championship decals must be present and fitted in the prescribed position. Advertising carried on any car must not be in direct conflict with the aims of the championship sponsors. The organisers reserve the right to forbid any car which does not fully satisfy these requirements from taking part in any event run under these sporting regulations.

13. RACE FINISHES.

13.1. After taking the chequered flag drivers must progressively and safely slow down, not pass any competitors ahead of them and return to the Parc Ferme area. Drivers must keep their helmets on and harnesses done up until arrival in Parc Ferme.

13.2. Parc Ferme regulations apply from the Finish Line until permitted to leave the Parc Ferme area.

13.3. Once in the Parc Ferme drivers required for podium presentations must proceed immediately to the podium. The drivers required are those finishing first, second and third.

13.4. Team personnel are only allowed into Parc Ferme at the invitation of the Championship Eligibility /Safety scrutineer. The presence of any unauthorised team personnel may render that car(s) liable to exclusion from the event.

13.5. Cars may be held in Parc Ferme for a minimum of 15 minutes after each qualifying session and race.



FIAT PUNTO ABARTH

APPENDIX 42

FIAT PUNTO ABARTH CHAMPIONSHIP

SPORTING REGULATIONS

1. GENERAL.

1.1. Introduction

The Fiat Motorsport Club (F.M.C.) will organise the Fiat Punto Abarth Championship which will be contested during the **2012** racing season. The Championship will run in accordance with General Competition Rules (GCR's) of MI, the International Sporting Code of the FIA (the Code), the General Racing Class Sporting Regulations (Appendix 41), and these rules.

1.2. Officials.

1.2.1. Championship Orgnaiser: Fiat Motorsport Club/ MI

1.3. Conditions of Entry.

1.3.1 Eligible Cars

Cars eligible to compete must be supplied through the Fiat Motorsports Club and are numbered 1 through 26. Each vehicle will be allocated a serial number.

1.3.2. Replacement cars are not permitted and a competitor may only use the one car that is originally scrutineered for their use at each meeting.

1.4. Registration.

1.4.1. The Registration Fee is TBA for the Fiat Punto Abarth class and will be accepted from the 1st January in the current year.

1.4.2. Registration Numbers will be the permanent competition numbers for the championship. The Championship co-ordinator will allocate each registered driver a permanent number for the season and these will be issued strictly on receipt of a fully completed registration form and fee. All race numbers must be obtained from the Championship co-ordinator.

1.5 Awards.

1.5.1. Prize Fund per Round : TBA

1.6 Fuel.

1.6.1. No refuelling is permitted during official qualifying nor prior to the completion of post qualifying scrutineering. Failure to comply will result in the exclusion of the competitor's qualifying times from that qualifying session.

1.6.2. No refuelling is permitted during races nor prior to the completion of the post race scrutiny. Failure to comply will result in the exclusion of that competitor from the race results.

1.6.3. The only petrol permitted is that which is bought at a roadside service station and can only be unleaded. This is to be used as purchased, with no additive whatsoever. See also Appendix 2 Article 28.2.

1.6.4. Competitors may be selected to provide fuel samples at Punto Abarth test days, practices, or races.

1.7. General Regulations.

1.7.1. Each competitor should provide a Championship Vehicle Logbook prior to the first event of the Championship. Failure to present the document at scrutineering for each event may lead to the imposition of penalties including a reprimand, and/or a fine and/or exclusion from the event concerned.

1.7.2. Testing.

With effect from two weeks before the 1st Championship round in the season no competitor, or team, or any person connected with same, may take part in any session (exclusive or otherwise) other than an official Fiat Punto Abarth Championship session at any licensed motorsport venue worldwide or at any other venue. A test session is deemed to be a session open to any category of vehicle and which has been regularly scheduled by the venue. In exceptional circumstances permission be sought from the Championship organisers for exemption from this limitation. Failure to comply with this testing rule may result in exclusion from all or part of the Fiat Abarth Championship or forfeit of points for up to two rounds after the breach, such penalty to be imposed by the Championship Registrar.

2. ADVERTISING.

2.1. At all Championship Meetings.

2.1.1. The stickers and livery of the Championship sponsors must be displayed in the manner prescribed by the Fiat Motorsports Club as available from the Club Secretary or Class co-ordinator.

2.1.2. Competitors must display the drivers name on each side of the car on the rear side window immediately below their race number.

2.1.3. All surfaces, excluding glass, which have not been reserved for stickers by the Championship sponsors or used for the application of race numbers, are free for use.



FIAT PUNTO ABARTH

APPENDIX 42

Advertising that may conflict with the Championship sponsors must be approved by the Fiat Motorsport Club and/or MI in advance, and the position, size and colour (if approved) will be at the discretion of the Fiat Motorsports Club and/or MI.

2.1.4. In order to qualify for points and prize money (if any), competitors must carry (provided that these items are available at the event) the correct championship decals on their race cars as outlined by the Fiat Motorsports club.

2.1.5. Any race suits, decals, stickers, badges and flags supplied must comply with the requirements of the Fiat Motorsports Club as available from the Club Secretary or Class co-ordinator.

2.1.6. Drivers must take part in any autograph session or press interview wearing their Championship race suit, should this be required by the Fiat Motorsports Club or their associated sponsors.

3. MISCELLANEOUS.

3.1. End of season award winners are required to attend the Fiat Motorsport Awards Prizegiving on a date and a venue to be advised. Failure to attend may result in the withholding of prizes and/or awards and/or points at the discretion of the Fiat Motorsports Club.

3.2. Should any car, including the Championship winning car, carry any form of sponsorship or advertising which conflicts in any way with the Championship sponsor or MI and its associate companies or sponsors, the car may not be used by the Championship sponsor for any publicity purpose.

3.3. In signing the Championship Regulation form each driver agrees that the sponsors of the current Championship may make use of their activities and successes in motorsport for any advertising, publicity and public relations purposes. The driver and entrant also agree that in any advertising or promotion with which they are associated the full championship title (including the Championship sponsor(s) name) will be used at all times.

4. CONTACTS.

Further information on the Fiat Punto Abarth Championship may be obtained from the Class co-ordinator:

Hal McGuckin

3 Roselawn Court, Castleknock, Dublin 15.

Phone : 01-8212549

E-mail: hmcguckin@iolfree.ie



FIAT MARK 1 PUNTO

APPENDIX 43

FIAT MARK 1 PUNTO CHAMPIONSHIP

SPORTING REGULATIONS

1. GENERAL.

1.1. Introduction.

The Fiat Motorsport Club (F.M.C.) will organise the Fiat Mark 1 Punto Championship which will be contested during the **2012** racing season. The Championship will run in accordance with General Competition Rules (GCR's) of MI, the International Sporting Code of the FIA (the Code), the General Racing Class Sporting Regulations (Appendix 41), and these rules.

1.2. Officials.

1.2.1. Championship Orgnaiser : Fiat Motorsport Club (FMC)/ MI

1.3. Registration.

1.3.1. The Registration Fee is TBA for the Fiat Mark 1 Punto class and will be accepted from the 1st January in the current year.

1.4. Awards.

1.4.1. In Season Incentives.

1.4.1.1. As in previous years, the FMC will strive to raise sponsorship and trade support for the class. All funds will be used to promote the class and minimize competitor costs. All funds available will be distributed among drivers in the manner considered best to achieve the following aims:

- Encourage the highest standards of car preparation.
- Encourage high standards of race discipline.
- Control overall costs of participation.
- Promote good standards of car reliability.

1.4.1.2. Incentives may only be earned by drivers who comply with the following requirements :

- Must be a valid registered driver.
- The car must meet the required presentation standards.
- Must not be excluded/disqualified from race results.

1.4.1.3. Further details regarding incentives will be advised to registered drivers from time to time.

1.4.2. End of Season Awards.

1.4.2.1. The four competitors with the highest overall points score from their counting rounds will receive trophies and other awards.

1.4.2.2. Further details of all awards will be published at a later date.

2. ADVERTISING.

2.1. Advertising which directly conflicts with the aims of the Championship sponsors is forbidden.

2.2. The registration form will contain a section where each driver must declare all advertising which their car will carry. The organisers will examine this declaration and will advise the applicant driver of any item which is unacceptable. If there is any substantial change to the advertising carried throughout the season, the declaration must be updated and resubmitted to the Championship co-ordinator.

2.3. Championship Decals.

2.3.1. Each driver whose registration has been accepted by the organisers will receive a pack of official championship decals. The pack will contain detail instructions regarding the exact locations where these must be positioned on the car.

2.3.2. It is a strict entry requirement that all decals are properly fitted and in the correct locations on the car.

3. CONTACTS.

Further information may be obtained from the Class co-ordinator :

Vicky Reilly
Phone : TBA
E-mail : vicky@team.ie



FIAT UNO

APPENDIX 44

FIAT UNO CHAMPIONSHIP

SPORTING REGULATIONS

1. GENERAL.

1.1. Introduction.

The Fiat Motorsport Club (F.M.C.) will organise the Fiat Uno Championship which will be contested during the **2012** racing season. The Championship will run in accordance with General Competition Rules (GCR's) of MI, the International Sporting Code of the FIA (the Code), the General Racing Class Sporting Regulations (Appendix 41), and these rules.

1.2. Officials.

1.2.1. Championship Orgnaiser : Fiat Motorsport Club (FMC)/ MI

1.3. Registration.

1.3.1. The Registration Fee is TBA for the Fiat Uno class and will be accepted from the 1st January in the current year.

1.4. Awards.

1.4.1. In Season Incentives.

1.4.1.1. As in previous years, the FMC will strive to raise sponsorship and trade support for the class. All funds will be used to promote the class and minimize competitor costs. All funds available will be distributed among drivers in the manner considered best to achieve the following aims:

- Encourage the highest standards of car preparation.
- Encourage high standards of race discipline.
- Control overall costs of participation.
- Promote good standards of car reliability.

1.4.1.2. Incentives may only be earned by drivers who comply with the following requirements:

- Must be a valid registered driver.
- The car must meet the required presentation standards.
- Must not be excluded/disqualified from race results.

1.4.1.3. Further details regarding incentives will be advised to registered drivers from time to time.

1.4.2. End of Season Awards.

1.4.2.1. The overall championship winner will be presented with the "Uno Cup" perpetual trophy and will have their name inscribed upon it. The winner of the secondary championship will be presented with the "Uno Shield" perpetual trophy and will have their name inscribed upon it.

1.4.2.2. These perpetual trophies remain the property of the Fiat Motorsport Club and must be cared for with respect and be insured by the recipient. The "Uno Cup" and the "Uno Shield" must be returned to the club prior to the last championship race of the following season.

1.4.2.3. Further details of all awards will be published at a later date.

2. ADVERTISING.

2.1. Advertising which directly conflicts with the aims of the Championship sponsors is forbidden.

2.2. The registration form will contain a section where each driver must declare all advertising which their car will carry. The organisers will examine this declaration and will advise the applicant driver of any item which is unacceptable. If there is any substantial change to the advertising carried throughout the season, the declaration must be updated and resubmitted to the Championship co-ordinator.

2.3. Championship Decals

2.3.1. Each driver whose registration has been accepted by the organisers will receive a pack of official championship decals. The pack will contain detail instructions regarding the exact locations where these must be positioned on the car.

2.3.2. It is a strict entry requirement that all decals are properly fitted and in the correct locations on the car.

3. CONTACTS.

Further information may be obtained from the Class co-ordinator:

Sabrina O'Reilly
Phone : TBA
E-mail : TBA



FORMULA VEE

APPENDIX 45

2012 FORMULA VEE CHAMPIONSHIP

SPORTING REGULATIONS

1. Formula VEE will have three separate championships. To qualify for points in any of these championships drivers must register as full members with the Formula VEE Association and pay the appropriate membership fee. Drivers must also display a minimum of two championship sponsor decals on their cars.

2. No driver can score points in any of the championships until their subscription is paid in full **and meet any other conditions set by the nominated MI affiliated club**. Points that would have been earned before the fee is paid **and conditions met** shall not be awarded where the fee is paid **and conditions met** after the date of the race where the points would have been earned.

3. Championship numbers will not be allocated to any driver until their membership fee is paid in full.

3.1. Championship numbers 1 to 10 inclusive, shall be reserved for use by the top ten drivers in the previous years National Championship. All other numbers shall be allocated on a first come, first served basis. A driver may choose to change the number allocated to him/her to a preferred number, so long as the preferred number is available.

4. Race grids will be decided by qualifying times only. The slowest 28 cars or maximum grid number will constitute a qualifying race. The remainder will constitute the grid for the final, from pole position backwards. Vacant places on the back of the grid for the final will be filled by the top finishers from the qualifying race(s).

4.1. If a driver is unwilling or unable to take his/her place on the back of the grid, the next placed driver will be offered this place, and so on until the grid is filled.

5. In the event of there being insufficient races scheduled at a race meeting to allow sufficient qualifying races to be held for the number of entrants, priority will be given first, to registered championship regular entrants, and then within the championship contenders priority will be given to drivers based on their championship standing.

6. Where an entry for a race is deemed a late entry, that driver will be assigned the next reserve slot, if relevant, and can only take a race position at the back of the grid, irrespective of qualifying times. If the race is not oversubscribed

and sufficient places exist in currently scheduled races, then the late entrant driver will be allowed to qualify and compete as normal.

7. Allocation to qualifying sessions, where there are multiple qualifying sessions, will be based on the results of the previous race meeting, where appropriate, or otherwise at the discretion of the class co-ordinator. Late entries will be allocated to the slower qualifying session.

8. There shall be no more than one championship round on a single race day.

9. All Formula VEE drivers will be graded 'A', 'B' or 'C'. All drivers compete for points in the National Championship. 'A' drivers ONLY compete for points in the National Championship. 'B' drivers compete for points in the National Championship and the 'B' Championship. 'C' drivers compete for points in the National Championship and the 'C' Championship. 'A' drivers will be listed by name and the list provided to the MI Race Committee in January of each year.

9.1. 'C' drivers are novices (First time licence holders), or those who are deemed 'C' at the discretion of the committee.

9.2. 'B' drivers are all other drivers who do not qualify to be 'C' drivers, and have not earned 'A' status **and have not won any circuit racing championship in the previous three years**.

9.3. Any 'B' or 'C' championship driver who finishes in the Top Ten of the National Championship shall be upgraded to 'A' status for the following and subsequent seasons. Where appropriate, no driver shall be upgraded unless all the drivers ahead of him / her in their respective championship are also being upgraded.

9.4. An 'A' driver can may be automatically downgraded from 'A' to 'B' if that driver has finished (in championship final race) in the top five less than eight times in the previous 18 contested races. Downgrades must be applied for before the drivers first race of the season. Downgrades cannot be applied mid-season.

9.5. 'B' and 'C' championship points are awarded in the order the drivers finish the race. i.e. if the first 'B' driver to finish the race came home in outright 8th, then they would get the points in the National Championship accordingly, and in the 'B' championship they would get top points.



FORMULA VEE

APPENDIX 45

If the second 'B' driver finished the race in 12th position, then they would get second highest points in the 'B' championship. A 'B' driver cannot score points in the 'C' championship. A 'C' driver cannot score points in the 'B' championship. If a 'B' or 'C' driver scores points in their own championship AND the National Championship, those points remain separate.

9.6. For the purpose of assigning points to 'B' and 'C' drivers, the results of the qualifying race(s) are taken as a continuation from the last person to finish the final. For instance a 'C' driver who finished last in the final would be considered to be higher placed than a 'C' driver who won the qualifying race, and so on.

10. Each of the Formula VEE championship races shall be of a minimum of 12 laps duration **or 12 minutes**, with 15 laps for the National Championship **or 15 minutes**. (Lap means a full lap of the championship circuit).

11. The sponsor of the championship(s) reserves the right to attach to each car promotional stickers, of a size and in a position agreed with Formula VEE and Motorsport Ireland. Wearing of Sponsors hats is mandatory on the podium and during any TV or press interviews. The Sponsor's namebadge must also be in the correct position on the race suit.

12. No points or prizes will be awarded to those competitors who fail to comply with the regulations.

13. Where drivers have equal points in a championship, the tie shall first be resolved by reference to the current MI Yearbook. If a tie is not resolved by the MI Yearbook regulations, then the driver who scored the highest single score first will take the higher overall position.

13.1. Prizes are awarded for the top six overall in the National Championship and the top ten overall in the 'B' and 'C' Championships **or those drivers that have competed in 50% or more of the championship rounds, whichever is the lesser**. Non registered drivers may be asked to make a contribution to class running costs.

14. PRESENTATION & SAFETY.

14.1. Competitors must ensure that their cars comply with the conditions of eligibility and safety throughout qualifying and racing.

14.2. At the start of each event, qualifying or race, each car must be clean and in good order.

14.3. All exhausts must be painted or otherwise 'presentable'.

14.4. *It is strongly recommended that all competitors ensure that a standard battery jump connector, Radionics Part Number 612-066 Red SB50 Amp Connector or identical be fitted to the car either adjacent to the master switch or attached to the gearbox near the starter motor to facilitate the use of third party battery packs when needed.*

15. POINTS SCORING FOR CLASS CHAMPIONSHIPS.

15.1. National Championship points will be awarded as per the MI Yearbook. (Appendix 15). The two lowest scoring rounds shall be dropped for the purposes of championship totals (notwithstanding penalties).

15.2. The 'B' and 'C' championships will score as follows: 25/22/20/18/16/14/13/12/11/10/9/8/7/6/5/4/3/2/1. The number of dropped rounds for the 'B' and 'C' championships shall be decided at the AGM each year or at the general meeting where the race calendar is formally adopted.

16. PENALTIES.

16.1. Any driver found to have an engine or gearbox declared ineligible as the result of an eligibility check shall be excluded from the points for all races from the point at which the engine/gearbox was sealed (including the race at which the engine/gearbox was sealed) to when the eligibility check was performed, and the driver shall have points deducted for the three previous race finishes in the same season. These penalties are in addition to any penalties incurred as per MI GCRs.

16.2. *The engine or gearbox or both of the winner of the national championship and up to two other cars shall be sealed for eligibility inspections at or before the last race of the season. The costs incurred in these inspections shall be the sole liability of the competitor, with the exceptions of scrutineers expenses which the competitor shall not be liable for unless the car fails to pass the eligibility inspection. These eligibility checks may be in addition to anything requested by Motorsport Ireland.*

16.3. All other failures to comply with regulations that result in the driver being excluded from a race's results shall result in total loss of points for that round only.

16.4. Any round at which a penalty is incurred shall be a counting round towards the drivers overall championship total, and shall not be eligible to be a 'dropped' round.



FORMULA VEE

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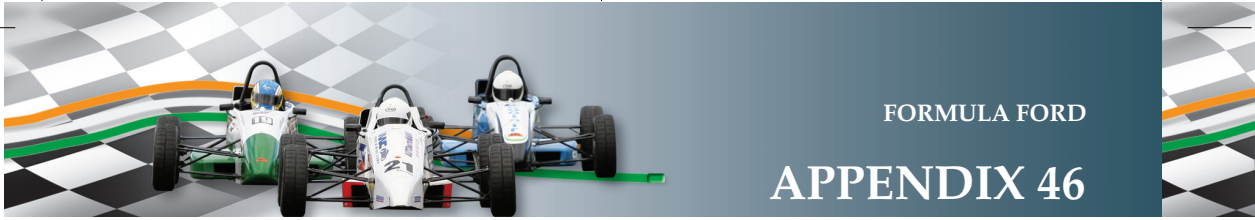
17. REGULATIONS.

17.1. The championships are organised in accordance with the General Competition Rules of MI, the International Sporting Code of the FIA, the current MI yearbook, and any further amendments published in MI Bulletins.

18. SCRUTINEER.

The class scrutineer for 2012 is:

Mr Gerry Kehoe
1 Cul na Greine
Old Bawn
Dublin 24
Phone: 01-4522356



FORMULA FORD

SPORTING REGULATIONS

T.B.A.

FORMULA SHEANE

APPENDIX 47

FORMULA SHEANE

SPORTING REGULATIONS

1. THE CHAMPIONSHIP.

1.1. The Class will be known as Formula Sheane.

1.2. The Championship will consist of ten rounds, eight of which will count towards the total championship score. Competitors may not choose to drop rounds. Each round shall consist of a single qualifying session of no less than fifteen minutes duration to determine the grid positions for the subsequent race. The race shall be of no less than thirteen miles distance excluding warm up and slow down laps. No qualifying session or race may be amalgamated/ combined with any other class without the prior consent of the Formula Sheane representative. In the case of double header meetings each round is separate and exclusive from each other. Points etc. will be awarded for each round as set out in Appendix 15 Article 6.1.3.

1.3. The championship will incorporate a 'Scholarship' Championship running concurrently. The drivers excluded from participation in the Scholarship Championship are any driver who finished in the top 6 in last years Formula Sheane Senior Championship, or was a previous champion in any "A" Racing class or have finished in the top 3 in any "A" Championship under the MI regulations. All other drivers would be considered for the Scholarship Championship including newcomers and novices. The qualification for the Scholarship Championship status will be at the discretion of the Formula Sheane Committee and any final decision would be on the basis of majority only or where no majority exists the Chairman's casting vote.

1.4. The Championship will also incorporate a Formula Sheane Fun Cup. The Fun Cup will consist of six rounds nominated by the Formula Sheane Committee. Drivers wishing to compete in Fun Cup will apply to be considered to the Formula Sheane committee who will approve drivers as eligible for competing for the Fun Cup. Competitors in the Fun Cup may only use one set of tyres for the six rounds (Damaged/punctured tyres may be replaced with approval from the class scrutineer). Competitors are also allowed only one full day testing prior to or during the six round period. Points will be awarded as per the main championship. Fun Cup competitors will be required to pay a reduced registration fee under the same conditions as the main championship registration process.

2. ADVERTISING RIGHTS.

All advertising will be in accordance with GCR Chapter 15.

3. CHAMPIONSHIP REGISTRATION.

3.1. All drivers must be registered with the class co-ordinator to be eligible for Championship points. To register, a driver must pay the registration fee in full only by the direct debit scheme and also must complete the registration form. To qualify for points at a particular race meeting and from that point on, the registration fee and form must be received by the class co-ordinator not later than five working days before the race meeting. The registration fee for **2012** is: TBA.

4. CLASS CO-ORDINATOR.

Andy Kavanagh
Mobile: 087 245 6168
Email: archermotorsports@hotmail.com

5. TECHNICAL.

MI Race Committee will publish annually on the MI website the current Technical Regulations for all approved Championship Classes.



PORSCHE

APPENDIX 48

PORSCHE OPEN RACING SERIES SPORTING REGULATIONS

Deleted from 1.1.11.



HRCA

APPENDIX 49

2012 HRCA CHAMPIONSHIP SPORTING REGULATIONS

1. The Championship will be run in accordance with the GCR's of the MI. The International Sporting Code of the F.I.A. and these rules and technical regulations will constitute supplementary regulations.

2. The Historic Racing Car Association (HRCA) & its sponsors will promote a series of championship and non-championship events in conjunction with MI affiliated clubs over the season incorporating events chosen from the Hillclimb and Racing Calendar.

3. The MI / HRCA championship is open to competitors who are members of the HRCA and have registered for same and whose cars comply with the published regulations.

4. SPONSORSHIP.

The HRCA may provide sponsors. Personal sponsors decals may be utilised but must not be larger than the Championship sponsors decals. Details will be provided by the HRCA upon request.

5. CLASSES.

5.1. Class 1, 2, **3 & 4** will compete for the George Mangan Trophy.

5.2. Class 4 will complete in a separate club Championship (**Pre 55**) and will race separately at all times.

5.3. Class 1, 2 & 3 will race together at all championship events.

5.4. All hillclimbs will be run on a handicap basis. Handicaps will be allocated by the Hillclimb Committee.

5.5. The George Mangan Trophy and class 4 championships will be based on a combination of races run at accredited motor racing circuits & rounds of the MI hillclimb championship.

5.5.1. The GLEN OF THE DOWNS CHAMPIONSHIP is a scratch race championship for historic race cars.

5.5.2. Eligibility for the Glen of the Downs Championship.

Cars that are eligible for any of the Irish Historic, U.K. Historic or European Historic Championship may compete in this championship under their own technical regulations, and subject to the approval of the Class Championship Registrar. Examples of cars which may compete are Irish Mini Cars, U.K. Historic Sports 2000 type cars, Sport Racing Cars.

5.5.3. The number of counting rounds is as per the HRCA racing calendar with 2 dropped scores. The racing calendar will be published by Motorsport Ireland Bulletin and on Motorsport Ireland website.

5.6. Deleted.

5.7. Class Structure.

5.7.1. Cars in classes 1, 2 & 3 will be sub divided based on lap times recorded on Mondello Park National Circuit.

5.7.1.1. Group A below 61 seconds.

5.7.1.2. Group B 61 seconds to 69 seconds.

5.7.1.3. Group C 65 seconds to 69 seconds.

5.7.1.4. Group D 69 seconds and above.

5.8. Points for various championships will be scored depending on class / **group** position in the overall race results.

5.9. There will be 2 revisions of a competitor's class during the year to take into account the competitor's performance. Should some competitors post times quicker or slower than the cut-off times in 2 race meetings during the period, then they will be re-graded to the new class. Points for championships will be carried over.

5.10. New competitors will compete in **Group A** for their first 2 events after which they will be allotted a class.

5.11. A clearly identifiable method of differentiating between various classes will be introduced before the start of the racing season.

5.12. Any registered competitor may request the HRCA Eligibility Scrutineer to examine the Vehicle Identification Form of any other registered competitor to decide if the details on that form conform to the class technical regulations. Protests concerning eligibility are to be made as per GCR 163.

6. PRESENTATION.

All cars must be presented in a condition that will boost the reputation of the HRCA. The class co-ordinator may ask a competitor to improve the presentation of any car that is not in keeping with the spirit of the HRCA



HRCA

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7. CONTACTS.

7.1. Class Principal: TBA.

7.2. Class Co-Coordinator / **Registrar:**

Alan Kessie

Phone: 086-1738437

Email: alan@askracing.com

7.3. Class Scrutineer:

Jimmy Wolverson.

8. POINTS SCORING.

8.1. In accordance with Appendix 15 of the 2012 Motorsport Ireland yearbook, except were less than 4 cars in a group as per 8.2. below.

Points will only be awarded to competitors, who return their vehicle identification form to the Class Championship Registrar before their first event. The Vehicle Identification form is available from the Class Co-Ordinator and is published with the Technical Regulations.

**8.2. If any Group within a race begins said race with three cars or less, the Group will be awarded reduced points on completion of the race. The points will be reduced as follows:
1st-17 points, 2nd-14 points, 3rd-12 points.**

8.3. Resolution of ties will be as per Appendix 15 of the 2012 Motorsport Ireland Yearbook.

9. TECHNICAL.

MI Race Committee will publish annually on the MI website the current Technical Regulations for all approved Championship Classes.

STRYKER

APPENDIX 50

THE 2012 MOTORSPORT IRELAND STRYKER SPORTS CAR CHALLENGE

SPORTING REGULATIONS

The Championship shall be run in accordance with the General Competition Rules of MI any Appendices issued by MI to those GCR's and these regulations. Where these regulations set out below are found to conflict with the Supplementary Regulations of any race meeting, these regulations shall apply and take precedence.

1. CHAMPIONSHIP POINTS.

1.1. At each Championship round points will be allocated in accordance with Rule No. 6 of Appendix 15 of this Yearbook.

1.2. Additional points for pole position and fastest lap will be awarded for each race.

1.3. If two people record equal times for pole or fastest lap the points will be awarded to the person who first records the time.

1.4. In the event of a tie for any championship position the competitors best result i.e. most wins, most seconds etc, as per Appendix 15 will decide the outcome.

2. CHAMPIONSHIP REGISTRATION.

All drivers must register for the Stryker Challenge in order to take part in any or all of the races before the start of practice for the first race which that driver enters or he will not qualify for the Championship points.

Registration fee shall be €150, payable to the SC (Stryker Challenge) registrar.

3. There will be 12 counting rounds, two of which may be dropped for championship points.

4. Competitors must allow the championship organisers to place race or championship sponsors decals on their vehicles if required. The championship organiser reserves the right to prevent a competitor from starting a race if that competitor refuses to accept such decals.

5. All grids shall be in 2 by 2 formation for all SSC races.

A standing start will be used at all SC Races

A minimum of one 'warm-up' lap will be run before each race whether or not there is also a formation lap.

6. The eligibility of the cars in the SC will be strictly policed and competitors must co-operate fully at all times with such scrutineering procedures as may be deemed necessary by the eligibility Scrutineer.

7. The minimum race distance for a SC race shall be 6 miles.

In the event of a race being shortened by the organisers prior to the start, the Registrar, at his sole discretion, shall decide whether to award championship points. Any driver who wishes to dispute this decision may appeal to the championship stewards.

8. Competitors may be required to wear sponsors headgear at prize presentations and/or to carry sponsors logos or decals on overalls. (For cars see regulation 4 above).

9. A driver may race a different car to the one in which he practiced but in that case must then start from the last place on the grid.

10. TECHNICAL.

MI Race Committee will publish annually on the MI website the current Technical Regulations for all approved Championship Classes.

11. An eligibility scrutineer will be appointed as nominated by the Technical Advisory Group.

10. AWARDS.

As per Appendix 41 (Standard Regulations for Racing) overall championship awards for 1st, 2nd and 3rd.



GT LIGHTS

APPENDIX 51

REELTECH GLOBAL GT LIGHTS CHAMPIONSHIP REGULATIONS 2012

SPORTING REGULATIONS

1. The championship shall be run in accordance with the **2012** general competition rules of MI and any appendices issued by MI to those GCR's and these regulations. Where these regulations set out below are found to conflict with the supplementary regulations of any race meeting these regulations shall apply and take precedence.

2. CHAMPIONSHIP POINTS.

At each championship round points will be allocated in accordance with Appendix 15 of this yearbook.

2.1. In the event of a tie for any championship position the competitors best result i.e. most wins, most seconds etc as per Appendix 15 will decide the outcome.

2.2. Points will be awarded to all cars that cross the finish line. After all championship rounds the competitor with the highest points score will be deemed to be the champion.

3. CHAMPIONSHIP REGISTRATION.

3.1. All drivers must register and pay the appropriate fee for the **Reeltech** Global GT Lights Championship in order to take part in either the championship or any race of Global GT Lights. Registration and Payment must be made 1 month before the first Championship round. Drivers who fail to register and pay will not be allocated points or allowed to race their car on the track in a Global Race.

3.2. Registration fee is payable to Global GT Lights and must be sent to the class co-ordinator 1 month before the first round.

4. COUNTING ROUNDS.

All rounds count towards the championship. You are allowed dropped round as per Appendix 15.

5. SPONSORS DECALS.

Competitors must place race or championship sponsors decals on their vehicles if required and in the approved position. The championship organiser reserves the right to prevent a competitor from starting a race if that competitor refuses to accept such decals.

6. STARTING PROCEDURE.

6.1. All grids shall be in 2 by 2 formation for all **Reeltech** Global GT Races

6.2. All starts will be rolling, if a competitor fails to take his position on the grid the next car must take his place and all subsequent cars then move up. No spaces can be left for a rolling start.

6.3. A minimum of one warm up lap will be run this will also be the formation lap to the rolling start.

6.4. All cars to be in tight formation before last corner approaching the start line. Should the weather change when the cars are on slick tyres the race must be stopped, cars allowed to change to wets and the race then re-run in full.

7. ELIGIBILITY.

The eligibility of cars in the **Reeltech** Global GT Lights Championship will be strictly policed and competitors must co-operate fully at all times with such scrutineering procedures as may be deemed necessary.

8. SPARE CAR.

A driver may race a different car to the one in which he qualified but he must then start from the last place on the grid.

9. TECHNICAL.

MI Race Committee will publish annually on the MI website the current Technical Regulations for all approved Championship Classes.



TOURING CARS

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2012 IRISH TOURING CARS CHAMPIONSHIP SPORTING REGULATIONS

1. TITLE AND JURISDICTION.

1.1. The ITCC Series is organised and administered in accordance with the General Competition Rules (GCR's) of Motorsport Ireland (MI), the International Sporting Code of the FIA (the Code), the Standard regulations for Racing (Appendix 40 MI Yearbook), the General Racing Class Sporting Regulations (Appendix 41 of MI Yearbook), any supplementary regulations issued by the Race Organising Clubs and these Regulations.

2. ORGANISERS

2.1. Series Principal Touring: Philip Burdock.

2.2. Series Principal Super Touring: Brian Sexton .

2.3. The eligibility scrutineer: Neil Wilson .

2.4. Race Day Co-ordinator, Championship Registrar & Touring Class Representative: Katie Burdock.

2.5. Super Touring Class Representative: Linda Sexton.

2.6. Driver Representative: Ciaran Timmons.

3. COMPETITOR ELIGIBILITY.

3.1. Entry into the ITCC series will be by invitation of the organisers.

3.2. Drivers and Entrant/Drivers must have all registration fees paid in full and must be in possession of a valid competition licence and an in-date ITCC authorisation badge which must be secured to their car for scrutiny.

3.3. All necessary documentation must be presented for checking at all rounds when signing on and/or at scrutineering.

3.4. The organisers reserve the right to invite any vehicle they deem fit on an invitational basis.

3.5. Drivers who have not paid the annual registration fee or the single day entry fee in accordance to section 4.2 of these regulations are not permitted to take part in an ITCC race.

3.6. Only cars with official ITCC Decals on designated areas will be allowed to take part in ITCC events (available from www.itcc.ie or ITCC organisers).

4. REGISTRATION.

4.1. Registrations will be accepted at any time.

4.2. All drivers must register as competitors for the series by returning the Registration Form, available at www.itcc.ie (COMPLETED IN FULL) and relevant fee.

4.3. Fees: Registration fee for the season is €400, which includes €100 deposit towards the entry fee for the UK round. No driver can score points in the championship until their registration is paid in full. Points that would have been earned before the fee is paid shall not be awarded where the fee is paid after the date of the race where the points would have been earned.

4.4. Permanent Competition numbers will be issued by the Organisers after their registration fee is paid in full. Championship numbers 1 to 10 inclusive shall be reserved for Super Touring drivers for use by the top ten drivers in the previous years' championship. The following numbers shall be reserved for Touring drivers for use by the top ten drivers in the previous years' championship: 11, 22, 33, 44, 55, 66, 77, 88, 99. All other numbers shall be allocated on a first come, first served basis. A driver may choose to change the number allocated to him/her to a preferred number, so long as the preferred number is available.

4.5. Non registered drivers who wish to participate in any round will not be issued official ITCC 2012 branded logos. Non registered drivers will be allocated a specific race number per round at a fee of €50 per race day, which will include a race day authorisation pass. Without this pass cars will not pass scrutineering. Non registered drivers who pay the €50 per race day and place 1st, 2nd or 3rd will receive a trophy on the day. Championship points will not awarded to drivers who pay the €50 race day fee, no matter what position the driver finishes in.

5. SERIES ROUNDS.

5.1. The ITCC Series will be contested over 12 races to be run at accredited motor racing circuits.

5.2. The series will consist of 2 classes, [Class A (Super Touring) and Class B (Touring). When



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registering, drivers must select which class to they wish to be entered in for the season. If a driver wishes to change class at any point in the season, they must write a formal request stating the reason why they wish to change. The class organizers reserve the right for the final say. If the change is granted, any championship points scored to date cannot be transferred to the other class.

5.3. A competitors finishing position for their class is determined by their finishing position after discounting cars from all higher classes.

5.4. The number of competitors competing in a class will be determined by the number of competitors from the class that are listed in the official results for the race (inclusive of non starters).

5.5. Where insufficient entries do not allow for the provision of separate races for Class A and Class B both classes will run together. Irrespective of qualifying times Class B cars will be gridded behind all Class A cars.

5.6. Cars must be presented in a clean and safe manner as stated in the ITCC Technical Regulations for 2012.

6. SPORTING REGULATIONS - JUDICIAL PROCEDURES.

6.1. These will be in accordance with the 2012 MOTORSPORT IRELAND Yearbook and these Regulations.

7. SPORTING REGULATIONS - RACE MEETINGS & RACE PROCEDURES.

7.1. Entries: Competitors are responsible for sending to the Race Organisers correct and complete entries and entry fees prior to the closing dates for each competing round.

7.2. Incorrect or incomplete entries (including Driver to be Nominated Entries) are to be held in abeyance until they are complete and correct and the date of receipt for acceptance of entry purposes shall be the date on which the Secretary of the Meeting receives missing or corrected information or fee.

7.3. Any withdrawal of Entry or Driver/Car changes made after acceptance of any entry must be notified to the Secretary of the Meeting in writing. If Driver/Vehicle changes are made after publication of Entry lists with Final Instructions, the Competitor concerned will be accepted in accordance with Motorsport Ireland GCR 113.

7.4. There will be a barrier time applied to the Touring car class at each circuit visited.

7.5. Qualifying:

Class A: The grid for the first race of a counting round will be determined by qualifying times. The grid for race two will be the finishing order of race one but with the top eight classified finishers reversed. That is to say, race one winner will start race two from grid position eight, race one runner up will start seventh and so on.

Class B: The grid for the first race of a counting round will be determined by qualifying times. The grid for race two will be the finishing order of race one but with the top eight finishers reversed. That is to say, race one winner will start race two from grid position eight, race one runner up will start seventh and so on.

7.6. Should a Touring class driver go below (faster) then the barrier time by any margin in a race, they will not be awarded championship points and prizes in that race. Should any driver break the barrier time in race one, they will start race two from the back of the grid. Should more than one driver break the barrier time in any race, the faster car will start at the back and so on. It is not permitted to drop a round where the driver has broken the barrier time. Points and prizes not awarded to a driver who breaks the barrier time will be awarded to the next driver who finishes under the barrier time.

7.7. Should a non registered driver who has paid the single day registration fee be permitted to enter any race, they must start the race from the back of the grid (after the last fully registered driver), regardless of their qualifying times. If more than one driver has paid the single day registration fee; the driver who qualified faster will be placed after the last fully registered driver.

8. RESULTS.

8.1. All practice Timesheets, Grids, Race results are to be deemed PROVISIONAL until all vehicles are released by Scrutineers after Post Practice/Race Scrutineering and/or after completion of any Judicial, Sporting or Technical Procedures including for any subsequent examination or test at any other specified location as permitted by these Regulations.

9. RACE PENALTIES.

9.1. Arising from post race Scrutineering or Judicial Action.

9.2. Minimum penalty: The provisions of Motorsport Ireland GCRs.



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9.3. For infringements deemed to be of a more serious nature the Clerk of the Course and/or Stewards of the Meeting are to invoke the provisions of GCRs.

9.4. As a registered championship with Motorsport Ireland and under FIA regulations, all disciplinary must be carried out by Motorsport Ireland officials. ITCC, could, under the code of conduct, request MI to hold an inquiry.

9.5. The organizers of the ITCC are determined to create a series of which is cleanly fought with good sportsmanship and good clean driving standards. Breaches of driving standards will be firmly dealt with. Any driver found guilty of a driving offence which results in penalty points under GCR will also have a series of points deducted proportionate with the severity of the points endorsement imposed, as follows:

- a) Points for yellow flag infringement = Loss of 12 championship points.
- b) Formal written reprimand = Loss of 8 championship points.
- c) Fine, time or place penalty = loss of 12 championship points.
- d) Exclusion from heat or race = Loss of 16 championship points.
- e) Exclusion from the meeting = Loss of 20 championship points.

10. AWARDS AND POINTS SCORING

10.1. Trophies will be presented at the end of each race or at an end of the meeting presentation ceremony.

10.2. Trophies will be provided to 1st, 2nd and 3rd place registered finisher of each class in each race.

10.3. A trophy will be awarded to the top Driver of the Day, to be decided by the sponsor. If the sponsor is unavailable, ITCC organizers will decide who to award the trophy to.

10.4. Where insufficient entries do not allow the provision of separate races for Class A and Class B both classes will run together. Irrespective of qualifying times Class B cars will be gridded behind all Class A cars.

10.5. Awards will be presented to the top three points scorers in both Class A and Class B at the awards night at the end of the season. The overall championship trophy will be awarded to the driver with the most points at the end of the season regardless of what class the driver is in.

10.6. 11 rounds count towards the championship, however one round (not event) can be dropped – lowest scoring round may be dropped. No round can be dropped where the barrier time has been broken.

10.7. Points system will be as follows as per Appendix 15 MI Yearbook and the method of resolving ties is as per appendix 15, (6.10) for each class with double points awarded for away (UK) rounds.

10.8. Class Championship Registrar contact details: Katie Burdock, katie@itcc.ie

10.9.

Finishing Position	Points Awarded
1st	20
2nd	17
3rd	14
4th	12
5th	11
6th	10
7th	9
8th	8
9th	7
10th	6
11th	5
12th	4
13th	3
14th	2
15th	1



GINETTA JUNIOR

APPENDIX 53

GINETTA JUNIOR IRELAND

SPORTING REGULATIONS

1. GENERAL

1.1. Introduction.

The Ginetta Junior Ireland series is organised in accordance with the General Competition Rules (GCR's) of MI, incorporating the International Sporting Code of the FIA (the Code), the General Class Sporting Regulations (Appendix 41) and these rules.

1.2. Officials.

1.2.1. Class Principal - Ann Stevens.

1.2.2. Class Representative - Jennifer Mullan.

1.2.3. Eligibility/Safety Scrutineer - as appointed by the Technical Advisory Group (TAG).

1.3. Competitor Eligibility.

1.3.1. Drivers.

Drivers must be a minimum of 14 years of age **and a maximum age of 17 years** prior to competing in their first race of the Ginetta Junior Ireland class. Drivers who reach the age of **18 years** during the racing season **(1st scheduled event to last scheduled event as per the annually published series racing calendar) may not take part in the remaining rounds of the series that season.**

1.3.2. Official entrants must be in possession of a valid **2012** MI Entrants licence.

1.3.3. Where there is no officially licensed entrant, the Parent/Legal Guardian or other delegated representative of the driver will assume the role of an entrant and must be present with the driver at all times.

1.3.4. Drivers must be in possession of a valid MI Junior Race Licence or MSA equivalent. This licence will be retained by the Class Representative for so long as the driver is participating in the Ginetta Junior Ireland series.

1.3.5. All necessary documentation must be presented for checking at sign-on for all rounds.

1.3.6. Cars.

Only those cars complying with the Ginetta Junior Ireland Technical Regulations, and/or any amendments issued from time to time and notified by way of a Bulletin by MI, will be allowed to compete in any round of the series.

1.3.7. Entrants/Drivers must ensure that their cars comply with the conditions of eligibility and safety throughout qualifying and racing.

1.4. Registration.

1.4.1. Registration for the series will be accepted from **1.1.2012** until the **1.9.2012**. All drivers must register as competitors for the series by returning the Drivers Registration form, **in full** completed, to the Class Representative prior to 12 noon on **1.3.2012** if they wish to register for the full series. Registrations are accepted on a "first come, first served" basis. The Class Representative reserves the right to accept registrations after this date. Acceptance of registration and the terms thereof is at the discretion of the Class Representative.

1.4.2. The Driver Registration form must be completed in full, signed by the driver, and if applicable by the Entrant and be accompanied by the Driver Registration Fee in full. The Driver Registration form must also be counter-signed by the Parent or Legal Guardian of the driver.

1.4.3. The Registration form is to be sent with a certified passport type photograph and proof of date of birth (e.g. a photocopy of birth certificate or relevant page from passport) to the Class Representative.

1.4.4. The acceptance or rejection of a registration application will be based on the information given on the registration form.

1.4.5. The Drivers Registration fee is €300 per driver who has competed in the series in the previous year. Registration fee is €350 for new drivers (refer to registration form for details). Cheques should be made payable to: Ginetta Junior Ireland.

1.4.6. A registration once made and accepted shall constitute a legally binding contract and a registration may only be withdrawn without penalty with the consent of the Class Representative. An Entrant or Driver whose registration is accepted and who otherwise fails to participate in the series shall forfeit their registration fees in their entirety.

1.4.7. Registration numbers will be permanent competition numbers for the series and will be allocated by the Class Representative. Number "1" is reserved for the series organisers use and may not be assigned by anyone other than **by** the organisers.

1.4.8. Unregistered competitors may participate in the series, but are not eligible to compete in the series championship. Fees payable to Ginetta Junior Ireland are as follows:



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- First event €50.00
- Second event €50.00 (or balance of registration fee €300.00)
- Third Event €250 being balance of the full registration fee.

1.4.9. Series championship points will not be backdated and will only apply from the date that the full registration fee is paid and fully completed registration form is received.

1.5. Scoring.

1.5.1. Points will be awarded to Registered Drivers within the series, listed as classified finishers in the Final Results as per Appendix 15, article 6.1.3 of the current MI Yearbook with the addition of 1 point for pole position.

1.5.2. In the case of a single round event an additional 5 bonus points will be awarded to all registered drivers who compete in that round.

1.6. End of Season Awards.

1.6.1. Awards for 1st, 2nd, and 3rd will be presented. They will be presented at an annual Class Prize Giving Dinner (date and venue TBA). Any recipient failing to attend may not be eligible to receive any prizes which may have been otherwise presented at this function. Such prizes may be withheld and may not be presented to any other driver.

2 ADVERTISING

2.1. The stickers and livery of series Sponsors must be displayed in the manner prescribed by Ginetta Junior Ireland available from the Class Representative.

2.2. All surfaces, excluding glass, which have not been claimed for stickers by the series Sponsor, or used for the application of the race number, are free for use. Conflicting sponsor's stickers must be approved by Ginetta Junior Ireland **and/or MI** in advance and the position, size and colour (if Approved) will be at the discretion of Ginetta Junior and/or MI.

2.3. In order to qualify for points competitors must carry the correct series decals on their race cars as outlined by Ginetta Junior Ireland.

2.4. Any race suits, decals, stickers, badges and flags supplied must comply with the requirements of Ginetta Junior Ireland as available from the Class Representative.

2.5. Drivers must take part in any autograph session or press interview wearing their race suit, should this be required by Ginetta Junior Ireland or their associated Sponsors.

2.6. Should any car, including the series winning car, carry any form of sponsorship or advertising which conflicts in any way with the series Sponsor and/or MI and its associated companies or sponsors, the car may not be used by the series Sponsor for any publicity purposes.

2.7. In signing the series Registration form each driver agrees the Sponsors of the current series may make use of their activities and successes in motorsport for any advertising, publicity and public relations purposes. The driver and entrant also agree that in any advertising or promotion with which they are associated the full series title (including the series sponsors name(s)) will be used at all times.

3. MISCELLANEOUS.

3.1. The organisers of the Ginetta Junior Ireland series are determined to create a series which is cleanly fought and with good sportsmanship prevailing.

3.2. Breaches of driving standards will be firmly dealt with. Any driver found guilty of a driving offence which generates penalty points under GCR 150 will also have series points deducted proportionate with the severity of the points endorsement imposed. Any deduction of series points will double that of the associated race licence endorsement as shown below:

- Formal written reprimand = loss of 4 series points
- Fine, time or place penalty = loss of 6 series points.
- Exclusion from heat or race = loss of 8 series points
- Exclusion from the meeting = loss of 12 series points.

3.3. It is mandatory to have video recording equipment fitted and operating during qualifying and racing. This must consist of at least one forward facing camera providing a reasonable view of circuit ahead such as would be expected to provide a representative 'driver's eye' view. The fitting of the cameras must satisfy the safety requirements of the Scrutineers. The choice of system is free, provided that playback is possible at the circuit by regular means, such as via a laptop PC, or by using equipment designed for the purpose provided by the competitor.

Competitors must ensure that this equipment is recording during races and make the recorded footage available on request by the Clerk of the Course or his representatives. Failure to do so may lead to disciplinary measures by the Clerk of the Course and may prejudice their defence in the case of an incident. This request is not restricted to the single forward facing camera mandated, but any additional video recording equipment utilised on the car.



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4. CONTACTS.

Further information may be obtained from:

Class Representative

Jennifer Mullan

Phone: +353 (0)85 7430825

E-mail : jennifermullan@ginettajuniorireland.com

Class Principal

Ann Stevens

Phone + 353 (0)86 2468700

E-mail: annstevens@ginettajuniorireland.com

Press/Media Officer

Paul Grogan

Phone +353 (0)86 1065301

E-mail: paulgrogan@ginettajuniorireland.com



FORMULA LIBRE

APPENDIX 54

2012 MAGIC BULLET BREEZEMOUNT FORMULA LIBRE IRELAND CHAMPIONSHIP SPORTING REGULATIONS

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1. GENERAL.

1.1 Introduction

The 2012 Magic Bullet Breezemount Formula Libre Ireland Championship incorporating The Leinster Leader Trophy for Formula Libre will be organised by the Formula Libre Drivers Association under a permit issued by Leinster Motor Club. The Championship will be organised in accordance with the General Competition Rules (GCR's) of Motorsport Ireland (MI), the International Sporting Code of the FIA (the Code), the General Racing Class Sporting Regulations (Appendix 41) the supplementary Regulations for racing and these Championship Regulations.

The Championship Organisers reserve the right to amend or vary these Sporting Regulations at any time before or during the Championship and further issue additional statements concerning the regulations from time to time, subject to MI approval. All such statements may be issued in a Bulletin by MI, on MI website (www.motorsportireland.com) and sent to all registered competitors by post or e-mail to the address detailed on the Championship Registration form or delivery to the competitor by hand by the Class Representative. All competitors, team members and persons associated with any of the above agree to be bound by the Regulations including any amendments, variations or statements relating thereto.

1.2. STATUS.

National.

1.3. Class Ethos, and Aims.

The 2012 Magic Bullet Breezemount Formula Libre Ireland Championship is organised to enable competitors to have a safe and enjoyable competition racing in single seater racing car or open topped sports car.

1.4. Eligibility of Competitors.

Intending competitors must be in possession of a current competition licence issued by an ASN affiliated to the FIA. It is the responsibility of the drivers to ensure that they are in possession of the correct licence grade for the event entered. In addition drivers must, in order to take part in Championship rounds, be racing members of the Formula Libre Drivers Association. It will be possible to take membership on a daily basis (see fees below).

1.5. Eligibility of Cars.

The 2012 Magic Bullet Breezemount Formula Libre Ireland Championship permits any proprietary single seater racing car, or open topped sports car. Prototypes and one off specials will be permitted subject to being of a standard of construction acceptable to the organisers. All vehicles must have the appropriate log book as issued by Motorsport Ireland or relevant ASN. The Organisers may restrict a class of car should its performance be regarded as inappropriate by reference to the entry in general. Replacement cars are permitted, however a competitor may only use more than the one car providing that it is originally scrutineered for his/her use at each meeting. At start of each event, qualifying or race, each car must be in a clean and presentable condition.

2. REGISTRATION.

All drivers, in order to take part in the 2012 Magic Bullet Breezemount Formula Libre Ireland Championship must register with the championship organisers.

The Annual registration fee is €120.00, or a daily fee can be paid of €30.00 per day.

All Registered drivers will be allocated championship points. Drivers, who pay the Annual registration fee by May 1st, can avail of Leinster Motor Club membership for €10.00. Each registered driver will be allocated a permanent Number for the season by the Championship Registrar, and must display supplied Sponsorship decals.

2.1. Class Officials.

Class Principal:

John Naylor, Phone 086-2565581.

Class Championship Registrar:

Ed McInerney, Phone 087-6497665.

Class Representative:

Garry Manning, Phone 087-2585759.

Class Scrutineer:

Jimmy Wolverson, Phone 087-7957148.

Note the Class Representative will work to ensure the smooth running of Formula Libre races. He will ensure that competitors are assembled on time and in their correct



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positions and will grid the cars in the manner as agreed by the Formula Libre Drivers Association.

2.2. Championship Dates. TBA

Dates will be also notified by email, or as shown under upcoming events on the class website <http://www.formulalibreireland.ie> and as published in the relevant Motorsport Ireland bulletin.

2.3. Class Structure.

There will be 2 classes, subject to there being a minimum of 5 registrations per class, and these will be:

1. For engine capacities up to 2.0 litre
2. For engine capacities above 2.0 litres (see also technical).
3. A class may be added for Formula Renault type cars. Full regulations for same will be published on the website and by Motorsport Ireland.

All classes will have equal status. In order to identify a competitor's class, number backgrounds will be colour coded, yellow for up to 2.0 litre and white for over 2.0 litres.

2.4. Points Scoring System.

The Championship will consist of 12 races, all 12 to count. There will be 6 events. Points will be awarded to classified finishers in each class in accordance with Appendix 15.6.1.3.1 of the Motorsport Ireland Yearbook. Where there are less than 5 starters in a class, that class winner's score will drop 1 place for every starter short. In the event of a race being cancelled, does not take place, and is not replaced, the number of counting rounds will drop by the number of cancelled rounds example 6 rounds, 5 to count.

1st-20pts, 2nd-17pts, 3rd-14pts, 4th-12pts, 5th-11pts, 6th-10pts, 7th-9pts, 8th-8pts, 9th-7pts, 10th-6pts, 11th-5pts, 12th-4pts, 13th-3pts, 14th-2pts, 15th-1pt.

3. RACE DAY FORMAT.

The purpose in stating the race day format below is to assist the organising club and bring continuity to the way Formula Libre races are run

Championship race days will consist of 1 qualifying session and 2 races per event. Drivers must proceed to the assembly area for qualifying and assemble as directed by the Class Representative, and the assembly marshal. This will be in an order pre-determined by the class coordinator so as to minimise traffic during the session. Grid positions will be

by fastest lap for Race 1, and second fastest lap for Race 2 as per Appendix 40, 13.2.1 MI 2012 Yearbook.

30 minutes after qualifying there will be a drivers briefing by the Class Representative followed by a subsequent meeting chaired by the Class Principal. Attendance at this briefing and meeting is compulsory. Failure to attend, or tardiness, may be penalised by a fine.

When a wet race has been declared, rear lights must be illuminated at all times

3.1. Qualification.

Each driver must complete a minimum of 1 timed lap of practice in the car to be raced and in the correct session in order to qualify for selection and order of precedence. Where a driver cannot complete a qualification lap, due to technical difficulties, the Class Representative may apply, on their behalf, to the Clerk of the course, to include the driver in the race, assuming that the Class Representative and the COC agree that the driver has the appropriate level of experience. If this occurs the driver will start from the pit lane. Note these procedures apply to racing at Mondello Park and Kirkistown only. At all other circuits the supplementary regulations for these events will apply.

3.2. Races.

The standard minimum scheduled distance shall be 20 kilometres whenever practicable but should any race distance be reduced at the discretion of the Clerk of the Course or Stewards of the Meeting it shall still count as a full event.

3.3. Starts.

All race start countdowns are to have a minimum elapsed period of 5 minutes from the time all cars are released to form up on the grid to the start of the Green Flag Lap(s).

The grid positions will be in a 1 x 1 staggered formation at all races where practical and appropriate, otherwise a 2 x 2 formation will be adopted.

Any cars removed from the grid after the 3 minute warning or driven into the pits on the Green Flag lap shall be held in the pit lane and may start the race after the last car to take the start from the grid has passed the Startline or pit lane exit, whichever is the later.

Any drivers unable to start the Green Flag lap or start are required to indicate their situation and any drivers unable to maintain grid positions on the Green Flag Lap to the extent that ALL other cars are ahead of them, may



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complete the Green Flag lap but **MUST** remain at the rear of the last row of the grid but ahead of any cars to be started with a time delay.

Excessive weaving to warm-up tyres by using more than 50% of the track width, and falling back in order to accelerate and practice starts, is prohibited.

A five-second board will be used to indicate that the grid is complete. The red lights will be switched on five seconds after the board is withdrawn.

3.4. Results.

Race results will be as issued by the timekeepers, and signed by the Clerk of the Course. A brief race report, results, and an updated championship table will be posted on the class website as soon as possible after an event. In addition the Class Representative may post a bulletin on matters arising.

4. AWARDS.

Details of awards will be published on the class website <http://www.formulalibreireland.ie/>.

The Leinster Motor Club in association with Magic Bullet will provide end of season championship awards to be presented at the championship organiser's awards presentation function. The overall championship winner will receive the Leinster Leader Trophy.

Formula Libre Ireland may distribute funds accrued (e.g. excess registration fees over class costs) in the form of cash or awards.

Note all results, awards and announcements will be posted on the class Website: www.formulalibreireland.ie

4.1. Method of Resolving Ties.

As outlined in Appendix 15.6.10 of the Motorsport Ireland Yearbook

5. CONDUCT.

Formula Libre racing by nature attracts cars of widely varying performance. While the onus is on passing drivers to navigate safe passage through traffic, it is incumbent on all drivers to be aware of and respect what is going on around them.

Any team member, representative or guest of a team who acts in an unsportsmanlike manner, or abuses an official representative of the sporting authorities and/or corporate sponsor, shall be expelled from the circuit. (refer GCR 114 MI Yearbook). Failure of the perpetrator to immediately comply with the expulsion order shall result in automatic cancellation of the entrant's entry for the subsequent event and/or a fine.

5.1. Judicial Procedures.

Competitors should contain and pursue grievances through the procedures as outlined in Chapter 11 of the MI General Competition rules and Appendix 40.19.2 of the Motorsport Ireland Yearbook. All competitors are reminded to familiarise themselves with these rules.

6. TECHNICAL.

• Technical regulations are free in the context of Formula Libre.

• A multiplication factor of 1.7 times engine cc will be used to determine the capacity of a forced induction engine.

• In the absence of technical regulations and in the interests of cost and reliability it is recommended that "RACE" designated tyres be used in competition.

• All cars should be presented for Scrutiny in a clean and tidy condition and ready to compete in accordance with Appendix 2 of the Motorsport Ireland Yearbook.

7. CLASS CHAMPIONSHIP REGISTRAR.

Name: Ed McNerney

Address: 32 Glasnevin Drive, Dublin 11, Ireland

Email: edmc@msn.com

Phone: + 353 (0)87- 6497665

Class website: www.formulalibreireland.ie



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MOTORSPORT IRELAND (MI) KART RACING REGULATIONS

SPORTING REGULATIONS

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1. GENERAL.

1.1. Kart Racing is governed by the FIA/CIK Internationally and Nationally by the General Competition Rules (GCR's) of MI. The Motorsport Commission of MI appoints the MI Karting Committee who administers the rules.

1.2. The MI Karting Committee shall:

- Effect the regulations for Kart Racing in Ireland
- Register drivers
- Administer the MI Championships.

1.2.1. The **2012** MI Yearbook, the **2012** MI GCR's and the FIA/CIK Yearbook of Karting will be used to administer Kart Racing in **2012**. In case of conflict within these rules and regulations the GCR's will take precedence. In case of conflict between Technical and other regulations the **2012** MI Kart Racing Regulations will take precedence for all National events.

1.2.2. All drivers racing in the MI Kart Racing Championships must register with the MI Karting Committee prior to racing in **2012**. Drivers will only use the number allocated to them.

1.2.3. The registration fees for 2012 is €20 for all classes.

1.2.3.1. The Karting Committee may, when a technical change is being introduced, charge an additional amount to the above fee for the supply of the revised component. If applicable to a class it will be notified in the relevant class technical regulations or in the monthly MI Bulletin.

1.3. Kart Race Meetings may only be organised by:

- Clubs affiliated to MI as Kart Racing Clubs
- or
- Motor Clubs affiliated to MI

1.3.1. Current affiliations as Kart Racing Clubs are:

- Irish Karting Club
- Southern Karting Club
- Munster Karting Club
- Meath & District Motor Club

1.3.2. Permanent tracks currently licensed by MI to race Karts are:

- Athboy Karting Centre, Athboy, Co. Meath
- Kartworld, Watergrasshill, Co. Cork
- Kiltorcan Raceway, Thomastown, Co. Kilkenny
- Pallas Karting, Tynagh, Co. Galway
- Whiteriver Park, Collon, Co. Louth

1.4. Classes.

1.4.1. Classes are as follows.

1.4.1.1. Junior Cadet, Comer Engine.

1.4.1.2. KF3.

1.4.1.3. Formula Rotax Minimax.

1.4.1.4. Formula Rotax Junior Max.

1.4.1.5. Formula 125 KZ2.

1.4.1.6. Formula Rotax National.

1.4.1.7. Super ProKart.

1.4.1.8. Formula Rotax 125 Max.

1.4.1.9. Super 4.

1.4.1.10. (Reminder in all classes - Article 1.5.6. applies).

1.4.2. Karts other than those listed in the above classes are not permitted for demonstration or otherwise without the approval of the MI Karting Committee.

1.4.3. The premier class for **2012** for the purpose of declaring a National Kart Racing Champion **is Formula Rotax 125 Max.**

1.4.4. The premier Junior class for **2012** for the purpose of declaring a National Kart Racing Junior Champion will be KF3.

1.5. Championships.

1.5.1. The following regulations shall apply to the MI Championships.

1.5.2. There will be 16 rounds (Finals) over eight race meetings of the MI National Kart Racing Championships for all classes.

1.5.3. Should a driver have any zero scores by reason of exclusion/disqualification from a round, such zeros will be adjudged to be counting rounds. Should a driver be excluded from a meeting, two zeros from that meeting (Date) will be deemed to be counting rounds.

1.5.4. To be eligible, drivers must register for the Championship. Points are awarded from the date of registration only.



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1.5.5. Registration is achieved by registering with the nominated Registrar (See 1.5.17).

1.5.6. There must be a minimum of 5 starters on the day in a class to qualify for Championship points.

1.5.7. Points for A-Finals and B-Finals will be as follows:

1st - 21 Points	9th - 7 Points
2nd - 17 Points	10th - 6 Points
3rd - 15 Points	11th - 5 Points
4th - 13 Points	12th - 4 Points
5th - 11 Points	13th - 3 Points
6th - 10 Points	14th - 2 Points
7th - 9 Points	15th - 1 Point
8th - 8 Points	

1.5.8. Fastest lap of the day, in a class, by a registered driver in a heat or a final will achieve 1 point.

1.5.9. Drivers can count a maximum of 12 rounds for Championship Points.

1.5.10. In the event of a round being cancelled or abandoned the counting rounds will be reduced by one for each round affected.

1.5.11. In the event of a tie, for any place, the tie will be decided under the following procedure

1.5.11.1. The greater number of 1st places in the 12 results counted will decide the tie.

1.5.11.2. If this does not give a result, the greater number of 2nds will be used.

1.5.11.3. If this does not give a result, the tie will be decided by the earliest scoring of a 1st in the season, or if no 1st the 2nds etc.

1.5.12. Starters are defined as anyone who is registered, signs on and competes in part of the meeting.

1.5.13. Drivers who are registered for a Championship, and wish to change class after September 1st, may only do so with the written approval the top 6 competitors in the class the driver wishes to enter and subject to 1.5.4 above.

1.5.14. Championship Awards.

Jordan Trophy (Presented by King Hussein of Jordan in 1963)

- Awarded to outright Champion of Ireland (see Article 1.4.3.)

Waterford Glass Trophy

- Awarded to outright Champion of Ireland Formula 125 class annually

Neil Shanahan Trophy

- Awarded by the Shanahan family in memory of their son Neil, to the outright Champion of Ireland KF3 Class, annually.

Class Trophies

- Awarded to all class winners.

1.5.15. Special Trophy Events.

- SKC "IRL Plate Championship"
- IKC "C Plate Championship"
- MKC "O Plate Championship"

1.5.16. Clubs must send event results to the Championship Registrar within 24 hours

1.5.17. The Championship Registrar is:

Zita Markey

127 Tymon Close, Old Bawn, Tallaght, Dublin 24.

Phone: 087-2831756

E-mail: zmarkey@sdublincoco.ie

2. ORGANISATION.

2.1. Officials.

2.1.1. Appointed Officials must be in conformity with MI GCR's Chapter 9 and must display their current M.I. Officials Licence.

2.1.2. No official or marshal shall be under 16 years of age, except for off track duties.

2.1.3. No official shall commence his duties before signing on.

2.1.4. One or more Kart Safety Scrutineers may be appointed to assist the Chief Scrutineer.

2.2. Signals.

2.2.1. Official Signals will be conveyed to drivers by the following flag signals.

2.2.2. Flag signals are illustrated in Appendix 8.

2.2.3. Starting a Race.

2.2.3.1. All starts are rolling starts.

2.2.3.2. Red lights extinguished: start racing. The green light will go on to indicate the track is live.

2.2.3.3. The National Flag should only be used as a starting signal if lights are unavailable or if the starting lights fail. Race will commence at the raising of the National flag.

2.2.3.4. A false start is indicated by the CoC or Start official waving his hand in the air, in a circular motion and or by waving a false start flag
- Green Flag with Yellow Chevron.

2.2.3.5. When the starter is satisfied the karts are approaching the start line in correct formation,



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the starting signal will be given by Red/Green light signal, or by the raising of the National Flag. It is an offence to break formation before crossing the start line itself and may result in the offender being moved to the back of the grid or otherwise penalised. Such penalties will not incur points or be recorded on a competition licence.

2.2.3.6. The use of other flags is as set out in Appendix 40 Article 15.

2.2.3.7. The chequered flag must be used to end all practice sessions

2.3. Race Procedure.

2.3.1. The CoC may permit a change in make of chassis or engine from those nominated on the entry form provided the application is made in writing before the end of scrutineering.

2.3.2. Only drivers who have completed a minimum of 3 practice laps will be permitted to race.

2.3.3. The maximum number of engines which may be used during a meeting is two, unless specifically varied in endurance events with a specific exemption from this requirement in the Supplementary Regulations. Changing the crankcase constitutes a change of engine.

2.3.4. Only one chassis may be used during a race meeting.

2.3.5. The number practising at any time shall not exceed the maximum permitted on track for a race plus 20%. This number will be stated on the event safety plan and MI track licence.

2.3.6. Adequate practice periods must be allotted to all classes which must include sufficient time for competitors requiring observation to be seen. Where possible novice and experienced drivers should be allocated separate sessions.

2.3.7. It is forbidden for gearbox and non-gearbox Karts to be on circuit at the same time.

2.4. Grids.

2.4.1. Grid positions for heats, (except where timed qualifying is being used) shall be on a random basis for first heat and inverted for second heat. Novices will start at rear of grid for all heats.

2.4.2. A driver having completed, the introduction to Karting course (See Appendix 1 Article 6.8.4) and completed less than 5 events and havin less than 5 signatures on the licence upgrade card is considered a novice.

2.4.3. Grid positions for B-Finals (the first final of the day in each class) will be based on a system of

points arising from the heats. In the event of a tie, the driver with the highest finishing position in the first heat goes in front.

2.4.4. In the case of a tie from split grids, the driver with the fastest lap in the first heat will go in front.

2.4.5. Points for grid position are as follows:

- First position - No points
- Second position - Two points
- Third position - Three points.

And so on for all finishers.

A non-finish gives the driver points of the number of signed on competitors in the heat plus five: so if there were 20 signed on competitors the points for a non-finisher would be 25.

Disqualified drivers will receive points of the number of competitors plus 7.

2.4.6 Grid positions for A-Finals (the second final of the day in each class) will be determined by the finishing order of the B-Final. Trophies will only be awarded in accordance with finishing positions in the A-Finals.

2.5. Noise.

2.5.1. Noise Safety.

The following regulationd are highly recommended and may become mandatory in future years.

2.5.2. Officials.

Every official and helper who is located close to the track should be issued with ear protection. This can be either earmuff style or disposable foam plug style.

2.5.3. Competitors.

It is highly recommended that every competitor wear earplugs at all times when driving a kart.

2.5.4. Dummy Grid and after race collecting area.

It is recommended that all personnel in the pit area use ear protection.

2.5.5. Dummy Grid, collecting area and spectators areas close to the track.

Signs are recommended advising that ear protection is recommended, and advising a source to purchase such protection.

2.5.6. Ear plugs.

Clubs should have supplies of disposable ear plugs for officials and for sale to spectators and drivers.

2.6. Starts.

2.6.1. The starting order for heats in which Karts shall be arranged will be determined by:

- Ballot.



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- Lap times recorded in official qualifying. This will consist of an out lap and 2 flying laps only or by a nominated number of minutes.
- Order of receipt of entry.
- Finishing order of a preceding race.
- As defined in Supplementary Regulations.

In all heats novice drivers must occupy the rearmost grid positions, but for finals may assume their earned position.

2.6.2. For National Championship rounds in Class KF3 starting order will be decided by lap times in official qualifying, provided there is a minimum of 5 drivers.

2.6.3. Karts on stands in the assembly area must not have engines running at any time. Karts may only be started when the kart has all four wheels on the ground and the driver is sitting in the Kart.

2.6.4. Karts will be formed up in the assembly area in two single files and will be push started or released when authorised by the Paddock official following the Green flag being shown.

2.6.5. No lifting of a Kart with the engine running is permitted on the dummy grid

2.6.6. When the Starter is ready for a race, he shall signal by raising a Green flag. This shall be the signal for the karts to be released onto the track.

2.6.7. When the Green flag is lowered, the entrance to the track shall be closed and no further karts shall be allowed to join the track. If a driver is unable to start from the assembly area after the display of the Green flag and he requests the intervention of a mechanic, he may be authorised to leave the assembly area on the orders of the CoC or other official and he will be started from the back of the formation

2.6.8. The Pole position shall be on that side of the track which is on the inside of the first corner after the start.

2.6.9. Any driver who receives assistance from anyone other than a signed on official outside the assembly area may be excluded from the meeting. An exception is provided for Junior/Cadet events as defined in Article 3.12.1 (below).

2.6.9.1. Any radio communication system between any driver on the track and any other body is strictly forbidden.

2.6.10. Drivers will be under Starters Orders from when the Green flag is displayed.

2.6.11. All starts shall be rolling starts. Any other form of start is forbidden.

2.6.12. No weaving or erratic driving on the rolling lap is permitted.

2.6.13. When the Starter is satisfied that the karts are approaching the start line in correct order, the starting signal shall be given as described in Article 2.2.

2.6.14. Karts may be selected at random prior to each qualifying session, race or final for technical checks by the Scrutineer in the presence of the CoC or their Deputy.

Any Kart that fails to comply with the regulations will be prevented from starting in that qualifying session, race, heat or final.

2.7. Race Stops.

2.7.1. If a race is stopped by the display of a Red flag the following procedures will apply.

2.7.2. If less than 2 laps have been completed by the race leader before the Red flag was displayed the race will be deemed null and void and a new start will be given.

2.7.3. If 2 laps or more have been completed by the race leader before the Red flag was displayed, but less than 75% of the scheduled distance completed (rounded to the nearest number of whole laps) the race will be restarted and run to make up the full race distance originally scheduled. Grid positions will be determined by the finishing order on the lap before the Red flag was displayed. The finishing order of the restarted race determines the result.

2.7.4. If 75% or more of the scheduled distance for the race (rounded to the nearest whole number of laps) has been completed by the race leader when the Red flag was displayed, the race will be deemed to have finished when the leading kart last passed the line at the end of the lap prior to the display of the Red flag.

2.7.5. Any kart not racing at the time of the incident which caused the race to be stopped may not rejoin the restarted race.

2.7.6. Karts and drivers involved in the incident that caused the display of the Red flag may only rejoin the race at the discretion of the CoC.

2.7.7. The CoC in consultation with the attending Medical Officer and Scrutineer must be satisfied that drivers and equipment are fit to restart the race.

2.7.8. In the event of being allowed to restart the driver(s) must start at the rear of the restarted race.



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2.7.9. While the race is stopped the whole course shall be considered as "parc ferme" and no work can be carried out to any kart. If a kart leaves the course during the race stoppage it will not be permitted to rejoin the race. When authorised by the CoC plugs may be changed and finger adjustments made to carburettor settings.

3. COMPETITORS.

3.1. Competitors must report for signing-on and Scrutineering at the time specified in the Supplementary Regulations.

3.2. Any competitor not reporting as instructed may be fined, or excluded.

3.3. At signing-on competitors are required to produce for examination

- Valid Kart Race Licence of the appropriate grade
- Valid Kart Entrants Licence for Junior Drivers (See GCR 108.3)
- Club Membership card (if required by supplementary regulations)

3.4. The Scrutineering document must be completed and signed by the driver prior to the commencement of the event. It is the responsibility of the driver to comply with the above, but if the driver is under 18 years of age, the signature on the scrutineering document must be that of the driver's parent/guardian/entrant.

3.5. No Kart may be driven in official practice, qualifying or a race until it has been passed by the MI Scrutineer for the event.

3.6. At the completion of any part of the event, if the equipment being used, including the chassis and engine numbers does not conform in all respects to the data on the scrutineering document, the driver will be excluded from the meeting.

3.7. For all events the CoC, in consultation with the Stewards, shall designate the race as wet, dry, or open. If it is designated.

3.7.1. Dry - then only dry tyres shall be used.

3.7.2. Open - the choice of tyres (i.e. wet or dry) is left to the competitor.

3.7.3. Wet - then only wet tyres shall be used.

3.7.4. A definition of wet tyres appears at 8.11.3. At no time may a Kart be fitted with a mixture of wet and dry tyres.

3.8. To be classified as a finisher, a Kart must cross the finish line with the driver seated correctly in his kart, within two minutes of the winner and have completed not less than 50% of the race distance, or as specified in the Supplementary Regulations.

3.9. Any driver leaving the track more than twice, whatever the reason, may be excluded from the race. Where a driver consistently drives with a wheel off the track or leaves the circuit, this may be deemed careless driving and the driver may be excluded from the race at the discretion of the CoC.

3.10. Once a race or practice session has started, Karts may only be worked on in a place of safety off the track, such as the assembly area.

3.11. Refuelling during a race is prohibited, unless specifically provided for in the Supplementary Regulations.

3.12. Any driver receiving outside assistance between coming under starter's orders and the end of the race, may be excluded from the results at the discretion of the CoC.

3.12.1. In Junior/Cadet events pushers may be allowed provided it is specified in the event Supplementary Regulations. Otherwise the provisions of 3.12 apply.

4. TRACK REGULATIONS.

4.1. Except while on track no competing Kart shall be driven without the specific permission of the CoC, but should be pushed on a suitable Kart trolley.

4.2. Any Kart which shall have left the track with all four wheels shall rejoin the track at the nearest point to exit from it, compatible with safety.

4.3. Any Kart unable to complete the course either during practice or the race shall be moved to a place of safety as regards the driver and other competitors and shall remain there with the driver until completion of the practice or race.

4.4. No Kart able to proceed under its own power shall be stopped either on the track or the verges of the course, but shall proceed to the pits or paddock.

4.5. All races shall be run in the direction specified on the track licence.

4.6. During a race or practice a Kart alone on the track may use the full width of the track.

4.7. Curves, as well as the approach and exit zones thereof, may be negotiated by the drivers in any way they wish, within the limits of the track. Overtaking may be done on the right or on the left.

4.8. Manoeuvres liable to hinder other drivers, such as premature direction changes on the straight; deliberate crowding of Karts towards the inside or outside of a curve or any other abnormal change of direction, are strictly



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prohibited and will be subject to penalties ranging from a fine to exclusion from a race. The repetition of such driving, even involuntary, may result in exclusion.

4.9. Any driver, driving in a manner which, even if unintentional, appears consistently to hinder or discourage another driver(s) seeking to pass may be halted by the display of the Black flag or otherwise penalised.

4.10. No competing Kart shall be driven in the reverse direction of the track except for the minimum distance to remove it from an unexpected situation, and then only under official supervision.

4.11. A driver shall at all times drive in a manner compatible with general safety and any penalty incurred under these regulations shall not prevent any appropriate action under GCR 139 in respect of careless, reckless or dangerous driving.

5. PROTECTIVE CLOTHING.

5.1. All helmets and protective clothing must be presented for inspection at scrutiny.

5.2. Helmets.

5.2.1. All helmets must have an approved for Kart Racing sticker as issued by MI or the MSA in accordance with Appendix 2 Article 17.1. For children the use of a Snell-FIA CMH (Children's Motorsport Helmet) is recommended. It is mandatory for first time licence holders racing in the Junior Cadet class to use helmets complying with either Snell-FIA CMS-2007 or Snell-FIA CMR-2007 Standards.

5.2.2. Advice on Purchase and Care of Helmets is in Appendix 4.

5.2.3. The accepted manufacturer labels that are accepted for Kart Racing by the FIA are illustrated in Appendix 5.

5.2.4. Impounding of Helmets.

5.2.4.1. Pre-event.

If the helmet does not conform with the required standard or is in a poor or dangerous condition, the Chief Scrutineer will impound the helmet for the duration of the meeting. At the close of the meeting the helmet will be returned as received, to the competitor concerned.

5.2.4.2. Accident during the Event.

If the competitor is injured, but not hospitalised, and the helmet is damaged, the Chief Scrutineer will impound the helmet and then seek the advice of the Stewards as to further action.

5.2.4.3. Accident during the Event and competitor evacuated to hospital with head injuries.

The Chief Scrutineer will ensure that the helmet has been seen by the Chief Medical Officer and

will then impound the helmet. Unless the Chief Medical Officer wishes to retain the helmet it must be despatched to MI. Unless specifically called for by the competitor it will be disposed of after six weeks.

5.2.4.4. Neither MI or their scrutineers are responsible for the safe keeping of impounded helmets.

5.2.5. Goggles and Visors

"Whirley" visors are acceptable when fitted on top of normal visors, but holes must not be cut in normal visors.

5.3. Other Protective Clothing.

5.3.1. The use of rib protectors is mandatory for all classes.

5.3.2. Clothing bearing the FIA / CIK homologation label will be accepted.

5.3.3. All clothing shall be securely fastened at wrists, ankle and neck and shall be adequate to minimise abrasions. Boots must cover and protect the ankles.

5.3.4. Driving suits and all other protective clothing must be worn at scrutineering and helmets, visors etc. presented for inspection by the scrutineers. Tailing or loose scarves are not permitted.

6. CIRCUITS.

6.1. A new application for a kart track licence shall be made at least thirteen weeks in advance of any date on which it is proposed to run a meeting. If the organisation is by a new club the first three meetings after the granting of a Track Licence shall be restricted to members of the organising club and shall be subject to observation before upgrading.

(a) Track Licences for new Short Circuits will only be granted after inspection by MI. Licences will be graded as follows:

(I) Suitable for all status meetings up to and including international.

(II) Suitable for all status meetings up to and including international. If a Long Circuit track incorporates a Short Circuit, these will be subject to individual grading and Track Licences.

(b) Existing Track Licences will be graded as above by an inspection.

6.1.1. There are four categories of track for Kart racing:

(a) Kart Permanent: Tracks with a lap distance of less than 1500m, where the shape can be determined at any time and all protective barriers are permanently in position.



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- (b) Kart Temporary: Tracks with a lap distance less than 1500m, where the barriers are not permanently positioned.
- (c) Round the Houses: Tracks using public roads or passing through residential areas or so classified by MI.
- (d) Long Circuits: Any Kart circuit, other than (c), exceeding 1500m in lap distance, and all licenced Motor Race Circuits.

6.2. Except where varied on the track licence, all tracks shall comply with the following conditions:

- (a) Minimum width - 6m.
- (b) Chicanes are not encouraged but when essential they must be approved by MI.
- (c) The surface shall be similar, i.e., sealed or unsealed, throughout the length of the track. The surface shall be durable and well graded, free of holes, rough spots or loose particles. All obstacles to a Kart leaving the track such as poles, ditches, etc, shall be suitably protected.

6.2.1. The track edges shall be marked in a manner approved by MI. Half tyres set in the ground must not be used, and there shall be no drop between the edge of the track and the verge. Any marker shall be of such size and weight that it will not constitute a hazard.

6.2.2. Where alternative routes on the same track may be used, those not in use must be clearly marked by bollards or marker boards. Painted lines on the track surface are not sufficient.

6.2.3. Any area which may at any time during practice or racing contain any persons, other than organising staff carrying out their duties or competitors actually taking part in an event, should be protected from the track and from area where Karts are in motion, by a physical barrier and safety precautions conforming to the following specification: A barrier of a permanent type having a continuous, smooth, vertical face of at least 1m in height on the track side and of sound construction so as to be incapable of deflection away from the track if struck by a Kart travelling at the maximum speed likely to be achieved at that portion of the track. If this barrier is used to contain the persons in the enclosure it must be sited not less than 3m from the edge of the track. On a bend or corner, if this barrier is less than 10m from the edge of the track, there should be a post and rope fence, keeping spectators a further 1m behind the barrier. Under no circumstances may any spectator be within 3m of the edge of the track on a straight, or 8m on a bend or corner.

6.2.4. Greater safeguards may be required if an area is deemed to be hazardous or in the vicinity of Karts travelling at high speeds. In all cases, the maximum possible space shall be allowed in head-on situations and through any

corner. Energy absorbing protection of either tyre stacks or straw bales should be placed directly in front of the ultimate barrier in head-on situations.

6.2.5. No variation of barriers/enclosures from the Track Licence will be permitted unless specifically approved by MI.

6.2.6. Access to all enclosures and to the track should only be by means of controlled entrances.

6.2.7. Temporary tracks will be subject to MI approval and must conform in all ways to the approval conditions.

6.3. The maximum number of starters will be determined as follows unless otherwise stated on the Track Licence.

Min. width 6m:

Non-gearbox - 4 per 100m, max 24

Gearbox - 3 per 100m, max 18

250 Int - 3 per 100m, max 18

Min. width 7m:

Non-gearbox - 5 per 100m, max 34

Gearbox - 4 per 100m, max 30

250 Int - 4 per 100m, max 30.

6.3.1. The grid for a standing start should be laid out so that there is at least 4m between the front of each row. The grid formations shall be:

6-8m 2x2x2 maximum

8-9m 3x2x3 maximum

9-12m 4x3x4 maximum

(if approved by MI).

7. EQUIPMENT.

7.1. The following minimum equipment is mandatory for all Kart meetings.

7.1.1. Warning notices, tickets, armbands, etc., as specified hereunder.

7.1.1.1. Warning Notices.

The following notices should be prominently displayed as appropriate.

7.1.1.2. At any entrance to an event or part of an event on private ground - Notice A.

7.1.1.3. Unless any enclosure is surrounded by secure fencing, in the area between the enclosure and the course there shall be displayed at intervals Notice B or notices stating that "This area is prohibited and the public are not permitted".

Notice A

WARNING MOTOR SPORT CAN BE DANGEROUS DESPITE THE ORGANISERS TAKING ALL REASONABLE PRECAUTIONS UNAVOIDABLE ACCIDENTS CAN HAPPEN THEREFORE YOU ARE PRESENT AT YOUR OWN RISK.



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Notice B

PROHIBITED AREA SPECTATORS ARE NOT PERMITTED IN THE AREA BEHIND THIS NOTICE ANY SPECTATOR ENTERING THE PROHIBITED AREA WILL BE REGARDED AS A TRESPASSER.

7.1.1.4. Organisers may also use a notice stating "It is a condition of your presence that you will obey the instructions of Marshals in relation to the Safety of yourself and others".

7.1.1.5. All officials, Marshals and members of the press must be identified by armlets, tabards or badges which are not transferable. They undertake their duties at their own risk and must not go nearer the course than is essential to the performance of their duties. No official under the age of 18 should be given duties which require him to be outside an enclosure.

7.1.1.6. The programme of the meeting, all tickets, badges, tabards, labels (including Kart labels) and passes of admission whether for spectators or officials shall bear or be accompanied by a copy of Notice A.

7.1.1.7. Where the wording is printed on the reverse side of a ticket, armlet or lapel badge, the words "for conditions of admissions see over" should appear on the face of the ticket, armlet or lapel badge.

7.1.2. Flag signals shall be as detailed in Article 15 of Appendix 40.

7.1.3. Fire extinguishers (minimum 2kg dry powder) with certificate from manufacturers or their agents issued annually prior to the season's racing to the effect that they are in working order. As a minimum one must be available at the start line, two in the paddock in clearly visible positions, and one at each Marshal Post.

7.1.4. Track cleaning equipment comprising two units of:

- (a) One ordinary shovel
- (b) Two brooms
- (c) A metal container of 3 kg dry cement or suitable cleansing agent for removing oil.

7.1.5. A public address system available to the Clerk of the Course (a hand-held powered megaphone is acceptable as a minimum), for communicating with competitors.

7.1.6. A starter's Sheet and an Officials' signing-on sheet, in the control of the Secretary of the Meeting.

7.1.7. An Official Notice Board prominently sited in the paddock for all official communications, grid positions and race results.

7.1.8. Scales with current Calibration Certificate, Scales are deemed correct on the day. The onus is on each competitor to check their weight. Minimum weight limits will be strictly enforced for all classes.



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MOTORSPORT IRELAND (MI) KART RACING REGULATIONS

TECHNICAL REGULATIONS

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8. TECHNICAL - GENERAL.

8.1. All karts must comply with both the General and Class Technical Regulations

8.2. Modification, addition, variation or tuning other than specifically permitted in these technical regulations is prohibited.

8.3. Anyone requiring clarification or definitions concerning the Technical Regulations should apply in writing to the MI Karting Committee. Any approved changes will be announced in the monthly Motorsport Bulletin and the effective date will date of publication, unless other wise stated.

8.4. IF In Doubt - Don't.

8.5. The approval of a kart or component is an indication of acceptance solely for the purpose of these regulations and is not to be taken as a guarantee or warranty as to the standard of its design or manufacture, or its fitness or suitability for any use to which it may be put

8.6. Karts must be presented for Scrutiny on suitable trolleys and in a clean and "ready to race" condition (subject to exemption from the Scrutineers).

8.7. Scrutiny.

8.7.1. The Scrutineer must be satisfied that the Kart is safe, is of an adequately strong construction, does not include any components of a temporary character and presents no undue hazard to its driver or to other competitors

8.7.2. In particular, steering, brakes and wheels must be adequate for speeds that are likely to be attained.

8.7.3. For all classes, a homologation fiche copy and other documents regarding the equipment being used must be made available by the competitor to the Eligibility Scrutineer on request.

8.7.4. The MI Eligibility Scrutineer is
Mr Patrick Fitzgerald
Phone: 086-3065446.

8.8. Chassis - General.

8.8.1. Chassis must be constructed from magnetic steel tubing. Cross section free.

8.8.2. No additional holes for lightening or any other purposes, with the exception of those

required for seat fixing, may be drilled in the rolling chassis. Only those holes shown in the manufacturers homologation or in the manufacturers official replacement parts list will be allowed.

8.8.3. Chassis Dimensions (Excluding Cadet Class).

8.8.3.1. Wheelbase.

Minimum 101 cm, Maximum 127 cm.

8.8.3.2. The track must be a minimum of two-thirds of the wheelbase.

8.8.3.3. Maximum overall length.

210 cm including bodywork.

8.8.3.4. Maximum overall width.

All classes 140 cm.

8.8.3.5. No part may protrude outside the quadrilateral formed by the front fairing, rear bumper and outer face of the wheels and tyres.

8.8.3.6. No part of the kart shall be higher than 60cm from the ground, except for a structure solely designed as a headrest.

8.8.4. Chassis frame must be permanently marked with a unique number and year of manufacture in an easily recognisable position.

8.8.5. Flooring.

There must be a floor made from rigid material that, as a minimum, stretches from the seat to the front of the Kart. It must be edged on each side by a tube or a rim to prevent the drivers feet from sliding off the floor.

8.9. Suspension.

8.9.1. Any method of suspension either by elastomeric material or by pivots is prohibited.

8.10. Wheels.

8.10.1. The wheels must be equipped with pneumatic tyres (with or without tubes) and the front wheel hubs must be fitted with rolling element bearings.

8.10.2. The attachment of the wheels to the axles/hubs must incorporate locking pins or self locking nuts. Hubs with single retaining bolt feature must not protrude beyond the end of the axle unless designed for that purpose and must incorporate a retaining device. Extended hubs with single or twin retaining bolts designed to overhang the end of the axle are permitted.



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8.10.3. No form of hub nave plate is permitted.

8.11. Tyres.

8.11.1. For all classes permitted tyres area as detailed in Appendix 70 Article 12.

8.11.2. Tyres must be freely available as single units to the open market so that customers are not forced to buy either complete sets or other equipment in order to purchase a particular tyre.

8.11.3. Wet Tyres.

To qualify as a wet tyre, tyres must be moulded in such a way to ensure that grooves are created to leave a minimum of 60% and a maximum of 85% of the total surface area as a raised tread pattern. The grooves to have a minimum depth of 2mm.

8.11.4. Remould tyres are prohibited

8.11.5. The use of tyre heating/ heat retention devices and chemical tyre treatments /compounds is prohibited. In addition no competitor or entrant is permitted to have tyre heating or heat retention devices or chemical tyre treatment/compounds in their possession. The only exception to this rule is Cadet tyres, which may be cleaned, using a hot air gun only, provided that this occurs no less than 30 minutes prior to race start.

8.11.6. Only one set of matched tyres may be used per event.

8.11.7. A photo-ionization detector (PID) with a calibration certificate for isobutylene at 100ppm issued in the preceding twelve months may be used for testing for chemical treatment of tyres.

8.11.7.1. A reading greater than 3ppm on a PID as described in 8.11.7. with the sampling probe within 5mm of any part of the tyre or thread, where the reading is 50% greater than the average readings found from other competitors in the race will be deemed evidence of chemical treatment as per 8.11.5.

8.11.7.2. If chemical treatment of tyres is established before the race, using the above technique or any other, the competitor will not be allowed to race as chemically treated tyres may constitute a safety risk and contravenes 8.11.5.

8.12. Brakes.

8.12.1. Master cylinders must be fitted in a position safe from impact, at least 15cm behind the front bumper or to the rear of the pedals.

8.12.2. Hydraulic pipes must be securely fitted to the chassis upper sides and should be protected against damage.

8.12.3. All Karts must have a secondary cable linkage from the brake pedal to the master cylinder. This cable must be a minimum of 1.8mm in diameter.

8.12.4. Drum brakes are not permitted.

8.12.5. An efficient rear brake disc protective pad is mandatory.

8.13. Steering.

8.13.1. Steering must be controlled by a steering wheel which must have a complete shape.

8.13.2. Flexible steering controls by cable or chain are prohibited.

8.13.3. All parts of the steering must have a method of attachment offering maximum safety (split pins, self locking nuts or burred bolts).

8.13.4. A support collar mounted to the steering shaft below the steering clamp is mandatory.

8.14. Seat.

8.14.1. The seat must be of a type homologated by the FIA /CIK for use in Kart Racing and be fitted in accordance with manufacturers specification. Details on the website: www.cikfia.com

8.15. Pedals.

8.15.1. Whatever the position of the pedals, they must not protrude forward of the chassis or bumper.

8.15.2. The accelerator must be operated by a pedal equipped with a return spring.

8.16. Exhaust.

8.16.1. The exhaust system shall discharge behind the driver and shall not exceed a height of 45cm.

8.16.2. The terminal part of the silencer must not cross the quadrilateral formed by the outside of the wheels and the front and rear bumpers. It must point sideways and downward so as not to present a hazard.

8.16.3. There must exist protection to prevent any contact between the driver in the normal driving position and the exhaust system.

8.17. Fuel.

8.17.1. The general definition of fuel is stated in appendix 2 Article 28.2.

8.17.2. Fuel measurement may be carried out at events by a Digatron fuel tester.

8.17.3. The use of devices fitted to fuel lines such as "Powerplus", "Proplus", or " Carbonflow" is prohibited.



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8.17.4. The use of power boosting or octane additives in any fuel is prohibited. Some power/octane boosters may cause cancer.

8.17.5. All classes must ensure that a minimum of one litre of fuel is present at the end of the race to enable a fuel test to be conducted. The penalty for non-compliance will be exclusion from the results of that race.

8.17.6. In addition all fuel used must be purchased from the filling station designated in the Supplementary Regulations for each event. Failure to do so will result in non-compliance.

8.17.7. The only compliant engine lubricant for 2-stroke engines is fully synthetic Shell Advanced Racing X.

8.17.8. For all MI Kart Racing Championships as defined in Article 1.5. of these regulations a kart or karts may be chosen at random for fuel testing at any time.

8.18. Fuel Tank & Pipes.

8.18.1. The fuel tank must be fixed in such a way that neither it nor the fuel pipes, which must be flexible, present any danger of leakage during the competition.

8.18.2. For non-gearbox the petrol tank must be mounted within the area of the chassis frame.

8.18.3. If plastic is used for the tank it must be of a type suitable for carrying fuel.

8.18.4. In the 125cc and 250cc classes the tank capacity must not exceed 15 litres.

8.18.5. The tank must supply the fuel pump with fuel under normal air pressure only.

8.19. Bumpers.

8.19.1. The Kart must have protection (bumpers) at the front and rear which shall have a strength and construction appropriate to their function. All tubing used must be of magnetic steel.

8.19.2. A homologated FIA/CIK Rear Protection System in accordance with FIA/CIK regulations used in lieu of a rear bumper, is only permitted for FIA/CIK chassis homologated from 1/1/06. This does not apply to Cadet Karts.

8.20. Bodywork.

8.20.1. Bodywork is permitted on all Karts as specified in these regulations and per FIA/CIK regulations. No part of the bodywork may be used as a fuel tank or to carry ballast.

8.20.2. All bodywork must be soundly constructed from non-metallic material, and if plastic, be shatter proof. It must be designed to

provide maximum safety for the driver and other competitors both during normal racing or an accident and must not present any sharp edges.

8.20.3. Nothing, including any form of bodywork, may be vertically above any portion of the drivers head, body or limbs.

8.20.4. No form of "skirt" is permitted and only the tyres must come into contact with the circuit.

8.20.5. Front fairings.

8.20.5.1. Front fairings are mandatory for all classes. The purpose of the front fairing is to prevent, in conjunction with the rear bumper the potential for wheel over wheel incidents.

8.20.5.2. Installation of the fairing must be such that that the front bumper complies with the regulations and if the fairings were removed would be the foremost part of the Kart.

8.20.5.3. Fairings must not extend beyond the plane through the outside of the front wheels with the front wheels in the straight ahead position with wheels in the outermost position, nor extend beyond the plane through the top of the tyres.

8.20.5.4. Fairings must comply with the dimension requirements shown in the diagrams in the current FIA/CIK Yearbook of Karting.

8.20.5.5. On gearbox karts the fairings must not exceed the maximum width of the kart.

8.20.6. Side Protection.

8.20.6.1. Side protection is mandatory for all classes. The purpose is to prevent the possibility of wheel to wheel interlock For non-gearbox Karts this will be accomplished by the fitment of side-pods as defined in 8.20.6.3 below Gearbox Karts must use side-bars with or without bodywork or side pods as defined in 8.20.6.4 below.

8.20.6.2. Any form of side protection must not be nearer the ground than the floor tray and must have adequate clearance between any part of the side-bar(s), or side-pods and the tyres, but any clearance must not exceed 100mm with wheels in the straight ahead position.

8.20.6.3. Non-gearbox karts.

8.20.6.3.1. Side-pods must not extend beyond the plane through the top of the front and rear tyres, nor extend beyond the plane through the outside of the front and rear tyres with the front tyres in the straight ahead position and with the wheels in the outermost position.

8.20.6.3.2. The pod on the engine side may incorporate the possibility of being used as a noise box for the engine intake.



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8.20.6.3.3. The surface of the pods must be uniform and smooth and not incorporate any holes other than those required for fixing or the passage of air.

8.20.6.3.4. Side-pods must be securely fixed to the chassis frame by a minimum of two points each, and may incorporate a lateral support on the outer surface. Lateral support bars must be positively attached to the chassis by a minimum of two fixing points.

8.20.6.3.5. Side-pods must cover at least 2/3 of the width of each rear tyre at all times

8.20.6.4. Gearbox karts.

8.20.6.4.1. Protection must not extend beyond the plane through the top of the front and rear tyres, nor beyond the plane through the outside of the front and rear tyres with the front wheels in the straight ahead position and with the wheels in the outermost position.

8.20.6.4.2. Where a single side-bar is used, the height of the bar must not exceed that of the top of rear axle.

8.20.6.4.3. All side-bar tubing must be a minimum of 15mmdiameter magnetic steel of adequate wall strength, and must cover a minimum of 2/3 of the width of each rear tyre all times.

8.21. Numbering and Number Plates.

8.21.1. Number Plates and numbers must be as per Appendix 70 Article 12.

8.21.2. Number plates must be at least 20cm by 20cm fitted to the front and rear of the kart. Numbers must be at least 15cm high and 2.5cm stroke width.

8.21.3. No club may alter the requirements concerning the numbering of karts.

8.21.4. MI reserves the right to nominate special "numbers" such as IRL, O and C.

8.21.5. The first nine in all MI Championships may use their finishing position number for the following year should they continue to race in the same class and in the same type of event. MI has withdrawn No 24 and No 98 from all classes.

8.22. Timing Transponders.

8.22.1. All drivers must have their own transponder for each class.

8.22.2. All transponders to be placed on the rear of the kart seat in a vertical position

8.23. Weight.

8.23.1. The minimum weight limit is the combined weight of driver, kart and engine with

kart in complete racing trim as at the end of each race or heat or timed qualifying.

8.23.2. After a kart and driver have competed in a race or a heat or timed qualifying it is not permissible to alter the weight of the kart or driver in any way before being weighed. The driver must have his crash helmet and goggles with him for weighing.

8.23.3. The kart must be weighed with all four wheels in contact with the weighing mechanism. A kart and driver may make a maximum three attempts to meet the specified minimum weight.

8.23.4. Ballast.

If it is necessary to use ballast in order to achieve a minimum weight, no single piece of ballast may exceed 5kg. The ballast can only be fixed to the seat. The fixing of the ballast to the kart seat must be by bolts (minimum of 8mm diameter) with locknuts with washers (minimum o.d. 20mm). There must be at least 2 bolts, suitably spaced for each piece of ballast. If two pieces of ballast are placed on top of each other they must be retained by four bolts (two for each piece). Cable ties are not suitable for securing ballast.

8.23.5. No solid object (excluding jewellery, dog tags) may be carried on the drivers person.

8.23.6. Plugs and plug spanner may be secured on the kart in a suitable receptacle, secured to the kart in a manner acceptable to the Scrutineer.

8.23.7. Clubs may run classes with heavier weight limits, if so specified in the Supplementary Regulations, but may not reduce weight limits.

8.24. Rear Red Light.

8.24.1. A rear red light is mandatory on all karts. It must be fitted forward of the rear bumper, be rear facing, securely fixed and visible to following drivers. The rear red light must be in working condition throughout the meeting. It must be switched on in wet weather conditions on the decision of the CoC. Failure to have alight fitted in working condition may result in a penalty in accordance with GCR 142.

8.24.2. Deleted.

8.24.3. Deleted.

8.25. Engines and Transmissions.

8.25.1. The Scrutineer must be satisfied that the engine, transmission, exhaust system and all associated parts are installed in a workmanlike manner and will perform safely and present no undue hazard to the driver or other competitors.



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8.25.2. Chain guard/ Driving Belt.

8.25.2.1. A chain guard above the transmission is compulsory. The top of the engine sprocket, the drive itself (i.e. chain or belt) and axle sprocket, gear etc. must be covered from above down to the centre line of the rear axle sprocket. In addition there must be efficient side protection to prevent the driver from inserting a finger in the transmission where the drive meshes with the equivalent of the engine sprocket.

8.25.2.2. 125 Gearbox and SuperProKart chain guard must be fitted as deemed appropriate by the eligibility Scrutineer.

8.25.2.3. The gear change in Formula 125 must be mechanical with no electrical, electronic, hydraulic or pneumatic operation or assistance. No form of ignition to aid gear changes is permitted e.g. Continuous Traction System (CTS).

8.25.3. Engines.

8.25.3.1. The engine is understood to be the sole propelling unit of the kart in running order. It may be of two or four stroke cycle type.

8.25.3.2. Only engines readily available through normal commercial channels may be used. Supplementary Regulations may vary this rule.

8.25.3.3. Forced induction in any form is not permitted.

8.25.3.4. Power valves are allowed in some classes only. All electronic /microchip methods of operation are forbidden.

8.25.3.5. Any form of variable ignition advancing or retarding systems are forbidden. The use of programmable electronic engine management systems, which can be varied while the kart is in motion, is also forbidden.

8.25.3.6. For any engine that is homologated with a dry clutch, it is prohibited to use any form of lubrication on the clutch components.

8.25.4. Only sealed leak proof batteries are permitted. Proprietary lead acid, sealed for life, properly mounted and protected, are acceptable.

8.25.5. All karts capable of self-starting, either by way of a starter motor being fitted to the engine, or by means of a pull cord, must be fitted with an operational on/off ignition switch clearly marked with the "OFF" position.

8.25.6.

The use of tank tape adhesive material or any other discardable materials on radiators of water cooled engines is allowed. These must not be discarded while the kart is in motion.

8.26. Transmissions.

8.26.1. All classes must use axles of magnetic material.

8.26.2. The drive shall always be to the rear wheels. Any method may be used provided no differential is incorporated.

8.26.3. All methods of chain oiling or greasing whilst the kart is in motion are forbidden. Supplementary Regulations may vary this rule.

8.27. Data logging.

Data logging is permitted. Any information logged may be viewed on request by the scrutineers. A competitor may be requested to fit and carry an MI owned data logger on his/her kart during a practice or race.

8.28. Newcomers / Black Plates.

A Clerk of the Course or the Stewards of the Meeting, acting as a body, may waive or mitigate a penalty for infringement of the technical regulations. This waiver may only be applied during the period in which the driver is competing whilst carrying a black coloured number plate, and to those drivers who are not eligible for an award.

9. CLASSES.

9.1. Class regulations are detailed below and must be read in conjunction with the regulations above.

10. CLASS TECHNICAL REGULATIONS.**10.1. JUNIOR CADET CLASS.****10.1.1. Junior Cadet - Comer Engines.**

10.1.1.1. Chassis. The chassis must remain as homologated in all respects and may only be subject to approved modifications for safety reasons.

10.1.1.2. Only additions permitted along with modifications solely concerned with their fitment are: Seats, Nassau panels, Front fairing, Ballast to achieve the required weight or for repair purposes.

10.1.1.3. The following may not be added:

- Fuel filters, except as specified under Article 10.1.3.1.
- Any part not homologated except as stated above.

10.1.2.1. Dimensions.

Wheelbase: 900mm +/- 5mm.

Overall length: 1650mm (max).

Tubing: 25mm or imperial equivalent unpainted (BSI tolerance allowed).

Steering: Castor, camber, and king-pin inclination must be fixed as homologated.



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10.1.2.2. Floor.

There must be a rigid, flat floor from the seat to the front and side chassis side members, secured to them, so that the tubes prevent the drivers feet from sliding off. If perforated, the holes must not have a diameter exceeding 10mm. Any perforation of the floor must be a production feature and be homologated as such.

10.1.2.3. Side Pods.

Must conform to Appendix 70 Article 8.20.

10.1.2.4. Bumpers.

Must conform to Appendix 70 Article 8.19. Consist of two horizontal tubes. The upper having outer extension forming a closed loop, with two link tubes to the chassis anchorage points at least 450mm apart at any point. The radius of the outer extensions is free but it should not be less than 2 1/2 times the tube outside diameter.

- Have the upper tube and uppermost extension element 225 ± 25 mm above the ground in dry configuration.
- Have the lower tube connected between the two uprights and 75mm ± 25 mm above the ground in dry configuration.
- Have the lower tube secured at all times if bolted.

10.1.2.5. Front Fairing.

Must conform to Appendix 70 Article 8.20. For the Cadet classes the vertical front surface referred to in MSA Yearbook regulations K183 must be a minimum of 200mm x 35mm.

10.1.2.6. Torsion Bars.

Karts homologated with rear torsion bars must be raced with the bars in place and locked at all times. Front and side torsion bars are not permitted.

10.1.2.7. The following are specifically prohibited; Parts made of Kevlar, carbon fibre (other than seats and floor pan), magnesium, titanium.

10.1.2.8. Seat. The seat is free. Additional bolt on seat stays/mountings are permitted to a maximum of one per side, position free. Bolt fixings must be used at each end of the seat stays.

10.1.3. Carburettor.

10.1.3.1. The carburettor for the Comer W60 engine are:

1. The Tillotson type HL-392A, stamped "MI" complete with Tillotson in-line fuel filter type FS-1.
2. Tillotson HW-17 complete with Tillotson in-line fuel filter type FS-1.

Both must remain unmodified and conform in all respects to the master carburettor as held by MI with the exception of marks on the outside for identification purposes only.

10.1.4. Engine.

10.1.4.1. Power unit Comer W60 60cc two-stroke engine equipped with recoil starter, ignition, centrifugal clutch, carburettor, air box and exhaust system.

10.1.4.2. The power unit as raced must conform in all aspects with the homologation fiche and may be checked at any time during the event. No addition of or other change of material is permitted. No modification or tuning for whatever purpose are allowed except where expressly permitted. Where specific dimensions are not given for the engine and its supplied accessories in the homologation fiche, dimensions will be checked against a control engine held by MI.

10.1.4.3. The following repairs are specifically permitted.

10.1.4.3.1. Repair of damaged threads in the crankcase and cylinder with helicoils. The repair of the cylinder head spark plug thread is not permitted.

10.1.4.3.2. Should the throttle cable bracket on the crankcase be broken, the original bracket may be welded back in its original position.

10.1.4.3.3. The only replacement parts which will be allowed are those supplied by and listed on the manufacturers parts list for the W60 60cc homologated engine (except for plug caps, which may be replaced by parts of other commercial manufacture and which must be directly equivalent).

10.1.4.4. The only spark plugs permitted are Bosch W2AS or Bosch W08AS.

10.1.4.5. Deleted. See 8.17.7.

10.1.4.6. Engine management equipment/systems are prohibited.

10.1.4.7. The use of a rev counter and data logging is permitted.

10.1.4.8. Engine ignition timing is free, provided that when it is set and locked, timing cannot be varied by the driver when the kart is in motion.

10.1.4.9. Engine Sealing.

10.1.4.9.1. All engines must be sealed with an official seal to prevent modification. All engines are issued with an official identity card. It is the competitor's responsibility that the numbers inscribed on the engine and seal must correspond with those on the identity card at all times.

10.1.4.9.2. Only authorised dealers will be issued with seals for use during maintenance of the engines. The identity card must be filled in and signed by the authorised dealer.



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10.1.4.9.3. The engine must be presented at scrutineering with the official class seal intact and the identity card lodged with the scrutineer if requested to do so. The card must be collected by the competitor at the end of the race meeting.

10.1.4.9.4. Should a seal become damaged, loose or lost during racing it must be reported to the Chief Scrutineer of the meeting before leaving parc ferme. To allow the competitor to continue racing the scrutineer may at his discretion re-seal the engine with an official MI seal. The new seal number must be entered on the engine's identity card and signed by the scrutineer. The engine must be taken to an official dealer with the MI seal intact to be re-sealed with an official class seal before competing at the next race meeting.

10.1.4.10. Permitted Modifications.

10.1.4.10.1. A wet box or splash guard may be attached to the W60 air box provided this in no way modifies the size or shape of the intake trumpet or creates a ram effect. The W60 air box cannot be modified to aid in the attachment of wet box and attachment must be of a non-permanent type i.e. tape, cable ties.

10.1.4.11. Transmission.

10.1.4.11.1. The drive must be direct, so that the crankshaft and rear axle are connected by a single length of chain.

10.1.4.11.2. All methods of chain oiling and greasing while the kart is in motion are forbidden.

10.1.4.11.3. A guard covering the transmission is compulsory and must comply with Appendix 70 Article 8.25.2.

10.1.4.11.4. Axle. Solid magnetic parallel bar of 25mm diameter. Horizontally split quick release bearing mountings are not permitted.

10.1.4.11.5. Sprockets. Free.

10.1.4.12. Brakes.

10.1.4.12.1. Mechanical or hydraulic operation, solid disc, operating on rear axle only. Interruptions on the friction surface (drilling, slots etc.) are permitted on homologated systems only and must be specified on the homologation form.

10.1.4.12.2. The brake linkage must be duplicated.

10.1.4.13. Wheels and Tyres.

10.1.4.13.1. Wheels may be of two piece or mono aluminium construction i.e. spun aluminium, diecast aluminium or sandcast aluminium.

10.1.4.13.2. Hubs may be separate or integral.

10.1.4.14. General.

10.1.4.14.1. It is the responsibility of each competitor to ensure that the equipment used is in compliance with these regulations and that up to date documentation is in their possession.

10.1.4.14.2. Weight and Tyres are listed in Appendix 70 Article 12.

10.2. KF3 CLASS.

10.2.1. Introduction.

10.2.1.1. The Class is the MI Championship Class for the Junior Driver of the Year (the Neil Shanahan Trophy).

10.2.1.2. The class mimics the CIK European class I in all respects except for rear bumper, maximum kart width dependent on tyres, where a tyre is selected from the CIK list for use in the class.

10.2.1.3. The class affiliation is CIK/MI.

10.2.2. Chassis.

10.2.2.1. CIK homologation for KF3 or JICA.

A chassis once homologated is eligible for six years use. NEW CIK homologated chassis have been introduced from 1/1/06 which are eligible for this class.

10.2.2.2. The maximum width is 1400mm.

10.2.2.3. Bodywork.

When wet tyres are fitted only CIK regulations apply.

10.2.2.4. Bumpers.

CIK homologated rear protection systems or MI Specification rear bumper is permitted.

10.2.3. Engine.

10.2.3.1. CIK homologated. Must be TM model MF1 40/M/15 manufactured prior to 31/12/2009.

10.2.3.2. Carburettor.

The unit designated by the CIK/FIA for the KF3 class is the Tillotson HW-10A carburetor. CIK Homologation number 6/C/15.

10.2.3.3. Ignition.

10.2.3.3.1. Homologated ignition system with a specific limiter at 14,100 rpm. Any electronic system allowing an auto-control of the parameters of the function of the engine while the kart is in motion is prohibited.

10.2.3.3.2. An engine cut-out switch is mandatory. The switch must be located in such a way that it is possible for the driver, normally seated in his driving position, to trigger it.

10.2.3.3.3. A sealed (maintenance free) battery must be fitted.



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10.2.3.4. Exhaust.
CIK homologated.

10.2.3.5. Intake Silencers.
CIK homologated with filter, maximum intake diameter(s) 23mm. Mounted airtight to the carburettor. Mounting method free.

10.2.3.6. Fuel Tank.
As per CIK regulations.

10.2.3.7. Clutch.
As per CIK Article 19.

10.2.3.8. Transmission.
As per CIK regulations.

10.2.3.9. Brakes.
CIK homologated.

10.2.4. Tyres and Wheels.
10.2.4.1. Wheels are free. It is compulsory to use safety screws to prevent the tyre from leaving the rim (external part only).

10.2.4.2. Tyres.
See Appendix 70 Article 12.

10.2.5. Weight.
See Appendix 70 Article 12. The minimum weight of the kart (without fuel) to conform to CIK regulations. Maximum kart weight without driver is 110kg at any time.

10.2.6. General.
10.2.6.1. Plate Colour.
See Appendix 70 Article 12.

10.2.6.2. Age.
See Appendix 70 Article 12.

10.2.6.3. Data logging is permitted. Any information logged may be viewed on request by the Scrutineers. A competitor may be requested to fit and carry an MI owned data logger on his kart during practice or race.

10.2.6.4. Homologation fiche copy and other eligibility documents regarding the equipment being used must be made available by the competitor to the Scrutineers on request.

10.2.6.5. Sealing of engines at Scrutineering to follow CIK and/or MI practice.

10.2.6.6. Modifications.
10.2.6.6.1. Any modification is forbidden if it is not explicitly authorised.

10.2.6.6.2. Modification includes any operation likely to change the initial aspect, the dimensions, the drawings or the photographs of an original homologated part.

10.2.6.7. Adjunction of material or parts.

10.2.6.7.1. Any adjunction or fixation of material or of parts is forbidden if it is not expressly authorised. Removed material may not be used again.

10.2.6.7.2. Rebuilding the frame geometry, following an accident, is authorised by adjunction of the materials necessary for the repairs (additional metal for welding etc.).

10.2.6.7.3. Other parts, which may be worn-out or damaged, may not be repaired by or fixation of material, unless the regulations specifically authorise it.

**10.3 FORMULA ROTAX MINIMAX
& FORMULA ROTAX JUNIOR MAX**

10.3.1 Chassis.
Any chassis conforming to MSA Direct Drive regulations.

10.3.2. Engine.
10.3.2.1. The eligible engine in this class is Rotax FR125 Junior Max with restrictors. The Junior Max to Senior Rotax FR125 Max fiche plus extensions for the Junior, including the cylinder.

10.3.2.2. The official engine supplier is JAG Engineering, and its approved dealers. This engine is a single cylinder, liquid cooled, reed valve, two-stroke.

10.3.2.3. All engines must be sealed between cylinder, crankcases, cylinder head and the reed valve block with an official seal to prevent modification.

10.3.2.4. All engines must have an official identity card. The numbers inscribed on the engine and seal must correspond those on the identity card at all times. The identity card must be filled in and signed by an authorised dealer.

10.3.2.5. Only authorised dealers with seals for use during maintenance of the engines.

10.3.2.6. The engine must be presented at scrutineering with the official class seal intact. The identity card must be available for presentation to a scrutineer at any time during the meeting.

10.3.2.7. Should a seal become damaged, loose or lost during racing it must be reported to the Chief Scrutineer of the meeting before leaving parc ferme. To allow the competitor to continue racing the scrutineer may, at his discretion, reseal the engine with an official MI approved seal. The new seal number must be entered on the engine's identity card and signed by the scrutineer. The engine must be taken to an approved JAG Engineering dealer with the seal intact to be resealed with the official class seal before competing at the next race meeting.



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10.3.3. Modification.

10.3.3.1. The engine and its ancillaries not be modified in any way and must conform to the official fiche.

10.3.3.2. The engine must be raced in standard form as manufactured by Rotax.

10.3.3.3. Filing, grinding, polishing, surface treating, machining or lightening any component is expressly forbidden.

10.3.3.4. The addition of any material to any component is not allowed.

10.3.3.5. All parts used in or on this engine must be of original manufacture or source except where expressly allowed. The engine is to be used with air box, carburettor, fuel pump, radiator, battery, wiring loom, ignition system and exhaust system as supplied by the manufacturer.

10.3.3.6. Position and method of mounting the battery, wiring loom, exhaust system and fuel pump are free, provided they are securely fixed to the satisfaction of the scrutineers of the meeting and in accordance with MI regulations.

10.3.3.7. The radiator must be fitted to the right hand side of the engine using standard hoses and connections as supplied by Rotax.

10.3.3.8. Filing of the crankcase to allow easy water connections is allowed. Fitting of helicoils and inserts to repair damaged threads is allowed, providing such repairs are not used to derive any benefit other than rectification of damage.

10.3.3.9. The use of thermal barrier coating / ceramic coatings on or in the engine is not allowed.

10.3.3.10. Temperature senders must only be fitted either to the hole provided in the cylinder head or attached to a cooling hose. Drilling cooling hoses is not allowed.

10.3.4. Carburettor.

10.3.4.1. The specified carburettor is Dell'orto VHSB 34 QD or QS.

10.3.4.2. All parts of the carburettor, including the body, are to be unmodified and run as supplied by Rotax. With the exception of markings on the outside of the body for identification purposes only.

10.3.4.3. The carburettor must have VHSB 34 (cast in the body) and QD or QS (stamped on the body).

10.3.4.4. All parts must comply with the official fiche.

10.3.4.5. The only adjustments allowed are the main jet, external airscrew, throttle stop adjustment screw and needle position on the five grooves provided. Needle jet atomiser FN266. Choke jet 60. Idle jet 30, idle jet emulsion tube 30. Needle K27 or K98 (Rotax Part no 261191). Float needle valve 150. Slide 40. Floats 5.2gr. Atomiser type 2. Alternative idle jet 60, idle jet emulsion tube 60 and 3.6gr floats. The venturi must have 34 cast and 12.5 or 8.5 stamped on top of the venturi.

10.3.4.6. Throttle cable and adjusters are free.

10.3.4.7. It is permitted to use a single length of vent tube looped across the two air vents of the carburettor with a hole or slot cut on the side of the vent tube at the top of the loop.

10.3.5. Intake Silencer.

10.3.5.1. Air box type 1 or type 2 b.

10.3.5.2. The air box supplied with the engine must be unmodified and used with its filter in place.

10.3.5.3. The air box must be used with its inlet points facing downward except when a race is declared wet and it can then be inverted to allow its intakes to face upwards.

10.3.6. Exhaust System.

10.3.6.1. The exhaust system and silencer may not be modified in any way except for the addition of brackets to allow easy fixing. The pop rivets securing the silencer end plate may be replaced with screws. The use of a jubilee clip to secure the end plate pop rivets or screws is allowed. These modifications are allowed providing there is no benefit in performance.

10.3.6.2. It is permitted to paint the exhaust system with black paint. The use of any other coating or plating is not allowed.

10.3.6.3. It is permitted to make minor repairs by welding or brazing to the exhaust system provided there are no alterations to the original dimensions.

10.3.7. Transmission.

10.3.7.1. Transmission is direct from engine to axle via a single length of chain. The clutch supplied with the engine must be used with all of its standard components.

10.3.7.2. The clutch must be triggered/engaged at a maximum engine speed of 3,000rpm so that the kart with driver should move forward.

10.3.8. Brakes.

10.3.8.1. Hydraulic disk brake operating on rear wheels only.



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10.3.9. Tyres.

See Appendix 70 Article 12.

10.3.10. Weight.

See Appendix 70 Article 12.

10.3.11. Plate Colour.

See Appendix 70 Article 12.

10.3.11. General.

10.3.11.1. An on/off ignition kill switch must be fitted.

10.3.11.2. General Junior Max: This class will run to MSA Specification. Weight, Plate colour and Tyres as per Appendix 70 Article 12.

10.4. KF2 CLASS.

Deleted.

10.5. FORMULA 125 KZ2.**10.5.1.** Specification as per CIK/FIA KZ2.**10.5.2. KZ2 - Formula 125****10.5.2.1. Introduction.**

10.5.2.1.1. The class mimics the CIK KZ2 class but with some relaxations to permit existing National engines to be used by replacing the cylinder head and carburettor on older engines and permits old style karts in the ICC category. Engine regulations have been brought into tighter conformity to CIK KZ2 from 2005 e.g. port heights.

10.5.2.2. Chassis.

Any chassis complying with MI regulations for gearbox karts

10.5.2.3. Bodywork.

Complying with MI regulations for gearbox karts. Bubble type Nassau panels and "wedge" shaped front fairings are not permitted.

10.5.2.4. Engine.

10.5.2.4.1. Any water cooled single circuit single cylinder reed valve engine homologated by the CIK for the KZ2 class, with the exception that pre 2004 engines may be a variant with a cassette gearbox.

10.5.2.4.2. Maximum cylinder cubic capacity 125cc.

10.5.2.4.3. An additional inner circuit for the normal functioning of a thermostat is allowed.

10.5.2.4.4.

The original parts of the homologated engine must always comply with and be similar to the photographs, drawings and physical height described on the homologation form.

10.5.2.4.5. Tuning regulations.

10.5.2.4.5.1. All modifications to the homologated engine are allowed except:

Inside the engine:

- Stroke
- Bore ((outside maximum limits)
- Connecting rod Centre Line
- Number of transfer ducts and inlet ports in the cylinder and crankcase.
- The total exhaust opening angle is 199 degrees maximum as per CIK KZ2 regulations.

Outside the engine

- Number of carburettors (1 only)
- Diameter of choke
- External characteristics of the fitted engine except for machining necessary for the application of bar code stickers (via homologation extension)
- The reed valve box (dimensions and drawings) must be according to homologation.

10.5.2.4.5.2. Modification of the external appearance of the engine does not include the fixations of the carburettor, of the ignition, of the exhaust, of the clutch or of the engine itself, provided that their homologated position is not modified.

10.5.2.4.5.3. Cylinder head volume must be a minimum of 13cc using CIK specification plug insert and measured as the CIK method. Ambient temperature is defined as any temperature between +5°C and +50°C.

10.5.2.4.5.4. Spark plugs are free but must be unmodified and the thread must not protrude into the combustion chamber. Modification to the spark plug thread is not permitted except for helical repair so long as the repair is to the full depth of the thread.

10.5.2.4.5.5. Ignition system must be to CIK homologation for the class.

10.5.2.4.5.6. All systems of injection and /or spraying of products other than permitted fuel are forbidden.

10.5.2.5. Silencing.

10.5.2.5.1. The Air Intake / Silencer Box must be CIK-FIA Homologated. Air intakes / tubes maximum diameter is 30 mm.

10.5.2.5.2. Exhaust.

Homologated exhaust is not mandatory. The magnetic steel metal thickness must be 0.75mm minimum.

10.5.2.6. Carburettor.

10.5.2.6.1. Carburettor made of aluminium, with a venturi type diffuser with a maximum diameter of 30mm round. The carburettor must be Dell'orto VHSB 30(CS) or (BS) Code 9303. The carburettor must remain strictly original.



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10.5.2.6.2. The only settings allowed may be made to: the slide, the needle, the floats, the float chamber, the needle shaft jets and the needle kit, subject to all the interchanged parts being of Dell'orto origin.

10.5.2.6.3.

The incorporated petrol filter and the plate (part no 28 on the technical drawing no 7) may be removed; if they are kept, they must be original.

10.5.2.7. Transmission.

Gearbox must remain as homologated by the CIK/FIA. Changes to gear ratios are not permitted, and the number of gears must remain six. Control must remain mechanical without any servo system or ignition cut system.

10.5.2.8. Brakes.

Brakes as MSA Yearbook K135-137.

10.5.2.9. Tyres.

A70 Article 12. The class is limited to 5" diameter wheels with a maximum tyre width of 7.1".

10.5.2.10. Weight. As per Appendix 70 Article 12.

10.5.2.10. Number Plates. As per appendix 70 Article 12.

10.5.3. Class Formula 125 National.
Deleted.

10.6. FORMULA ROTAX 125 MAX CLASS.

10.6.1 Chassis.

Any chassis conforming to MSA Direct Drive regulations.

10.6.2 Engine

10.6.2.1. The eligible engine is the Rotax FR125 Max.

10.6.2.2. The official engine supplier is JAG Engineering and its approved dealers.

10.6.2.3. The engine is a single cylinder, liquid cooled, reed valve two-stroke.

10.6.2.4. All engines must be sealed between cylinder and crankcases with an official seal to prevent modifications.

10.6.2.5. All engines must have an official identity card.

10.6.2.6. The numbers inscribed on the engine and seal must correspond with those on the identity card at all times. Only authorise dealers will be issued with seals for use during maintenance of the engines. The identity card must be filled in and signed by an authorised dealer.

10.6.2.7. The engine must be presented at scrutineering with the official class seal intact.

The identity card must be available for presentation to a scrutineer at any time during the meeting.

10.6.2.8. Should a seal become damaged, loose or lost during racing it must be reported to Chief Scrutineer of the meeting before leaving parc ferme. To allow the competitor to continue racing the scrutineer may at his discretion reseal the engine with an official seal. The new seal number must be entered on the engine's identity card and be signed by the scrutineer. The engine must be taken to an approved JAG Engineering dealer with the seal intact to be resealed with the official class seal before competing at the next race meeting.

10.6.2.9. Modifications.

10.6.2.9.1. The engine and its ancillaries may not be modified in any way and must conform to the official fiche.

10.6.2.9.2. The engine must be raced in standard form as manufactured by Rotax.

10.6.2.9.3. Filing, grinding, polishing, surface treating, machining or lightening of any component is expressly forbidden.

10.6.2.9.4. The addition of material to any component is not allowed.

10.6.2.9.5. All parts used in or on the engine must be of original manufacture or source except where expressly allowed.

10.6.2.9.6. The engine is to be used with air box, carburettor, fuel pump, radiator, battery, wiring loom, ignition system and exhaust system as supplied by the manufacturer.

10.6.2.9.7. Position and method of mounting the battery, wiring loom, exhaust system are free, providing they are securely fixed to the satisfaction of the scrutineers of the meeting and MI regulations.

10.6.2.9.8. The radiator must be fitted to the right hand side of the engine using standard hoses and connections as supplied by Rotax. Filing of the crankcase to allow easy fitting of water connections is allowed.

10.6.2.9.9. Fitting of helicoils and inserts to repair damaged threads is allowed, provided such repairs are not used to derive any benefit other than rectification of damage.

10.6.2.9.10. The use of thermal barrier coatings / or ceramic coatings on the engine or the exhaust system is not allowed.

10.6.2.9.11. Temperature senders must only be fitted to the hole provided in the cylinder head or attached to a cooling hose. Drilling cooling hoses is not allowed.

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10.6.2.10. Carburettors.

10.6.2.10.1. Dell'orto VHSB 34 QD or QS.

10.6.2.10.2. All parts of the carburettor including the body are to be unmodified and run as supplied by Rotax. The carburettor must have VHSB34 (cast in body) and QD or QS (stamped on body).

10.6.2.10.3. All parts must comply with the official fiche.

10.6.2.10.4. The only adjustments allowed are the main jet, external air screw, throttle stop adjustment screw, and needle position on the grooves provided. Needle jet atomiser Type 2 FN266, Choke jet 60, Idle jet 30, Idle jet emulsion tube 30, Needle K27 or (Rotax Part no 261191), Float needle valve 150, Slide 40 are mandatory.

10.6.2.10.5. Idle jets, idle jet emulsion tubes and floats may not be mixed and only used in one the two following combinations :

- Combination 1: Idle jet 30, idle jet emulsion tube 30, floats 5.2gr
- Combination 2: Idle jet 60, idle jet emulsion tube 60, floats 3.6gr.

10.6.2.10.6. The venturi must have 34 cast and 8.5 or 12.5 stamped on the top of the venturi.

10.6.2.10.7. Throttle cable and adjusters are free.

10.6.2.10.8. It is permitted to use a single length of vent tube looped across the two air vents of the carburettor with a hole or slot cut the side of the vent tube at the top of the loop.

10.6.2.11. Intake Silencer.

10.6.2.11.1. Air box type 1 or 2 may be used. The air box supplied with the engine must be unmodified and used with its filter in place.

10.6.2.11.2. The air box must be used with its inlet points facing downward except where a race is declared wet and it can then be inverted to allow its intakes to face upwards.

10.6.2.12. Exhaust System.

10.6.2.12.1. The exhausts system and silencer may not be modified in any way except for the addition of brackets to allow easy fixing.

10.6.2.12.2. The pop rivets securing the silencer end plate may be replaced with screws. The use of a jubilee clip to secure the end plate pop rivets or screws is allowed. These modifications are allowed provided there is no benefit in performance.

10.6.2.12.3. It is permitted to paint the exhaust system with black paint. The use of any other coating or plating is not permitted.

10.6.2.12.4. It is permitted to make minor repairs by welding or brazing to the exhaust system provided there are no alterations to the original dimensions.

10.6.2.13. An on/off ignition switch must be fitted as per Article 8.25.5.

10.6.2.14. Transmissions.

10.6.2.14.1. Transmission is direct from the engine to axle via a single length chain. The clutch supplied with the engine must be used with all of its standard components.

10.6.2.14.2. The clutch must be triggered/engaged at a maximum engine speed of 3,000rpm so that the kart with driver should move forward.

10.6.2.15. Brakes. Hydraulic disk brake operating on rear wheels only.

10.6.2.16. Tyres. As per Appendix 70 Article 12.

10.6.2.17. Weight. As per Appendix 70 Article 12.

10.6.2.18. Number Plate. As per appendix 70 Article 12.

10.6.2.19. Age.

The class is open to any driver aged 16 or over. Transfer from junior class to this senior class as per Appendix 1 Article 9.

10.7. SUPER PROKART CLASS.**10.7.1. Chassis.**

SuperPro chassis as manufactured by Formula 1 Karts.

10.7.2. Brakes.

One hydraulic disk brake to be fitted to the rear axle only.

10.7.3. Axle.

Rear axle size open. Hollow or solid magnetic material only.

10.7.4. Rear Bumper. As per Article 8.19.

10.7.5. Weight. As per appendix 70 Article 12.

10.7.6. Engine and Drive.

10.7.6.1. One Honda GX620 series engine to be fitted. Pull or electric start.

10.7.6.2. No parts other than the standard Honda genuine parts for the engine type to be fitted.

10.7.6.3. The only permitted modifications to the engine are:

- Removal of external parts of the governor mechanism, but the governor mechanism must remain operational inside the engine.



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- The throttle linkage is free.
- The engine crankcase breather is free, if vented outside the engine a catch tank of 500ml minimum must be used.
- Crankshaft/conrod assembly: re-grinding of the standard Honda crankshaft is permitted for repair purposes only. Offset grinding is prohibited.

10.7.6.4. No machining or removal of any part of the engine unit by any means is permitted. The original casting marks must be visible on all surfaces of the engine. The original Honda finish must remain on all parts and components of the engine in their original place unless these regulations specifically state that you are allowed to remove them.

10.7.6.5. Carburettor.

10.7.6.5.1. The standard Honda carburettor as originally fitted to the engine must be used, with the original air filter and choke fitted in its entirety.

10.7.6.5.2. The plastic outer cover may be trimmed around the choke lever/ throttle cable to facilitate clearance of the linkage.

10.7.6.6. Fuel Pump.

A standard Honda fuel pump must be used.

10.7.6.7. Exhaust and Silencer.

10.7.6.7.1. The exhaust manifold and fitment to the engine are free.

10.7.6.7.2. A silencer must be used. It must comply with Article 8.16.

10.7.6.8. Transmission.

10.7.6.8.1. A dry air-cooled centrifugal clutch which cannot be adjusted in position must be used.

10.7.6.8.2. A maximum engagement speed of 2500rpm. engine speed to transmit drive to the rear axle.

10.7.6.9. Bodywork. As per Article 8.20.

10.7.6.10. Sealing and Identity.

10.7.6.10.1. All engines must be sealed between the crankcase and the cylinder heads with a minimum of one seal per cylinder.

10.7.6.10.2. The competitor must ensure that they are in possession of an engine log book bearing the engine number and numbers of each seal.

10.7.6.10.3. Engine repairs, if carried out at events, must be under the supervision of a licensed scrutineer or the nominated Super Prokart engine sealer (as listed below), who will then reseal the engine and amend the log book accordingly.

10.7.6.10.4. The nominated Super Prokart engine sealer/service person is:
Mr David Thompson, Canal Forge, Cloncumber, Ballytue, Naas Co.Kildare.
Phone: 086-8338445
or e-mail dthompson@esatclear.ie

10.7.6.11. Number Plate.

As per Appendix 70 Article 12.

10.7.6.12. Tyres.

As per appendix 70 Article 12.

10.7.6.13. Age.

The class is open to drivers aged 16+ as per appendix 70 Article 12.

10.8 Super 4 Class

10.8.1. A 4 stroke class for Senior Drivers.

10.8.2. Chassis.

10.8.2.1. Any chassis conforming to MI Direct Drive regulations.

10.8.2.2. Rear Axle.

Deleted.

10.8.3. Engine and Drive.

10.8.3.1. One Biland SA250 4 stroke twin 250cc water cooled engine or one Swiss Auto SA250 4 stroke single 250cc liquid cooled engine, fitted with restrictor (25mm) as supplied by Saxon Motorsport..

10.8.3.2. All engines must be sealed between the cylinder head and rocker cover using a MI Approved seal defined in article 10.8.3.8.2.

10.8.3.3. Engine, carburettor including air filter, exhaust, water and electrical systems must be fitted as recommended and as supplied by the approved engine supplier Saxon Motorsport in accordance with engine fiche. An optional air box is available, to be used with Swiss Auto SA250 Engine, only airbox supplied by approved engine supplier is eligible.

10.8.3.3.1. If required Saxon Motorsport can fit a second cam belt tensioning pulley to the Biland SA250 engine, complete with backplate and cover.

10.8.3.3.2. Fitting of helicolis and inserts to repair damaged threads is allowed, providing such repairs are not used to derive any benefit other than rectification of damage, as deemed by the Scrutineer.

10.8.3.3.3. The exhaust system and silencer may not be modified in any way.

10.8.3.4. Carburettor.

10.8.3.4.1. The following carburettor settings must be used:



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10.8.3.4.1.1. Billand, 2xDell'orto PHBL 22:

- Atomiser D34
- Slide 30
- Needle D31
- Jets: Main 92 Pilot 54

10.8.3.4.1.2. Swiss Auto, 1xDell'orto PHF 30:

- Needle K23
- Jets: Main 112 Pilot 52
- Restrictor 25.00mm as supplied by Saxon Motorsport.

10.8.3.4.2. A carburettor support bracket may be used, such a bracket must be to the satisfaction of the scrutineer.

10.8.3.5. Drive to the rear axle by one length of chain using the clutch supplied with the engine. Only one sprocket to be fitted to the rear axle.

10.8.3.6. Modifications.

10.8.3.6.1. The engine unit must be raced in standard form as supplied by The Approved Engine Supplier, Saxon Motorsport.

10.8.3.6.2. No machining or modifying of any part of the engine unit or its ancillaries is allowed.

10.8.3.7. Bodywork.

10.8.3.7.1. Bodywork to comply with Article 8.19 and 8.20.

10.8.3.8. Eligibility.

10.8.3.8.1. The competitor must ensure that they are in possession of a valid engine log book and tyre log at all times during racing.

10.8.3.8.2. Only MI approved seals may be used. Those being issued by MI or other ASN's, or by Saxon or their Irish agent Ivan Thompson.

10.8.4. Weight.

As per Appendix 70 Article 12.

10.8.5. Number plates.

As per Appendix 70 Article 12.

10.8.6. Tyres.

10.8.6.1. As per Appendix 70 Article 12.

10.8.6.2. Dry tyres to be used for a minimum of two consecutive race meetings. A tyre log book will be supplied for the purpose of controlling tyre usage.

10.8.6.3. Damaged tyres may be changed subject to the scrutineers approval.

10.8.7. Drivers.

Senior class aged 16 +.

10.9. Rotax National.**10.9.1. Chassis.**

Any chassis conforming to MSA Direct Drive regulations.

Formula Rotax 125 Max Class rules 10.6.2.1. to 10.6.2.19. inclusive apply to this class.

10.9.2.20. Drivers can not compete in both Rotax National and Formula Rotax 125 Max at the same meeting.

10.9.2.21. Dry tyres to be used for minimum of two consecutive race meetings. A tyre log book will be supplied for the prpose of controlling tyre usage.

10.9.2.22. Damaged tyres may be changed subject to scrutineers approval.

10.9.2.23. No testing allowed at the track on Saturday prior to raceday. Novece drivers (black plates) will be exempt from this rule.

10.9.2.24. Engines of winning drivers including carburetor and exhaust will be available for sale immediately after A finals to any competitor racing on the day in the class for €3000.00.

10.9.2.25. Competitors wishing to purchase the winning engine as described above must register their intent with the event secretary before racing commences on raceday. If more than one competitor registers such intent a raffle will be held to decide who has the option.



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12. SUMMARY OF PERMITTED CLASS WEIGHT, NUMBER PLATE, AGE & TYRES.

Class	Weight Kgs	Number Plate	Age	Licence	Dry Tyres (c)	Wet Tyres
Junior Cadet (Comer) 60cc	100	Yellow / Black Nos.	8-12	Cadet/Junior	DunlopSL3 Front 10x3.6x5 Rear 11x5.1x5	Dunlop KT3 Front 10x3.6x5 Rear 11x4.5x5
Formula Rotax Minimax 125cc	142	Green / White Nos.	11-16	Cadet/Junior	Mojo D1	Mojo W2
Formula Rotax Junior Max 125cc	148	Red / White Nos.	13-17	Junior/Nat B upwards	Mojo D1	Mojo W2
KF3 125cc	150	Yellow / Black Nos.	12-16	Junior/Nat B upwards	Mojo D2	Bridgestone YKP
Formula Rotax Max 125	165	Blue / White Nos.	15+	Junior/Nat B upwards	Mojo D2	Mojo W2
Formula Rotax Max National 125	175	Green / White Nos.	15+	Junior/Nat B upwards	Mojo D2	Mojo W2
Super Prokart 620 cc	195	Yellow / Red nos.	16+	Junior/Nat B upwards	Maxxis MH	Dunlop KT11
Super 4	175	Yellow / Black Nos.	16+	Junior/Nat B upwards	Maxxis MH	Dunlop KT11
Formula 125 KZ2	180	Red / White Nos.	16+	Junior/Nat B upwards	Vega XM	Dunlop KT11

Notes:

(a) Formula Rotax Minimax: if under 12 years of age 6 finishes in Cadets required.

(b) Formula 125 KZ2 is a gearbox class, all others direct drive..

(c) For slicks (dry tyres) all classes are restricted to one set of tyres, stamped by the scrutineers, per championship round. Only tyres stamped on the day of the event may be used for that day's racing. Formula Rotax Max National and Super 4 classes must use a set of slick (dry) tyres for two consecutive race meetings in accordance with their tyre log book.

(d) Once upgraded to a Senior class a competitor may not revert to a Junior Class.

(e) Junior and Cadet classes are the first 4 classes shown in the table above.



HILLCLIMBS & SPRINTS

APPENDIX 80

REQUIREMENTS FOR THE ORGANISATION OF ALL MI HILLCLIMBS & SPRINTS

This document should be read in conjunction with Appendix 106 Safety Criteria for Temporary Motorsport Venues.

1. DEFINITIONS.

1.1. Hillclimb.

An event in which each vehicle takes the start individually to cover the same course, ending with a finish line situated at a higher altitude than the start line. The time taken to cover the distance between the start and finish lines is the determining factor for establishing the classifications.

1.2. Sprint.

An event similar to a hillclimb but with no gradient of significance.

2. HILLCLIMB/SPRINT COURSE.

The total length of the course should not exceed 2.5kms, and should not be less than 1km.

3. INSPECTION.

3.1. New Venues.

Pre-inspection of all new venues is mandatory. A group of experienced people, appointed by the Hillclimb/Sprint committee together with the appointed Safety Officer, will inspect each proposed venue a minimum of 6 months before an event takes place, to **confirm** its suitability as a Hillclimb or Sprint venue **in accordance with the safety criteria set down by the Hillclimb / Sprint Committee**. The approval of this group will be required for the use of a particular course.

3.2. Existing Venues.

Pre-inspection of existing venues should be carried out by the appointed Safety Officer and an experienced person appointed by the Hillclimb / Sprint Committee 8 weeks prior to the event **to confirm its continued suitability as a Hillclimb or Sprint venue in accordance with the safety criteria set down by the Hillclimb / Sprint committee**. Their recommendations must be included in the drafting of the Safety Plan.

4. SAFETY PLAN.

Clubs running a hillclimb or sprint must submit a safety plan to MI. The Hillclimb/Sprint Committee will provide a copy of the required contents of a safety plan and a guidance timetable for the successful running of an event to each club hosting a hillclimb or sprint event. Some of the criteria in these documents will be mandatory and will be indicated as such.

5. SAFETY OFFICER.

The duties and responsibilities of a Safety Officer are outlined in a document prepared by the Committee and a copy will be provided to each club and appointed Safety Officer.

6. STARTERS.

The maximum number of starters permitted is 85, unless prior agreement for a higher number has been obtained in writing from the Hillclimb/Sprint Committee. The number may have to be reduced, depending on venue location, paddock size etc.

7. FINAL INSTRUCTIONS.

Final instructions, which form part of the Supplementary Regulations, should be sent to all competitors and officials prior to the event and be provided to all late entrants at official sign-on. They should include clear directions to the hill, time of drivers briefing, paddock parking instructions, the timetable including prizegiving details, and other relevant information.

8. PADDOCK.

The paddock should use a large field or large yard in preference to a roadway. The organisation of the paddock requires the appointment of a minimum of 2 marshals exclusively to carry out this task from the time the first competitor arrives at the hill or track.

9. PADDOCK NOTICE BOARD.

A notice board should be set up in a suitable place, typically at the sign-on caravan for example, where final instructions, run results, etc. should be available for viewing during the event.

10. SCRUTINY.

10.1. General.

Scrutineering will take place in the paddock. Optional scrutiny may be made available on the evening before the event in a suitable location.

10.2. Noise.

As per Appendix 2.

10.3. Fire.

In the event of an incident involving fire of any size a competitor must notify and make their car available to the Scrutineers of the event for a full examination.

11. DOUBLE DRIVES.

Double drive cars must display a sticker provided by the organisers. The sticker should be a black "D" on white background, 4" in diameter, to be placed on top of the windscreen, or middle of the



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bodywork in front of the driver for single seaters. The onus is on each competitor to ensure their correct competition number is on the car before going to the start line.

12. START & FINISH AREAS.

12.1. There must be a minimum of 12 metres cordoned off before the start line. The 200 metres after the start line will be designated forbidden ground.

12.2. Tyre warming of any type, whether spinning wheels before the start or using tyre warming blankets etc. is strictly prohibited. The start line and the finish must be kept clear of all vehicles and spectators. A second line must be marked 400mm back from the timing beam position at the start line to ensure consistent competitor line up position. The finish line must be marked by a chequered board on both sides of the course.

12.3. Start Procedures.

Where possible cars should be run in class order and all single seaters should run together without saloon cars mixed among them. There must be a minimum of 20 seconds between the start of all cars with the exception that no saloon car can start behind a single seater until the single seater has finished the course or passed mid-point on a course longer than 2.5km and vice-versa.

13. ROUTE MARKING.

13.1. As early as possible on the day put up direction/diversion signs indicating alternate routes for the public.

13.2. Particular care should be paid to painting white lines around potholes, rocks, and bad bumps.

14. COURSE SAFETY EQUIPMENT.

14.1. Fire Extinguishers

Fire Extinguishers (minimum of 4kg powder) certified by manufacturers or their agents annually to the effect they are in working order. As a minimum one must be at the start and each marshals post on the course.

14.2. Hazard Protection.

Straw bales are preferred, but car tyres can be used provided they are bound in bundles of 4/5 and if used to protect sign post etc. they must be cut and placed around the post and then bound in position. The use of tractor or truck tyres are strictly forbidden.

15. CHICANES.

15.1. Efforts should be made to avoid venues which require chicanes. Consider roads with junctions. If chicanes are required use only marker cones. The roadway must be painted about their bases **allowing 10 to 12 metres between elements of the chicane.** A penalty will

be applied only if the cone is moved outside the painted area of the base. The penalty for moving a cone will be forfeiture of the run (DNF). 30 metres before the chicane will be designated forbidden ground **3-2-1 boards must be erected at equal distance in advance of the chicane.**

All penalties must be notified to timekeepers and competitors at the end of each run.

15.2. White Flag Rules.

White flags should be a minimum of 600mm x 600mm. White flags will be used at chicanes/stop box to indicate that a penalty has been incurred. One white at the exit and a following white at approximately 50M further on, or at the next flagpost.

16. RED/YELLOW FLAG RULES.

16.1. Red flags should be a minimum of 600mm x 600mm. Marshals must raise the flag at the incident point and all flags to the bottom of the hill in turn. Show waved flag to any following traffic up to the incident point. Competitors must stop as soon as possible in a safe place to one side of the roadway or track and await instructions. Await roaming marshal's instruction to lower the flag. Flags should normally be kept lowered when cars are returning to the paddock or start area. If cars slow down or stop during this procedure, waved flags should be shown to alert following competitors.

16.2. Red Flag Locations.

First red flag should be within 20m of the start line, all red flag posts must have line of sight to the next flag post on the course and cover 200m past the finish line. Flag post locations must be passed by the M.I. Safety Officer to ensure marshal safety. A timed red flag check will be carried out by M.I. Safety Officer before racing commences maximum time allowed 20 secs per KM.

16.3. Yellow Flag Rules.

Yellow flags should be a minimum of 600mm x 600mm. Yellow flags shall be waved to slow down competitors after crossing the finish line. These marshals can also be red flag marshals after the finish line.

17. RETURN ROUTES.

Return route must be controlled at all times during events and Judges of Fact are recommended on the route. A 30 kph speed limit is mandatory for this section.

18. REPAIRS.

Have a standby team organised to deal with course emergencies such as oil spills. Ensure that any walls or fences damaged during the event are repaired quickly. Remember local landowners need them for stock security.



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19. MARSHALS.

All marshals and incident teams should be given adequate briefing before an event by knowledgeable individuals. A set marshals briefing will be provided by the Hillclimb Committee to each club hosting an event and must be read out at marshals briefing in the company of the M.I. Safety Officer.

20. ROAMING MARSHAL.

Clear the roads before the start of each run using a spectator control vehicle, from the top down. **This is a mandatory requirement.** A motorcycle is strongly recommended. The use of quads / trikes is forbidden. The roaming marshal should have sufficient experience and authority to deal with minor problems and should carry a radio to summon assistance if required. The use of a baton system is recommended. The roaming marshal should remain at the start line during a run and be ready to despatch immediately in the event of a red flag. Once dispatched the roaming marshal has control of the hill and nothing else should be dispatched until he summons it.

21. DRIVERS BRIEFING.

Hold the driver's briefing at the advertised time. During the briefing, point out any particular hazards on the hill/course. Explain the importance of obeying marshal's signals and stress the meaning of the red/yellow flags. Be sure to explain the running order and turnaround system. **A guideline document will be provided by the Hillclimb Committee to each club hosting an event.**

22. DRIVERS.

22.1. Only one person is allowed in a competition car at any time during the event.

22.2. Each driver must ensure that they present the complete and signed scrutiny sheet for their car when they attend event sign-on.

22.3. If a change of cars is required during an event all relevant paperwork must be completed to reflect the change, the Stewards of the event must also be consulted for their approval.

23. DRIVERS ACCIDENT RECORD.

If it comes to the attention of MI that a competitor is showing a tendency to have accidents, the individual may be requested to attend a meeting with MI to discuss the matter.

24. TIMING STRUTS.

Championship registered competitors will be required to have a vertical timing strut fitted to their car. The strut shall be finished in matt black over its total area (250mm x 50mm). The top of the strut shall not be less than 350mm from the ground and not more than 450mm. No other or further forward part of the vehicle may interrupt these dimensions or activate the timing.

25. AMBULANCE.

25.1. Only one ambulance is required at the venue, although the club may provide more at their discretion.

25.2. Where the course is the maximum distance of 2.5km a second ambulance must be provided. The second ambulance unit must have both competent personnel and a competent member of rescue services on board.

26. PRACTICE AND TIMED RUNS.**26.1. Practice.**

A competitor must count the first completed run as their practice run. ***Practice must be timed to record competitor compliance for the organisers but practice times must not be issued to competitors.***

26.2. Timed Runs.

Clubs should aim for a minimum of three timed runs. Times, with any penalty incurred, should be made available to all competitors at the end of each run.

27. PRIZE GIVING.

Attempt to have a Saturday night prize giving function. The Sunday prize giving should be held in the paddock as soon as practical after the final run.

28. NOVICE AWARD.

Clubs are asked to present such an award for their event.



HILLCLIMBS & SPRINTS

APPENDIX 81

CONNAUGHT COMPETITION ENGINES MOTORSPORT IRELAND HILLCLIMB/SPRINT CHAMPIONSHIP FOR THE FRANK KEANE TROPHY

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1. Connaught Competition Engines Hillclimb/Sprint Championship will run in accordance with Appendix 15 and the following regulations:

2. CHAMPIONSHIP ROUNDS FOR 2012.

The Championship shall be decided on the basis of performance by drivers in the following rounds:

31 March/1 April	Wexford Hillclimb
14/15 April	Galway Hillclimb
2/3 June	Carlow Hillclimb
30 June/1 July	Kirkistown Sprint
18/19 August	MEC Sprint Weekend
1/2 September	Donegal Hillclimb

Best **8** results to count.

3. CHAMPIONSHIP CLASSES.

3.1. Definitions.

Production Saloon: Bodyshell as per originally manufactured. GT & Sportscar: Production and non production GT and Kit Cars. Turbo /supercharged: CC x 1.7 applies to classes 1, 2, 3A, 3B, 6, 8, 9.

3.2. Classes.

- (1) Production saloons up to 1400cc including Mini's with removable front section.
- (2) Production saloons **from 1401cc up to 1650cc.**
- (3a) Production saloons 1651cc to 2050cc, GT/Sportscars up to 1650cc.
- (3b) Production saloons and rally cars over 2050cc, GT/Sportscars over 1650cc and 4WD Rally Cars.
- (4) Classic Cars per HRCA regulations. (See Appendix 49) & FIA Appendix "K".
- (5) Formula Ford 1600 as per Ford Motorsport regulations & Formula Vee as MI appendix 45 regulations.
- (6) 2050 Formula Libre. Single Seaters up to 2050cc having only one camshaft and Single Seaters up to 1650cc with two or more camshafts.
- (7) Libre Unlimited and all other 4-wheel drive cars.

(8) Rally cars up to 1650cc.

(9) Rally cars 1651cc to 2050cc.

Subject to sufficient entries these classes must apply to all Championship events. The organising clubs may include extra classes or amalgamate classes, subject to the approval of the Hillclimb /Sprint Committee.

Rally cars must run "Rally Ready" - to include tyres, seats etc.

4. LICENSING.

Minimum of National B Speed Licence is required with the exception of competitors competing in classes 3, 6, 7 and 9 which are required to have a minimum of a National A Speed licence.

5. ELIGIBILITY.

MI appointed Scrutineers in co-operation with the Hillclimb/Sprint Committee shall be the judges of the eligibility of a car for the class for which it is declared. A registered competitor can only score championship points in the class for which they are registered, however they may substitute their regular car for another provided it does not exceed the criteria of the class in which they are registered.

6. REGISTRATION.

In order to score points in the championship, competitors must complete the registration form and return it to MI together with the registration fee of €40. Registration will close prior to the commencement of practice for Round 9. Points will only be awarded to registered competitors from their date of registration. Competitors are required to display the championship decal during competition.

7. EVENTS.

The events nominated by MI for inclusion in the championship shall be open events. In order to be eligible for championship status clubs must have run a hillclimb or sprint event in the previous year. Events must cater for all championship classes and priority must be given to registered Championship competitors, provided entries are received by the closing date and are in order. At events where organisers amalgamate or subdivide classes to suit the number of entries, results must also be published for each of the Championship classes as published.



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8. COMPETITION NUMBERS.

Competitors will be allocated their competition numbers upon registration. Individual requests for specific numbers will be allocated where possible. The number 1 will be reserved for the previous years overall champion. The number previously allocated to the "Champion" will remain live during this period. The number 14 will not be issued or used for Championship competition. Clubs must allocate competitors with their Championship competition numbers, and provide a space on the Entry Form accordingly.

9. POINTS.

Drivers will be allocated points based on the overall scratch result, the overall class results and the overall handicap results. For all rounds, the marking system is: Overall: 10, 9, 8, 7, 6 etc. Classes: 10, 9, 8, 7, 6 etc.

10. COUNTING ROUNDS.

If more than 10 rounds are run, the number of counting rounds shall be 10, if 10 or less rounds are run all rounds shall count.

11. TIES.

Competitors with equal best runs in an event will be awarded the points for their finishing position. Ties in the championship will be resolved by comparing the quality of places (i.e. number of 1st's, 2nd's, 3rd's etc.) obtained by the competitor in the events contributing to their end result.

12. AWARDS.

Overall Championship/ The Frank Keane Trophy
The overall championship is on a scratch basis. At the end of the season competitors results will be confirmed by the Hillclimb/Sprint Committee and the following awards will be given.

1st Award
2nd Award
3rd Award

13. CLASS CHAMPIONSHIP.

All competitors will score points in accordance with Para. 9. At the end of the season all point scoring competitors class results will be confirmed by the Hillclimb/Sprint Committee, with each competitor using only points scored in one class. The following awards will be presented.

3 or less competitors - 1st place award only.
4-5 competitors - 1st and 2nd place awards only
If 6 or more competitors - 1st, 2nd and 3rd place awards.

In order to receive awards in the Championship drivers must compete in at least 5 rounds.

14. HANDICAP AWARD FOR THE TOM PRENDERVILLE MEMORIAL TROPHY.

The competitor who scores the most points on handicap, and who does not receive an overall or class award, will be the Handicap winner.

15. CLUB AWARD FOR THE DICK BAILEY MEMORIAL TROPHY.

The club that runs the best event in the Championship calendar as indicated by registered competitors, Stewards reports and judged by the Chairman of the H/S Committee in consultation with the Committee members. The club deemed the winner has automatic national championship status for the following year if requested.

16. CLUBS.

Organising clubs must conform to the advertising and publicity requirements of the Championship sponsor as agreed by the Hillclimb/Sprint Committee. Clubs must display all advertising as supplied. Failure to comply may result in loss of championship status.

17. RESULTS.

A Registrar will be appointed annually to receive results, record and prepare an up-to-date table of placings in both the overall, class and handicap competitions.

18. RETURNS.

Clubs organising championship events must make an official return to:

The Registrar,
MI Hillclimb/Sprint Championship,
Motorsport Ireland,
34 Dawson Street, Dublin 2.
(FAX 01 - 6710793)

They must arrive within 3 days of the completion of the event. The returns must give the following information:

18.1. Name of driver.**18.2. Class****18.3. Position in order of finishing of all drivers in overall and class order.****19. PENALTIES.**

Clubs which fail to make returns in accordance with Para 20 can be penalised at the rate of €100 per week, or part thereof, by which the results are delayed beyond the prescribed date. Such clubs may also be refused further permits for open competitions for a period to be specified by MI, who can also remove or withhold Championship status for any period.



RALLYCROSS

APPENDIX 82

2011/2012 MOTORSPORT IRELAND (MI) NATIONAL RALLYCROSS CHAMPIONSHIP REGULATIONS

1. STANDING REGULATIONS.

All events will be held under the International Sporting code of the F.I.A. The General Competition Rules of MI, these Standing Regulations and the supplementary regulations, which will be issued prior to the events.

2. CHAMPIONSHIP REGISTRAR.

Tom Kirwan

3. DATES.

Round 1 - Mondello 16 October 2011
Round 2 - Mondello 31 October 2011
Round 3 - Mondello 20 November 2011
Round 4 - Mondello 4 February 2012
Round 5 - Mondello 19 February 2012
Round 6 - Mondello 4 March 2012

Note: 2012 dates to be confirmed.

Counting Rounds

A competitor may only count scores from **5** out of **6** rounds. If a competitor is excluded from a round(s) they may not use that event as a dropped score.

4. CHAMPIONSHIP REGISTRATION.

Automatic with event entry.

5. ELIGIBLE COMPETITORS. (PREVIOUSLY NUBER 8)

All drivers must be in possession of a current medically approved competition licence (MI only), or a valid competition licence and medical certificate (MSA only).

6. ENTRY FEE. (PREVIOUSLY NUBER 8)

€195.00 per event entered. Entries close 10 days prior to event, after which a late entry fee applies. No entries will be accepted within the 3 days prior to the event or on the day of the event. Late entry fee plus €40.00.

The above fee includes Personal Accident Insurance. The Organisers reserve the right to alter these fees in the event of increased costs.

7. ALL ENQUIRIES / ENTRIES.

Rallycross Entries,
Mondello Park Ltd., Donore, Naas, Co. Kildare.
Tel: 00353 (0)45 860200 Fax: 00353(0)45 860195

8. ADVERTISING. (PREVIOUSLY NUBER 5)

Competitors may be required to carry advertising as supplied to publicise the championship in

accordance with GCR 117, and in order to qualify for points or awards.

9. CHAMPIONSHIP DIVISIONS. (PREVIOUSLY NUBER 6)

SUPERCARS

Class 1 Supercar- 2 & 4 WD drive vehicles.

Note: The manufacturer of the space frame should submit a technical specification including the materials used and it must be approved by the Technical Advisory Group (TAG).

MODIFIED

Class 3a - Vehicles over 2150cc.

Class 3b - Vehicles over 1650cc up to a maximum of 2150cc, including FIA Division 2 vehicles (Group A and N)

Class 4 - Vehicles up to a maximum of 1650cc including FIA Division 1A

Note: The manufacturer of the space frame should submit a technical specification including the materials used and it must be approved by the TAG.

STOCKHATCH

Class A & B as per MI Yearbook and/or MI technical specification sheet.

JUNIORS

As per MI yearbook Appendix 84 and/or MI technical specification sheet.

RALLYCARS

Class 9 - Rallycars up to 1650cc

Class 9a - Rally cars over 1650cc up to 2050cc

Class 9b - Rally cars over 2050cc, 4WD's and all Turbos to be restricted.

RX 150 as per MI technical specification sheet (see MI website).

10. TRANSPONDERS.

Transponders will be in use at each meeting. Each driver will be required to have their own transponder for fitting to their vehicle. It is the driver's responsibility to ensure that their transponder is operating correctly. Any problems should be brought to attention of organisers immediately.

Note: Double drivers both require separate transponders.

11. NUMBERS.

Must be displayed on both sides of the car, and on the bonnet/roof at a 45 degree angle, to the

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right hand side of the car. The numbers must be at least 200mm high with a 40mm stroke wide – black on a white background. To aid the assembly and grid officials a number must be displayed on the top of the windscreen on the passenger side, 100mm high and of visible colour.

The top 10 finishers in the Overall Championship and Stock Hatch Cup will be awarded a race number for the following season based on finishing position.

12. DOUBLE DRIVING.

Will be permitted.

Finals – If both the double drivers qualify for the same final, the fastest will be deemed to qualify unless declared otherwise by the end of heats.

13. SCRUTINY & SIGN ON.

Will be held at the circuit before practice and racing. Scrutiny will be held by class in the following order: Supercar, Modified, Stock Hatch, Junior and Rallycar. Sign-on closing time is 15 minutes before the start of drivers briefing.

14. DRIVER BRIEFING.

Will be held for all competitors before practice. Non attendance will result in a fine of €50. A roll call may be taken.

15. PRACTICE.

Will be held prior to the start of the heats. Competitors must attempt a practice session. Competitors must practice in the car to be raced. Juniors will practice together as a class with a maximum of 8 cars per session. Due to time constraints at events there may be only one practice attempt per competitor unless a second attempt is allowed at the discretion of the CoC. **Where possible the organisers will attempt to run Classes together in practice.**

16. METHOD OF STARTING.

16.1. Heats.

Up to 8 cars will start in each run in a 3-2-3 grid formation. The exception are Supercars who may start 4 abreast and Juniors who will start 6 to a grid in a 2-2-2 formation. Rallycars will start separately at 5 second intervals.

Each competitor will have at least 2 runs of 3 or 4 laps as notified in event procedure bulletins issued on the day.

16.2 Finals.

Up to 8 cars will start each final in a 3-2-3 grid formation.

All retirements must be reported to the organisers.

17. START PROCEDURE AND FALSE START.

17.1. Starting will be by lights or flag. Competitors are under starters orders when the

red light comes on. It will stay on from 1 – 5 seconds and the race starts when the red light goes out. When a false start occurs, the starting lights will flash amber. The driver(s) concerned will be warned and placed on an additional row behind the last row of the grid. The starting procedure will begin again. If a driver makes a second false start in a run then they will be excluded from that run. In a final they will be excluded and placed last in the result for that race. In the case of a breakdown the judge's decision will be final.

17.2. In the case of a failure of the start lights, there will be a flag start, with cars starting on the drop of the flag. A judge of fact, who will be named in the final instructions, will adjudicate on all starts. If a jump start is detected the race will be stopped by a red flag.

17.3. Only event officials and drivers will be permitted in the start area, unless otherwise instructed by the Clerk of the Course.

17.4. The Clerk of Course may order a restart if he considers there to have been a faulty start.

18. RACE STOPS.

18.1. Should a race need to be stopped a red flag will be shown at the start/finish line and at all flag posts around the circuit, drivers should cease circulating at racing speed and return to grid area unless otherwise instructed by CoC or officials, and this area will be treated as parc ferme. Cars will not return to paddock unless instructed Any driver doing so will be deemed to have retired and can take no further place in the subsequent re-run of that race/final.

18.2. Any car which is stationary when the race is stopped will be deemed to have retired from the race and may only be allowed to restart at the discretion of the CoC.

18.3. Any car which did not start the original race will not be allowed to start in the re-run.

18.4. Re-runs will be permitted only when a red flag has been shown. All other incidents will be treated as force majeure.

18.5. A competitor/s who causes red flag may only be allowed to restart at the discretion of the CoC.

19. FLAG SIGNALS.

Will be as specified in the International Sporting Code, with the exception that a Green Flag and a Blue Flag will not be used.

20. DRIVER CONDUCT.

20.1. The Clerk of the Course will have the power to exclude any driver from a qualifying run or final, if the said driver is deemed to have caused



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unnecessary contact. The Clerk of the Course's decision is final and will be posted or announced as soon as possible.

20.2. The Clerk of the Course or Stewards of the Meeting reserve the right to impose penalties in accordance with GCR 139 and 142.

20.3. Video or similar equipment, which is under the control of the organisers can be used to facilitate the decisions of the Clerk of the Course's or Steward's.

20.4. If it appears from any evidence that there may have been actions by a competitor or a member of his team considered to have brought the championship into disrepute, the Championship Registrar will be entitled to report the matter to Motorsport Ireland.

20.5. The Clerk of the Course and/or Stewards reserves the right to delay any investigation until he has the opportunity to examine all the evidence available to him.

21. PENALTIES.

21.1. Any competitor who leaves the circuit such that all four wheels are off the course may incur a time penalty or may be considered as having retired from the heat or final at the point at which they left the course whether or not an advantage is gained. Competitors may only return to the course when it is safe to do so and in a safe manner, acknowledging other competitors.

21.2. A minimum number of penalty markers may be employed to define a course and a penalty of 5 seconds may be imposed for displacing such a marker. A driver passing on the wrong side of a marker may incur a no-run.

21.3. Any breach of driving standards will be dealt with firmly. Video evidence from the official CCTV system or nominated TV Production Company, or footage from any other proven source, may be utilized as evidence.

21.4. Any driver who is excluded from an event will lose all championship points for that event and must count that event as one of their championship scores.

22. DAMAGED VEHICLES.

Any car damaged in an incident on circuit, must be returned to the scrutiny bay for examination by the Scrutineers. Only when the Scrutineers release the car, can it continue in the event. Any competitor involved in an accident, who fails to follow this procedure, will be excluded and further action may be taken.

23. EVENT RUNNING ORDER.

Qualifying heats normally be run in the following order: Supercar, Modified, Stock hatch, Junior,

and Rally car. Competitors in each category where possible will qualify within their own category.

Finals will be in the following order:

D Finals - Supercar, Modified, Stock Hatch, Junior, **RX 150's** and Rally car
C Finals - Supercar, Modified, Stock Hatch, Junior, **RX 150's** and Rally car
B Finals - Supercar, Modified, Stock Hatch, Junior, **RX 150's** and Rally car
A Finals - Supercar, Modified, Stock Hatch, Junior, **RX 150's** and Rally car
Superfinal

Where there are insufficient competitors to run a final it will be deleted from the running order.

24. QUALIFYING HEATS.

24.1. The onus is on the competitor to arrive at the paddock assembly area in time for his run, any driver who missed his run may not be allocated a place on a later grid. Full details of grids will be posted in the paddock. However, it is the competitor's responsibility to ascertain what heat they are running in and what final they have qualified for.

All the Heats will be timed.

To qualify for a final, a competitor must complete two out of three qualifying heats (or one out of two).

24.2. Entry into the Finals will be determined by either;

1. Adding together the lowest two points scores where three Heats are run.
2. The single lowest score where two Heats are run.
3. If only one Heat is run the points for that Heat.

24.3. TIES: Where competitors have equal scores at the end of the Qualifying Heats the tie(s) will be resolved as follows:

1. The best (lowest) single points score
2. The next lowest score
3. The next lowest points score (the discarded heat).
4. The fastest single time.

25. FINALS.

25.1. To be a minimum of 1 lap more than heats.

25.2. Superfinal to be a minimum of 1 lap more than the finals.

25.3. There will be a minimum of an A Final run for each class and possibly more.

8 cars will qualify in the D final
7 cars will qualify in the C final
7 cars will qualify in the B final
7 cars will qualify in the A final



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The first car from each final will qualify for the next final and be placed at the back of the grid to make up 8 cars in finals C-B-A.

The fastest 16 rally cars will qualify for 2 x 8 car finals. Class awards will be given based on driver's time in the final.

25.4. Holding Area.

All competitors qualified for the Superfinal must take their car to the holding area. Cars must enter and leave the holding area under their own power and no work may be carried out on the cars in the holding area. Any competitor who is not present with their car when the holding area closes, will be judged to be a non-starter and the next eligible competitor will take their place.

The holding area will close at the start of the last A Final to be run.

25.5. Superfinal.

25.5.1. There will be a Super Final for the 8 competitors, plus 8 reserves, in order of fastest times from the A finals. The finalists and reserves will be drawn from Supercar, modified and Stock hatch Categories.

25.5.2. Results from the final will determine points towards the MI Rallycross Championship points table. Competitors who non-finish in their A Final, but who are able to race in the Superfinal are eligible to do so. The distance covered in laps will determine the order in which non-finishers qualify for the Superfinal.

Where two or more non-finishers are tied on results, as recorded by the timekeepers, their qualifying position for the A Final will be considered to resolve the tie. Where this fails to resolve the tie, fastest times in the qualifying Heats will be considered qualifying position for the A final and then fastest times will be considered to resolve a tie where there are two or more A Final non-starters who are eligible for the Superfinal.

25.5.3. The top qualifier in the Superfinal may choose any of the three starting positions on the front row of the grid. The second qualifier the highest remaining position and the third qualifier the last remaining front row position.

25.5.4. The number of laps they complete and the times taken to complete these laps, as recorded by the official timekeepers, will classify competitors who retire from the Superfinal. Parts of uncompleted laps will be ignored.

25.5.5. Competitors who do not take the start of a final will be classified after non-finishers, but before reserves that do not take the start. Where there are two or more non-starters, they will be classified according to the order in which they qualified for the final in question.

25.5.6. There may be a parc ferme at the end of the Superfinal. All cars that presented themselves in the holding area and any cars that retire from the Superfinal must return to the parc ferme. The parc ferme shall remain in place for 30 minutes after the end of the final. Any car that leaves before this time, unless as instructed by an authorised official, shall not be eligible for Championship points. All competitors whose cars are in parc ferme area must remain at the venue until their competition vehicle is released from the parc ferme area.

26. CHAMPIONSHIP POINTS - EACH CLASS - QUALIFYING ROUNDS.

1st. - 20 points	7th. - 11 Points
2nd. - 17 Points	8th. - 10 Points
3rd. - 15 Points	9th. - 9 Points
4th. - 14 Points	10th. - 8 Points
5th. - 13 Points	11th. - 7 Points
6th. - 12 Points	12th. - 6 Points

Class Championship points will be based on the lowest points score from the heats.

Stock Hatch Cup **and Shield** and Modified Championship points based on Finals.

27. OVERALL CHAMPIONSHIP POINTS EACH DIVISION - QUALIFYING ROUNDS.

27.1. Points Table.

1st. 20 points	9th. 9 Points
2nd. 17 Points	10th. 8 Points
3rd. 15 Points	11th. 7 Points
4th. 14 Points	12th. 6 Points
5th. 13 Points	13th. 5 Points
6th. 12 Points	14th. 4 Points
7th. 11 Points	15th. 3 Points
8th. 10 Points	16th. 2 Points

Points for the **2011/2012** MI Winter Championship will be awarded for the Superfinal at each Championship event.

27.2. Ties.

In the event of a tie in the championship, the driver with the greatest number of first places will be deemed to be the winner. If this should fail to produce a result the greatest number of second places will be counted and so on until a definitive result is achieved.

27.3. In the event of a Rallycross being abandoned or curtailed, allocation of points for that round will be at the discretion of the organisers e.g the starting grid for the Superfinal.

27.4. Protests against rule interpretation will be decided by M.I. Championship Stewards. See GCR's Chapter 14.



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28. AWARDS.**28.1. Event Awards.**

The following are the awards to be presented at an event, 1st, 2nd & 3rd in A finals and Superfinal and 1st in B and C and Rally car finals. Class awards will not be awarded at events except for Rally cars.

28.2. Overall Championship - Overall and Classes.

Overall - 1st, 2nd & 3rd Trophies.

Modified Championships - 1st, 2nd & 3rd Trophies.

Class Awards - 1st, 2nd & 3rd Trophies.

Stock Hatch Cup 16 valve - 1st, 2nd & 3rd Trophies.

Stock Hatch Shield 8 valve - 1st, 2nd & 3rd Trophies.

Special Awards

C.K.M.C. Member award will be awarded to the highest placed C.K.M.C. club member not receiving any other award.

C.C.C. Member award will be awarded to the highest placed C.C.C. member not receiving any other award.

Ladies award will be awarded to the highest placed lady driver.

If there is less than 3 starters in any division 1 award. 4-6 starters = 2 awards and over 6 starters = 3 awards.

29. CARS & SAFETY MEASURES.

29.1. Competitors must present their vehicle in a clean condition, with competition number in place, any relevant paperwork for scrutineering at the nominated time to take part in the event.

Cars must be of smart appearance with tidy bodywork, the Clerk of the Course or Chief Scrutineer may take any action deemed necessary to ensure the sports image is upheld.

Axle stands must be placed under the vehicle when more than one wheel is in the air. The axle stand must support the portion of vehicle in the air when work is being performed under the vehicle. Failure to do so may result in a fine.

29.2. Logbooks.

Logbooks are compulsory. Cars presented to scrutineers without a current logbook will not be permitted to start.

29.3. Rallycross cars.

Rigidly closed, non convertible saloon, sport and GT cars with full bodywork. All forms of lightening and tuning permitted.

29.4. Group A & N cars.

Must conform to FIA appendix J of the International Sporting Code. Homologation Papers will be checked at scrutiny. Previously homologated cars are eligible.

29.5. Stock Hatch.

Must conform to technical regulations as per MI yearbook and/or MI technical specification sheet.

29.6. Inspection.

All competitors are advised that they may be required at end of the meeting to remove the cylinder head on their car for inspection. All costs of subsequent rebuilding are the responsibility of the competitor concerned. (GCR 163). Engines may be sealed for further inspection at the discretion of the chief Scrutineer and CoC. Please note Appendix 2 Articles 28.5 and 28.6. Of the MI Yearbook.

29.7. Weight.

The minimum weight for all vehicles ready to race including with full apparel is:

Class 1 - 1000 kg

Classes 3A, 3 B - 800 kg

Class 4 - 700 kg

These weights will be reviewed throughout the season to ensure parity between the classes.

30. WHEELS & TYRES.

The complete wheel (Flange Rim & Tyre) must always fit inside a u-shaped gauge of which the extremities are 250mm apart. The measurement is to be made at the unloaded part of the tyre BUT WITH THE CAR READY TO RACE AND THE DRIVER ON BOARD. The use of tyres fitted with studs, spikes or chains are not permitted.

Tyre rollers and Tyre Warmers are prohibited.

Grooved tyres are authorised on the basis of a design homologated by the FIA. At any time during the race the depth of the grooves must be at least 2mm regardless of the type of tyres used and must cover minimum 75% of the surface.

In class 9, 9a and 9b Rallycross compound tyres are forbidden.

31. ALL CARS MUST BE FITTED WITH.

31.1. Full roll cage as per FIA appendix J and MI Yearbook Appendix 2.



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31.2. Laminated or rigid plastic windscreen (polycarbonate only) Synthetic screens must not be tinted. Tinted glass screens are only permitted if they are original for this car.

31.3. Towing eyes fitted front and rear which are strong enough to ensure that a recovery vehicle may tow the car. The eyes must not protrude beyond the cars perimeter. The towing eyes must be painted bright red, yellow or orange and must be fitted and marked so as to be easily found by rescue personnel.

31.4. Two red rear brake lights of the anti-crash type working with or replacing the cars original brake lights, situated in a visible position Rain light mounted in a visible and safe position at rear of vehicle.

31.5. Original type driver's door must be retained.

31.6. Dual circuit brakes.

31.7. A handbrake is obligatory, it must be efficient and simultaneously control the two front wheels or the two rear wheels. Brake fluid reservoirs are forbidden inside the cockpit.

31.8. Only competition seats are permitted. These must be fitted with approved mounting brackets, the seat must be firmly attached to the floor / chassis. Head restraints are compulsory and must be built into the seat. (FIA approved seats are Compulsory, see MI Yearbook Appendix 2).

31.9. An appropriate FIA approved harness must be fitted with a minimum of 4 fixing points.

31.10. A general cut off switch must be fitted on the left hand side of the front windscreen, it must have a clearly marked on / off position.

31.11. All headlamp glass must be covered to give adequate protection.

31.12. Mud flaps on all wheels.

31.13. Fire extinguishers.
See MI Yearbook Appendix 2. One handheld 1.75 litre AFFF or FIA approved equivalent. Installed automatic systems recommended.

31.14. Bodywork.
All cars must be rigidly closed non convertible models, and the bodywork MUST be in presentable condition.

31.15. Anti theft devices must be removed from the steering column.

31.16. All cars fitted with electrical petrol pumps must have relay fitted.

31.17. Engine.

If supercharging is used, the exhaust gases from the waste-gate must exit into the vehicles exhaust system. Turbocharged cars must not be equipped with any device, which allows the boost pressure, or the electronic management system controlling the boost pressure, to be adjusted by the driver while the car is in motion. Ceramic components, variable diameter inlets and adjustable internal vanes on turbochargers are forbidden. Water injection is prohibited, even if it originally exists on homologated block. Spraying of the intercooler is prohibited. The use of pop off valves is PROHIBITED.

31.18. All cars must be fitted with a rear view mirror inside the car and 2 rear view mirrors on the exterior of the vehicle.

31.19. Window on the drivers side must remain closed or have netting fitted.

31.20. Noise.

For all cars a limit of 105 db is imposed, measured with a noise pressure meter regulated at position A and SLOW, placed at an angle of 45 degrees to and 50 cm distance from the exhaust outlet, with the car engine turning at 4,500 revolutions per minute.

32. DRIVER CLOTHING.

32.1. Crash helmet of the FIA approved type (see MI Yearbook Appendix 2).

32.2. If the car has an aperture cut in the windscreen, or is fitted with a laminated screen. Wear goggles or a visor.

32.3. Wear one-piece flame resistant overalls with tight fittings around wrists and ankles. (FIA APPROVED OVERALLS ARE MANDATORY) as per MI Yearbook Appendix 2.

32.4. Boots and underwear as per MI Yearbook Appendix 2.

33. EVENT OFFICIALS.

33.1. Each Rallycross event will be overseen by the officials listed below, assigned by the organising clubs. Each of these officials is a Judge of Fact.

- Clerk of the Course (CoC) and his Deputy and Assistant(s) are responsible for the fair and sporting conduct of the event, entrants and vehicles.
- Grid staff and their assistants
- Start and Finish line judges and their assistants.
- All other officials as listed in the Supplementary Regulations or bulletins.



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34. FORCE MAJEURE.

The organisers will endeavour, as far as is reasonable, to ensure that every competitor shall encounter equal course conditions and equal likelihood of hazard. However incidents often occur which are beyond the organisers control . When a competitor encounters such an incident or hazard and is competitively disadvantaged , it is unfortunate, but part of the nature of Rallycross events. This type of situation is deemed "Force Majeure".

Claims arising from conditions of Force Majeure are not allowed and are not subject to official action.

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STOCKHATCH RALLYCROSS CLASS

2011/2012 TECHNICAL REGULATIONS

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1. GENERAL REMARKS.

It is the driver's responsibility to ensure that the car driven complies with these regulations. All modifications are forbidden unless specifically authorised by these regulations. The term "standard" refers to the components as fitted to the vehicle by the original manufacturer when the model was in production.

If you have any queries about these regulations please contact the series coordinator or series eligibility scrutineer.

NB The competitor must be able to supply technical data to support any parts used on the vehicle.

2. ELIGIBLE VEHICLES.

The Stock Hatch Car Class is for hatch-back cars that are, or have been available on general sale to the public in the EU and are at least 3 years old. Homologation specials are not permitted. The cars will run in two classes to include:

2.1. Class A.

2.1.1. Cars with engine capacities up to 1600cc having no more than 2 valves per cylinder.

2.1.2. Cars with engine capacities up to 1400cc, which may have more than two valves per cylinder.

2.2. Class B.

2.2.1. Cars with engine capacities up to 1600cc with more than two valves per cylinder and fixed valve timing.

2.2.2. Cars with engine capacities up to 1400cc with variable valve timing.

Both classes will run together in heats and finals.

These capacity limits are excluding any increase from the permitted maximum rebore of +0.5mm over standard bore size.

2.3. N.B. It is the intention of the Organisers, If necessary, to equalise the performance of cars. This may be done by various means including.

2.3.1. A minimum weight limit.

2.3.2. Addition of air intake restrictors.

2.3.3. Addition of engine rev. limiters.

2.3.4. Grid Positioning.

These changes may be imposed at any time during the season.

There is no appeal against the implementation of any of the performance equalisation control measures.

3. SAFETY REQUIREMENTS.

3.1. Roll cage.

A roll cage must be fitted which complies with Appendix 2 of MI yearbook. In addition side intrusion (door bars) must be fitted to both sides of the cage. The entire cage must be constructed from a material complying with Appendix 2 16 of MI yearbook, the number of fixing points is free and the method of fixing is free.

The addition of additional strengthening braces and their material is free, provided that the front bulkhead is not penetrated and that no part of the cage extends rearwards beyond the rear shock absorber mounting points.

3.2. Fire extinguisher.

A fire extinguisher MUST be fitted; the minimum specification must be to current MI regulations. If a plumbed-in system is fitted this MUST be capable of being operated by the driver when strapped into the driver's seat. In addition the system MUST be able to be operated from an external triggering point located at the base of the windscreen on the driver's side of the car. The external triggering point can be connected to the plumbed-in extinguisher system by means of a pull cable.

3.3. Safety harness.

An FIA approved safety harness must be fitted to the car, as per Appendix 2 of MI yearbook

3.4. Driver's seat.

The driver's seat MUST be securely fitted to the car. An FIA approved seat as per Appendix 2 must be used.

3.5. Battery cut-off switch.

A battery cut-off switch MUST be fitted to all vehicles that can be operated by the driver whilst strapped in the driver's seat. In addition the cut-off switch MUST be able to be operated from an external triggering point located at the base of the windscreen on the driver's side of the car. The external triggering point can be connected to the cut-off switch by means of a pull cable. The battery cut-off switch MUST isolate ALL electrical circuits (with the exception of those that operate fire extinguishers) and MUST interrupt the electrical supply to the ignition system.



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3.6. Windscreen.

All cars **MUST** be fitted with windscreen of laminated glass or alternatively of 5mm thick Lexan.

4. CHASSIS/BODY.

4.1. Body shell.

The standard body shell **MAY NOT** be strengthened except for the following:

4.1.1. The fitting of the roll cage as per 2A.

4.1.2. The reinforcing of the standard suspension mounting points may **NOT** extend to more than a radius of 100mm from the centre of the suspension point.

4.1.3. To prolong the life of the bodyshell it is permitted to carry out seam welding to the engine bay only.

4.1.4. The exterior of the car **MUST** remain as standard including mirrors, door handles and bumpers, although it is permitted to remove body mouldings, badges and the radio aerial. It is permitted to fit guards to engine sump and fuel tank. It is **NOT** permitted to reinforce the bumpers or their mountings.

4.1.5. Glass sunroofs **MUST** be replaced as per Appendix 2 of MI yearbook.

4.1.6. IN THE INTERESTS OF SAFETY IT IS STRONGLY RECOMMENDED THAT - The side and rear window glass may be replaced with Lexan or polycarbonate having a minimum thickness of 4mm. **If retained see Appendix 2 Article 19.2. Anti-shatter film.** Consideration **MUST** be given to driver's compartment ventilation and for this purpose only, it is permitted to cut a maximum of 4 X 2-inch diameter holes in each of the rear side and tailgate windows.

4.1.7. The interior of the car **MUST** have the roof lining; carpets and all the seats, apart from the driver's, removed. It is also required that any audio equipment, spare wheel, tools and standard seat belts be removed. It is permitted to remove the door trim panels and the window regulators (the windows must then be fixed to prevent them opening). It is recommended that aluminium or Kevlar panels replace the trim panels.

An interior mirror of a minimum of 50 sq. cm. **MUST** be fitted. The original dashboard **MUST** be retained although addition instruments may be fitted.

5. SUSPENSION.

5.1. It is permitted to fit non-standard shock absorbers and springs provided that they mount onto the body at the standard suspension mounting points. Any suspension units may only have a single method of damping adjustment, multi-adjustable and/or remote reservoir types are **PROHIBITED**. The ride height may be altered. Platforms may not be capable of adjustment.

5.2. It is permitted to fit uprated bushes to the suspension (**NO** rose type joints permitted).

5.3. It is permitted to fit, remove or uprate anti-roll bars (**NO** rose type joints permitted).

5.4. It is **NOT** permitted to have adjustable suspension geometry. I.E. Camber, Caster unless fitted as standard to the original car, the car **MUST** retain the type of suspension fitted as original equipment.

5.5. Camber must at all times be no more than 1 degree more or less than manufacturer's figure. Competitors must be able to provide manufacturer's data to prove original camber of their car.

5.6. Strut braces are permitted and their design and material are free, but they must not connect to the front bulkhead at any point.

ONLY THE MODIFICATIONS LISTED BELOW ARE PERMITTED.

6. ENGINE.

6.1. The engine and gearbox **MUST** be as originally fitted as original equipment to the car; they **MUST** retain their original configuration and location.

6.2. The engine may be rebored a maximum of + 0.5mm on the original size, but only standard pattern, unmodified pistons can be used. Competition orientated pistons are prohibited. Stroke must comply with Manufacturer's original specification.

6.3. Lightening of crankshaft or con rods is prohibited. Balancing may be carried out by pin drilling only.

6.4. Camshaft must be standard as originally supplied for the type of engine used. It may not be machined or reprofiled. Original timing gear must be used; offset keys or adjustable vernier drives may not be used, unless fitted to the original engine.

6.5. Cylinder head must be standard as originally fitted to the type of engine used. No gas flowing or polishing is allowed. Rough casting marks must be seen in inlet & exhaust ports and in

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combustion chambers. Heads may be skimmed for maintenance purposes only and may not exceed the manufacture's minimum dimension.

6.6. Competition Flywheels are not permitted. The standard unit may not be lightened or machined.

7. RADIATOR.

Water radiator and hoses are free but the original radiator location **MUST** be retained. It is permitted to remove the heater. The associated pipework can be blanked off or linked together in the engine compartment. Cold start systems may be disconnected or removed.

8. OIL COOLER.

It is permitted to fit an oil cooler within the engine compartment.

9. SUMP.

It is permitted to baffle the standard sump and modify the oil pick up pipe. Dry sumping is not allowed.

10. AIR INTAKE.

It is permitted to remove/replace the original air filter and filter housing with a performance filter or induction kit. Where engine breather pipes cannot be refitted to the induction system, breather pipes must go to a catch tank of adequate size.

It is permitted to change carburetor jets or injectors.

It is **NOT** permitted to modify the inlet manifold system including the throttle mechanism, carburetor, fuel pressure regulators, and throttle body apart from fitting an additional external throttle spring.

11. IGNITION SYSTEM.

The standard type of ignition/engine management system **MUST** be retained but it is permitted to fit a modified "chip".

Rev. limiters may be fitted or removed. Spark plugs and ignition leads are free.

It is **NOT** permitted to add additional ignition systems.

3D management systems are **NOT** permitted.

ANY form of traction control is **NOT** permitted.

12. EXHAUST SYSTEM.

The standard exhaust manifold **MUST** be retained. The exhaust system beyond the existing manifold is free but it **MUST** exit from the rear of the car and meet the current permitted noise levels for Rallycross.

13. FUEL SYSTEM.

The fuel pump, filter and fuel lines are free. Consideration should be given to the route taken by and the materials used for fuel lines to minimise any likely damage and reduce the danger of fire.

14. TRANSMISSION.

The standard gearbox casing and all internals **MUST** be retained except for Torque bias and limited slip differentials (LSD), which are **NOT** permitted even if fitted as original equipment. Torque bias and LSD's **MUST** be replaced by a normal differential with the correct ratio. ANY form of traction control is **NOT** permitted.

Gear ratios & final drive ratios must be the same as originally supplied for that model of car.

Standard drive shafts **MUST** be retained. The clutch drive plate and cover may be up-rated by a standard pattern type.

The following homologated gearboxes may be used as replacements for the Citroen Saxo VTR: Citroen Saxo VTS (homologation no. A-5564), Peugeot 106 Rallye (homologation no. N-5561, 1.6 Rallye gearbox only, not 1.3 Rallye gearbox), Peugeot 106 16v (homologation no. N-5565), Peugeot 106 XSi (homologation no. A-5453).

15. ELECTRICAL SYSTEM.

15.1. Battery.

The type of battery and location is free but it **MUST** be secured. Batteries located in cockpit must be in battery box.

The battery **MUST** be capable of repeatedly starting the engine.

15.2. Lights.

It is recommended that all lights are removed and similar shaped covers are fitted to the resulting holes.

Two high-level brake lights **MUST** be fitted to the inside of the rear window in accordance with the Rallycross section of the current yearbook.

A working rain light must be fitted to the rear of the vehicle.

15.3. Charging and lighting.

Both charging and starting circuits **MUST** be as standard and function efficiently.

16. STEERING LOCK.

The steering lock **MUST** be removed.

17. BRAKES.

17.1. The standard type and size of brakes **MUST** be retained although it is permitted to fit non-standard pattern friction material. Standard pattern after market ferrous discs and drums may be used. It is **NOT** permitted to use cross-drilled or grooved brake discs/drums.

17.2. Brake lines may be re-routed. It is recommended that flexible braided brake lines be installed.

17.3. It is permitted to replace/add a brake pressure-limiting valve. Any such valve **MUST NOT** be able to be adjusted by the driver while driving the car.



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17.4. It is permitted to remove or deform brake back plates and add additional ducting to aid brake cooling.

18. WHEELS.

Wheels MUST retain standard diameter, width and offset and be a maximum rim width of 5.5", unless the standard rim width of the model is greater. Non standard wheels may be fitted but standard diameter and offset must be retained.

19. TYRES.

A Yokohama A048R tyre may only be used. Tyres must not have a lower profile than originally fitted by the manufacturer.

20. FUEL & FUEL TANK.

20.1. Only Petrol pump fuel. NO additives are permitted.

20.2. The original fuel tank may be replaced by another, provided it is located in the original location.

20.3. If the fuel tank is located inside the car, a safety, FT3 type, tank MUST be fitted if the standard tank is not used.

20.4. An alloy fuel tank, maximum capacity 5 litres, fitted with fuel tank foam may be fitted inside the engine compartment.

21. WEIGHT.

*** A minimum weight including driver, is set for individual makes and models of eligible cars. When a make/model is entered for which there is not specified minimum weight for that make/model will be published in Motorsport Ireland bulletin prior to the first event in which that car participates.

Specified minimum weights will be reviewed after the second round and any adjustments will be published in the Motorsport Ireland bulletin.

CLASS A

Make/model	Weight (kg)
Austin Rover	
Metro 114 Gti	850
Citroen	
Saxo VTR	865
Ford	
Fiesta XR2i	880
MG	
ZR (on 16" wheels only)	930
Peugeot	
106 XSi	865
205GTI	850
Renault	
Clio 1.4 16V	900

Rover

214	940
Suzuki	
Swift GTi	720
Vauxhall	
Nova 1.6	825
Corsa	880
Volkswagen	
Golf GTI 1.6	845
Lupo	900
Polo GT	

CLASS B

Make/model	Weight (kg)
Citroen	
Saxo VTS	900
C2	900
Ford	
Fiesta	900
Peugeot	
106	900
Volkswagen	
Lupo	900

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22. MISCELLANEOUS.

22.1. Strut braces are permitted and their design and material is free, but they must not connect to any part of the front bulkhead.

22.2. Adequate ventilation MUST be provided to the passenger compartment.

22.3. MI, the organisers or the series scrutineer have the right to seal and inspect any aspect of the car.

22.4. Contravention of the regulations will result in the loss of TWO maximum scores in the Rallycross Championship. Other penalties including the loss of ALL points may apply depending on the severity of the contravention as decided by the Championship Registrar whose decision may be appealed to the MI Championship Stewards in accordance with the established procedure at the time.

22.5. **Sealing.** The Series Eligibility Scrutineer may wish to seal engines and transmissions as a control at any time during the championship or after an event for subsequent checking. The competitor MUST provide the facility to seal (3mm holes drilled in two adjacent bolt/studs/castings) so that no stripping can be done without removing a seal. The following items must have the facility to be sealed so that they cannot be removed and or dismantled without breaking the seal(s):

22.5.1. Cylinder head.

22.5.2. Camshaft.

22.5.3. Sump.

22.5.4. Inlet manifold.



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22.5.5. Exhaust manifold.

22.5.6. Clutch housing.

22.5.7. Gearbox.

22.5.8. Differential.

It is the aim of the Stock Hatch Rallycross division to encourage close safe racing at an affordable level and the organisers will constantly monitor the cars so that the spirit of the division is maintained.

23. CONTACT NUMBERS.

Graham Lenehan

087 6187305 (between 7 p.m. & 9 p.m.)

graham.lenehan@gmail.com

Tom Kirwan

086 8179664 (between 7 p.m. & 9 p.m.)

Keith Murphy

087 6776548 (between 7 p.m. & 9 p.m.)

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JUNIOR RALLYCROSS CUP

2011/2012 TECHNICAL REGULATIONS

1. INTRODUCTION.

1.1. These Technical Regulation are in accordance with MI Yearbook and it is the competitors' responsibility to ensure that the car driven complies with these regulations, all modifications are forbidden unless stated in these regulations and it should be understood that if it does not state that you can do it, you should work on the principle that you cannot.

1.1.1. Standard.

The word "standard" refers to components fitted to the vehicle by the original manufacturer and no modifications are permitted to these parts.

1.1.2. Standard pattern.

The words "standard pattern" refers to components, which are interpreted as "a replacement part, which has the same shape and features as the original part and is manufactured from the same materials as the original component and offers no performance advantage."

2. ELIGIBLE VEHICLES.

The Junior Cup is open to normally aspirated cars of less than 1000cc that were on sale within the EU. All vehicles must be of sound construction and well maintained and presented at scrutiny in a clean and race worthy condition and any car which in the opinion of the class Scrutineer does not meet with these requirements will be excluded from the event.

2.1. Classes.

Class A: Vehicles with single choke carburettors or single point fuel injection. As a guide a maximum power output of 50bhp is expected.

Class B: All other Vehicles, which have a high standard of specification, i.e. multi valve, twin cam, multipoint injection, etc.

Class C: MSA Junior Minicross cars. N.B. Class C will no longer be eligible to compete after the conclusion of the 2009/2010 Championship.

All vehicles may have equalization measures imposed upon them.

3. SAFETY REQUIREMENTS.

Requirements are in accordance with MI Yearbook Appendix 2.

3.1. Roll Cage.

A FIA approved roll cage including side intrusion bars must be fitted as per App. 2 of the Yearbook. This must not penetrate the front bulkhead and must not extend beyond the rear shock absorber mounting points. Protective

padding fitted as per MI Yearbook Appendix 2 16.1. or a Certified Cage eg. (MI/MSA).

3.2. Fire Extinguisher.

Fire extinguisher as per App. 2 of MI Yearbook. A plumbed in fire extinguisher system is recommended in accordance with App. 2 of the Yearbook. This MUST be capable of being operated by the driver when strapped into the driver's seat. In addition the system MUST be capable of being triggered from an external point at the base of the windscreen by means of a pull cable, which must be clearly marked by appropriate sticker.

3.3. Drivers Seat.

A FIA approved seat must be fitted securely to the vehicle in accordance with App. 2.20.

3.4. Safety Harness.

A FIA approved safety harness must be used in accordance with App. 2.7.3.

3.5. Electrical Cut Off Switch.

A battery cut of switch MUST be fitted to all vehicles and be capable of operation by the driver whilst sitting in the drivers seat. In addition it MUST be capable of operation from an external point at the base of the windscreen by means of a pull cable and clearly marked by the appropriate sticker. The switch must isolate ALL electrical circuits (with the exception of those that operate fire extinguishers) and must interrupt the electrical supply to the ignition system.

4. CHASSIS AND BODYWORK.

4.1. Chassis.

No chassis stiffening is permitted except for that which is derived from the fitting of the roll cage as per 3.1. No modifications or additions may be made to the roll cage and seam welding of the shell is prohibited.

4.2. Bodywork.

4.2.1. A laminated windscreen must be fitted; all other windows must be replaced by polycarbonate or lexan with a minimum thickness of 4mm. Consideration must be given to driver's compartment ventilation and for this purpose it is permitted to cut a maximum of four, 50mm holes in each of the rear side and tailgate windows. A vent hole may be added to the drivers door window large enough to accept the driver's arm then window webbing must be fitted.

4.2.2. Glass sunroofs MUST be replaced as per App. 2 of MI yearbook.

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4.2.3. The exterior of the car **MUST** remain as standard, with the exception of bonnet pins or similar retaining devices, which **MUST** be fitted. Any replacement body panels must be of original shape, material and thickness. It is not permitted to reinforce the bumpers or their mountings.

4.2.4. It is permitted to fit guards to engine sump and petrol tank.

4.2.5. Additional holes for cooling etc. may not be made to the bodywork.

5. INTERIOR.

5.1. The interior of the car **MUST** have the roof lining, carpets and all the seats, apart from the driver's removed. It is also required that all audio equipment, spare wheel, tools and standard seat belt be removed.

5.2. Front door panels and rear side panels must be removed and aluminium sheeting installed in their place, a method of opening and closing the doors from inside the car must be retained.

5.3. An interior mirror must be fitted. Appendix 2 4.2.3.

5.4. The original dashboard **MUST** be retained, additional instruments may be fitted but all standard instruments and switches must be retained in their original positions and remain operational.

5.5. The steering wheel may be replaced; this is mandatory where the car has an airbag as standard to Remove the airbag.

5.6. The heater, heater motor and all associated pipe work, ducting and vents must be retained and fully operational.

6. SUSPENSION.

6.1. Front suspension.

Koni Sport shock absorbers **MUST** be used. Lowering springs are permitted, refer to rule 6.6. Standard bushes **MUST** be used.

6.2. Rear suspension.

Koni Sport shock absorbers **MUST** be used. Lowering springs are permitted, and where required it is permitted to adjust torsion bar, refer to rule 6.6. Standard torsion bars **MUST** be used. Standard Bushes **MUST** be used.

6.3. Anti-roll bars.

Standard anti-roll bars **MUST** be retained and properly connected. It is not permitted to fit rose jointed or up-rated link bars. The standard rubber mounting bushes **MUST** be retained.

6.4. Bump stops.

Standard chassis bump stops must be fitted in original location.

6.5. Suspension mounts.

Standard suspension pick up points **MUST** be retained; it is not permitted to fit offset or adjustable mounts. Modification of standard mounts is prohibited.

6.6. Ride Height.

It is permitted to alter the ride height of the vehicle, but it must be able to be driven over a 100 mm block placed at any point under the car.

7. ENGINE.

Engines must be less than 1000cc capacity. There is rebore allowance of 0.20mm over standard. Position, mounting method and mounts must be as standard.

7.1. Bore and stroke must remain standard, it is permitted to replace valves, valve guides and valve seats with standard or standard pattern parts only.

7.2. Pistons, conrods and crankshaft must remain as standard and unmodified.

7.3. Camshaft.

Must remain as standard and unmodified, it is not permitted to regrind or reprofiled the camshaft, Camshaft timing must remain as standard.

7.4. Oil/Water cooling.

The water radiator must be standard and in the original location. Oil coolers are not permitted. Internal baffles may be fitted to the standard sump and it is permitted to modify the oil pick up pipe.

7.5. Induction System.

The standard induction system must be retained in its entirety, and must remain unmodified. It is permitted to replace the air filter with a non-standard element fitted inside original casing. It is not permitted to add any heat reflective covering to either the outside or the inside of any part of the system.

7.6. Exhaust system.

Standard exhaust manifold and front pipe must be retained and unmodified except for the removal of catalytic converters. Exhaust system must exit the rear of the car. The remainder of the system is free and must be silenced to meet required noise limit. It is not permitted to wrap any part of the exhaust system in any heat resistant covering.

7.7. Ignition system.

ECU's **MUST** be in standard "tune" and for that particular engine. You may be asked to provide proof of this, failure to do so will result in exclusion and forfeit of all championship points up to and including date of request. The original coil must be retained. Spark plugs and ignition leads are free.



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8. TRANSMISSION.**8.1. Gearbox.**

Standard gearbox and drive shafts must be retained.

8.2. Clutch.

The standard clutch pressure plate and disc must be retained.

8.3. Gear ratios.

Standard gear ratios and final drive ratios must be retained.

8.4. Differential.

The use of Limited slip, locked or torque biasing differentials are prohibited.

8.5. Traction control of any type is prohibited.**9. BRAKES.****9.1. Brake Discs/Drums.**

The brake disc/drums must be standard pattern and must not be drilled or grooved.

9.2. Brake Calipers.

Brake calipers must be original and unmodified.

9.3. Handbrake.

The handbrake must be fitted and operational, a hydraulic type handbrake is prohibited.

9.4. The standard brake bias valve must be retained and operational and not replaced by any pressure-limiting device.

9.5. It is permitted to fit non-standard linings, deform or remove back plates and fit braided brake hoses. Brake ducting may be fitted once it is connected to openings in the lower part of the front bumper.

9.6. ABS MUST be removed or disabled where fitted.

10. ELECTRICS.**10.1. Battery.**

Battery must be in original location but type is free. It must be capable of repetitively starting the engine without the use of a slave battery.

10.2. Alternator.

A standard alternator must be fitted and operational. A charge indicator light must be fitted.

10.3. Lighting.

Front headlights must be removed and replaced with covers of similar shapes. Fog lights must be removed. Two high level rear brake lights must be fitted inside the rear window, a rear LED warning light must also be fitted in the centre of the window near its upper edge and operate on an independent switch.

10.4. Wiring.

The complete original wiring loom must be retained and used.

11. WHEELS & TYRES.

11.1. Standard alloy or steel wheels must be used, with a standard offset.

11.2. Tyres must be road legal and thread MUST not be scrubbed/machined in any way.

11.3. The control tyres for all Junior Cars will be Avon CR322, Falken ZE912 or Yokohama A539.

12. STEERING.

12.1. Steering lock MUST be removed.

12.2. Power steering may be added or removed.

13. FUEL TANK/FUEL.

13.1. The standard fuel tank MUST be retained in the original position. It is not permitted to modify fuel tank in any way. Fuel lines are free and consideration should be given to the route taken and materials used to minimize any likely damage and reduce the danger of fire. Fuel filter is free.

13.2. Standard fuel pump MUST be used.

13.3. Only unleaded RON 95 type fuel may be used. All fuel MUST be of a type available on sale to the public at a retail forecourt. NO additives of any type including upper cylinder lubricants are allowed.

14. SEALING.

14.1. The organisers reserve the right to seal engines and transmissions as a control at any time during the championship or after an event for subsequent checking. The competitor MUST provide the facility to seal (3mm holes drilled in two adjacent bolts/studs/castings) so that no stripping can be done without removing the seal. The following items must have the facility to be sealed so they cannot be removed and/or dismantled without breaking the seal(s).

Cylinder head
Camshaft
Sump
Inlet manifold
Exhaust manifold
Clutch housing
Gearbox
Differential

15. EQUALITY OF PERFORMANCE.

15.1. The prime objective of these regulations is to facilitate equality of performance between different types of cars. The Organisers reserve the right to adjust these regulations to equalize power to weight ratios should any one type of car become dominant.

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15.2. The following minimum weights excluding driver are set for individual makes and models of eligible cars. When a make / model is entered for which there is not a specified minimum weight, a weight will be calculated and published in the Motorsport Ireland Bulletin.

Nissan Micra	700kg
Mini	620kg
Suzuki Swift	650kg
Peugeot 106	730kg
Toyota Starlet	685kg

16. DATA LOGGER.

16.1. A black box data logger may be fitted to ANY vehicle at ANY time on ANY event for the purpose of assessing power output.

16.2. Vehicles MUST be made available to be weighed at ANY time during an event.

17. APPROVED CHAMPIONSHIP DECALS

17.1. Competitors will be supplied with sponsors' decals to be positioned as instructed by the Championship Registrar. It is the driver's responsibility to ensure his/her car carries the correct decals in the correct position.

18. RULE COMPLIANCE

Failure to comply with these rules will forfeit the points of that round and any other round the organisers feel necessary.

19. LICENCE REQUIREMENTS AND COMPETITOR ELIGIBILITY.

Competitor: *Competitors in the Junior class must hold a Junior Speed Licence or Junior Race Licence at the start of the Championship and will be eligible to compete in this class until the end of the championship.*

Entrants: Competitor Entrant's Licence (Parents/Guardians attending events with junior). Entrants must remain at the circuit during the event so as to be available if needed by the event officials. Licence application forms available from MI.

20. CONTACTS.

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Tom Kirwan
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RX 150

2011/2012 TECHNICAL REGULATIONS

1. ELIGIBLE VEHICLES.

All RX 150's must comply with the following set of technical specifications for the MI Rallycross Championship.

If any modifications are made outside of that outlined below, drivers will face exclusion from the Championship.

All vehicles must be "as supplied" by RX Racing, without modification. RX Racing will supply, upon request, detailed specification of vehicle.

2. ENGINE.

954cc Four Stroke In-Line Four, 16v DOHC
PGM-FI Fuel Injection
Multi Chamber baffle Aluminium Billet Sump
Bore - 75mm
Stroke - 54mm
Compression Ratio - 11.5:1
Exhaust Cam Duration - 240 Degrees
Inlet Runner and Air Box
Tuning Frequency - 7250rpm
Exhaust tuning frequency - 10600rpm

3. GEARBOX.

Honda 6 speed sequential gearbox
Quaife cushion link drive
9 plate Honda wet clutch

3.1. Transmission.

Quaife geared link drive, reduction
Gearbox with reverse (acting after 6spd) and cushioned coupling
Quaife Automatic Torque Biasing differential
Final Reduction Ratios:

1st 15.47:1
2nd 11.11:1
3rd 9.20:1
4th 8.05:1
5th 7.39:1
6th 6.84:1

4. FUEL.

Only standard fuel pump may be used

5. CHASSIS.

38mm CDS tabular Steel Space Frame
Nylon Powder Coated
Safety tested beyond FIA Standards
Aluminium 5 bar plate floorpan

6. SUSPENSION.

Unequal Length double Wishbone all round
Front travel 343mm
Rear travel 368mm

7. SHOCK ABSORBERS.

Intrax one way adjustable gas charged dampers with spherical bearings as supplied by RX Racing.

8. STEERING.

Aluminium Rack + Pinion
1.3 Turns lock to lock
Turning circle 8.0m

9. BRAKES.

Dual Master cylinder (girling cylinders)
Adjustable balance bar,
Stainless steel cross drilled discs (6mm thick, 240mm diameter front, 223mm diameter rear)
Dual piston Brembo callipers all round
Stainless steel braided and coated hoses
Ferodo Platinum pads FBD108P

10. SEATING.

FIA approved high bac race seat see Appendix 2.20.
FIA racing harness see appendix 2.7.
Two mirrors fitted
Windows webbing fitted both sides.

11. WINDSHIELD.

6.5mm thickness Polycarbonate (electric wiper system fitted).

12. TOWING EYES.

Fitted front + rear

13. LIGHTING.

Rear fitting - one wet weather light and two brake lights see Appendix 2.2.

14. FIRE EXTINGUISHER.

Plumbed in system, activation within the cockpit and external pull cord.
Marked see appendix 2.6.

15. FUEL CELL.

23L aluminium race cell foam filled with roll over valve.

16. COOLING.

Aluminium radiator
11" thermostatically controlled electric fan

17. EXHAUST.

4 into 2 into 1 stainless steel system
Equal length headers
Stainless steel race can

18. WHEELS.

FRONT 13x7 Revolution Rims
REAR 13x8 Revolution Rims
Mud guarding and flaps fitted

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19. TYRES.

FRONT Yokohama AO48 185/60R13

REAR Yokohama AO48 205/60R13

20. DIMENSIONS.

Lenght 2800mm

Width 1800mm (not exceeding 1900mm
at outer point)

Height 1450mm (can be adjusted by team)

21. RACING WEIGHT.

Including driver 560 kg

**Ballast fitted below the seat or at front of
steering wheel.**

22. The use of HANS devices is recommended.

23. The Organisers see to ensure all cars are equal specification in ALL respects. Therefore, the organisers reserve the right to swap any component between competitors throughout the event. This may be done at any time, without prior notification and is not subject to any appeal.

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AUTOCROSS

APPENDIX 85

MOTORSPORT IRELAND (MI) NATIONAL AUTOCROSS CHAMPIONSHIP 2012

1. GENERAL PRINCIPLES.

1.1. The Championship shall be decided on the best performances in a number of selected events less two drop round scores. The number of selected events shall be decided annually with a view to allowing clubs which are willing and considered competent to run championship events. Those selected will be designated (A) in the MI Calendar of Motoring Events. A Competitor must compete in at least **six** selected events with at least one finish result to be eligible for any award.

1.2. The Championship shall be open to MI and MSA licence holders.

2. CLASSES - TO BE RUN STRICTLY TO CHAMPIONSHIP SPEC. BY CLUBS, AS FOLLOWS:

- 1 Sports Car & Saloons up to 1400cc with engine over driven wheels.
 - 1(a) **Deleted from 1.1.12.**
 - 2(a) Saloons & Sports Cars from 1401cc to 1650 cc with engine over driven wheels.
 - 2(b) All other Saloons & Sports Cars to 1650cc.
 - 3(a) Saloons & Sports Cars over 1650cc with engine over driven wheels.
 - 3(b) All other Saloons & Sports Cars not qualifying for Class 1,2 or 3(A).
 - 4(a) Specials, Single Seaters Sports Racing Cars all motorcycle engine cars including 4 WD cars up to 2100cc. Only open to competitors 18 years and over.
 - 4(b) Specials, Single Seaters & Sports Racing Cars including 4WD Cars over 2100cc. Motorcycle engines not allowed. Only open to competitors 18 years and over.
 - 5(a) "Intermediate class" for 16 – 18 year olds, holding a "Speed National B" license in saloon cars up to 1650cc.
 - 5(b) "Junior class" for 14 – 16 year olds, holding a "Junior" license in saloon cars up to 1400cc.
 6. Optional class for Club Members cars, unlimited cc. This class is not eligible for championship awards and points.
 - 6(a). Deleted from 1.1.11.

NOVICE AWARD: "Best Novice" award each event to the fastest competitor holding a competition licence for his/her first year.

Competitors must state on their entry form which class they are entering.

No extra classes to be added by Clubs "on the day".

3. PENALTIES.

5 Seconds for striking or passing on the wrong side of a STANDING red marker. Where flags have been knocked down during a run and not replaced competitors are still required to follow the intended course otherwise penalties as above will be applied.

4. COURSES.

All courses must be laid out in a safe configuration for competitors and spectators.

4.1. It is recommended that the track width should be a minimum of 6 metres across.

4.2. All markers must be clearly visible and made of safe material. Bamboo and electrical conduit are prohibited for use as stakes for red flags. Corrugated board is not to be used as material for red flags.

4.3. No course must go within 30 feet of a boundary fence or other hazard. Course to be laid out with one colour flag on the inside and a different colour on the outside of the track. The use of bamboo as a flag pole is prohibited.

4.4. Starts.

Flying starts from a dummy grid placed approximately 5 yards behind the Timing/Start line. Timing to commence when the front of the car crosses the Timing/Start line. Start and finish lines must be clearly defined and should be on a reasonable level surface.

4.5. Corners.

Corners on adverse camber should be avoided if possible.

4.6. Surface.

Should be reasonably level whenever possible.

4.7. Track.

A number of right and left hand bends (changes of direction) are recommended. Oval tracks are prohibited.

4.8. Changes to a course during an event between timed runs in the interest of safety to be at the discretion of the Clerk of the Course acting in consultation with the event's Safety Officer and the competitors and shall consist of moving course markers the maximum width of a cars' clear passage past the hazard causing the change. Any change to a course outwards should be compensated by reducing track at another corner and visa versa.

4.9. All Championship classes run first.



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4.10. When possible, organising clubs should avail of the services of qualified personnel. Where heavy plant and machinery is utilised, only certified drivers can operate the equipment.

4.11. All individuals involved in the preparation of a venue should be signed on.

5. CARS ON COURSE.

There shall be no overtaking and cars will be started singly at such intervals as will ensure that not more than 3 cars are on the course at any one time, and that the possibility of baulking is reduced to a minimum. In dusty conditions organisers should satisfy themselves that the visibility is adequate for all competitors, and if it is not they must reduce the number of cars on the course to rectify the position. Corners should be of a flowing nature, thereby avoiding tight turns and preventing rutting and undue damage to the field.

6. BAULKING.

Claims for baulking must be made at the end of the run in which the baulk occurred, and before the next run commences. A separate chute for baulked cars should be provided in clear view of timekeeper.

7. CARS AND EQUIPMENT.

Also to be read in conjunction with "Scrutiny Requirements, Appendix 2.

7.1. Modifications.

All forms of tuning and lightening permitted, and cars fitted with super-chargers, turbo-chargers and forced induction carburettors will have their capacity multiplied by 1.7. 4 WD will incur a 5% time penalty.

7.2. Twin engines NOT permitted.

7.3. Tyres.

Normal Production Car Road Tyres permitted including Mud and Snow type. Studded or altered pattern NOT permitted in any class.

Use of Quad or Agricultural and Unimog tyres are not permitted. All tyres used for autocross must be highway approved and have an E mark and A R Rating.

7.4. Deleted 1.1.11.

7.5. Limited slip and locked differentials are permitted as a modification.

7.6. Cars and equipment must comply with the MI scrutiny requirements for speed events, including the use of roll bars in all cars and laminated or Poly-Carbonated windscreens in Saloons.

7.7. Competitor Number should be displayed on both sides of the car carrying the championship sponsors logo and should be a minimum of 6" high black on white background.

7.8. Crash Helmets must comply with MI specification as listed in Appendix 2 of this Yearbook.

7.9. Fire Extinguishers must be minimum 1.75litre AFFF equivalent listed in Appendix 2.

7.10. Vehicle Logbooks are compulsory.

7.11. THESE REGULATIONS MAY BE ALTERED OR UP-DATED DURING THE YEAR AND SUCH ALTERATIONS WILL BE PUBLISHED IN MOTORSPORT BULLETIN.

8. DRIVERS.

Driving to a maximum of 3 drivers per car is permitted. Competitors must advise COC of change of competition car and be inspected in the driving position by scrutineer or safety officer.

9. ENTRY FEES.

9.1. Entry fees must not exceed €90.00 per event + personal insurance. €45.00 per event for classes 5(a) & 5(b) + personal insurance. **All entry fees to include €3 per Driver Registration fee.**

9.2. All club entry forms should carry the championship sponsor name.

9.3. Recommended start time for events:

Saturday - Start of timed races 3.00 p.m.

Sunday - Start of timed races 11.00 a.m.

10. POSTPONEMENTS AND CANCELLATIONS.

Where weather conditions make the running of an event doubtful the decision for or against running shall rest with the club concerned up to midday of the day prior to the event. In the event of a sudden deterioration on the day of the event, the decision shall rest with the Clerk of the Course in consultation with the Stewards of the meeting. In these circumstances organisers must make every possible effort to notify intending competitors of the cancellation. Any Club cancelling an event on the advertised date shall be required to satisfy the Motor Sport Commission that the decision could not have been taken before midday on a previous day, and in default of such evidence shall be liable for a fine of €150. In the case of such a cancellation the event will still count towards the Championship on a new date if granted by the Competitions Advisory Committee of MI in consultation with the Chairman and Registrar of the Autocross Sub-Committee. Should an event be abandoned for reasons of Force Majeure after one timed event the round will still count in the Championship



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provided the Stewards are satisfied the event could not continue. The Clerk of the Course at an event in conjunction with the Stewards of the Meeting may cancel an event if requested to do so by the land owner, or in the case of Force Majeure.

11. NOTIFICATION OF RESULTS.

11.1. The Registrar is to receive on the Friday before the event, a list of all entries that would be eligible for Championship points. This will be strictly enforced.

11.2. Organising clubs shall prepare on the day a duplicate set of results giving the following information:

- Full details of event Starters including entered class.
- Full details of Competitors timed runs including Class Results and Overall Results.

11.3. Name, full initials, MI Licence No., and the times of all competitors in each class to reach: Brian Keegan, Riverside, Firhouse Road, Dublin 24 within 8 days of completion of the event. A penalty of €10 per week will be incurred for any delay in forwarding results after this 8 day period. Where results in any class or classes are the subject of an unresolved protest or appeal, the results of other classes shall be submitted within the time limit stated, together with provisional results for the disputed classes.

11.4. It is requested that clubs endeavour to telephone the registrar with the above results either the night of the event or the next day.

12. RECOMMENDATIONS TO ORGANISERS OF ALL AUTOCROSSES

12.1. Priority must be given to providing grass fields whenever possible.

12.2. It is strongly urged that a consistent time table be maintained throughout the series. It is recommended that Each Competitor would commence Practice immediately following Mechanical and Paper Scrutiny.

12.3. No alcohol should be allowed in paddock.

12.4. Order of running; Class 1 cars will be the first cars to run the first championship event of the season. Class 2 at the second event, and so on rotated through the season. However, Saloons should run before Specials and buggies at each event.

12.5. The attention of Specials' Entrants Drivers and Scrutineers is drawn to MI regulations covering speed events, with particular emphasis on fireproof bulkheads and bodywork.

12.6. Awards.

The following awards must be made each day:

FTD: Fastest Time of the Day award to both the fastest saloon driver and the fastest special driver.

Awards: 1st, 2nd and 3rd awards in each class regardless of the number of competitors.

Ladies Awards: should also be made for 1st, 2nd and 3rd in the Ladies not receiving another class award.

Novice Award: should be made for the best Novice where applicable.

12.7. Adequate signposting from the nearest town(s) to the venue should be arranged; and clubs must make adequate efforts to publicise the events locally especially via community radio and press.

12.8. Organisers of Championship Events are requested to contact:

Brian Keegan,
"Riverside", Firhouse Road, Dublin 24.
Mobile: 086-6997474.

12.9. Organisers are requested to have a properly laid out paddock for cars and trailers.

12.10. Organisers must provide an up to date results board on display throughout the event.

12.11. Organisers are strongly urged to hold at least 2 timed runs with a maximum of 4 timed runs per event.

12.12. Timing.

Timing is the responsibility of the club who are running the event. It must record timesto 1/100th of a second (compulsory).

12.13. In the interest of preserving venues, organisers are requested to try and re-instate damage to fields caused by their event, or provide monetary assistance to the landowner to have this done.

12.14. Organisers should mark the surface of the course before practice.

12.15. Organisers are recommended to involve local charities in their events.

12.16. Each organiser shall appoint a Safety Officer for their event who be should named on the regulations.

13. SAFETY PRECAUTIONS.

It is now mandatory for organising clubs to provide adequate fire extinguishers around the course at every event. Organisers are reminded that MI Rules for Speed events in relation to ambulances, fire extinguishers, crowd control etc. also apply to Autocross.

A suitable all terrain vehicle (eg Tractor or 4x4 Jeep) must be provided.



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A suitable telephone communications system must be provided by the club for emergency use.

14. APPLICATION OF RULES.

Refer to GCR's Chapter 14.

15. PRACTICE.

All cars should complete a minimum of 1 lap practice. Practice should be for a maximum of four cars at a time, with no pace car or similar control. Cars arriving after the start of timed runs may not be given a practice run. Classes 5(a) & 5(b) should complete 2 laps minimum.

16. DOCTORS AND AMBULANCES.

Organisers should note that one proper Ambulance must be in attendance throughout.

Clubs must provide an emergency route for the ambulance which must be kept clear at all times during the event. The organising club must provide the ambulance crew with route notes and a map to the nearest Hospital. The Safety Officer should ensure that a person with knowledge of the route to the hospital, be assigned to the ambulance crew for the duration of the event.

17. ANNOUNCEMENT OF RESULTS.

17.1. Organisers should make a special effort to announce results and present awards not later than 1 hour after the conclusion of the event and results must be properly signed.

17.2. Prize giving should be held at the venue on Sunday evening.

18. SAFETY PLAN.

A safety plan is compulsory for all autocross events. A sample plan showing the minimum requirement is available from MI. Each club must appoint a safety officer for their event, and are requested to submit the safety plan with the permit application to MI. No permit shall be granted until a satisfactory plan has been received.

19. COURSE INSPECTION.

All Courses must be inspected well in advance by arrangement with a member of the MI Autocross Committee in conjunction with the Club's Safety Officer. A Course Inspection Report must be completed and returned to MI when the applying for the event organising permit.

20. COURSE - ADDITIONAL.

20.1. Paddock should not be in the centre of the track.

20.2. Paddock should be clearly defined, separately from spectator parking areas.

20.3. No spectators should be located opposite the end of the straights.

20.4. Paddock exit/entry should have "holding lanes" for competitors entering and leaving the paddock, with a marshal in charge.

20.5. A separate area away from the Paddock and general public should be allocated for parking competitors' trailers if space restrictions dictate.

21. SAFETY REQUIREMENTS.

21.1. Competitors are reminded that flame resistant clothing is compulsory at all autocross events. Minimum requirements are set out on Appendix 2 & 3 for Autocross event.

21.2. Competitors not adhering to safety requirements while competing will not be allocated a time for that run.

22. COMPETITOR CHECKS DURING EVENT.

Clubs are requested to appoint an individual(s) to check the readiness of Competitors at the Start of each timed run (goggles, helmet fastening, harness, gloves, suits, fire extinguisher etc.).

This should be carried out at random and at a reasonable distance or time before cars are allowed approach the start line so as not to interfere with the Competitors and timekeepers as they prepare to start their timed run.

Competitors should not be released to the start line if they do not comply.

23. A.T.V.'s.

Not allowed at autocross venues.

24. PASSENGERS.

No passengers allowed in competition vehicles during competition or practice. Only 1 exception junior drivers will be allowed practice with a parent/mentor in their vehicle.



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NATIONAL AUTOCROSS CHAMPIONSHIP 2012 INFORMATION

30. POINTS TABLE.

30.1. Points as under will be allotted as per the final results as signed by Clerk of Course of each championship event of each registered Competitor, eg. 1st Registered Competitor, 2nd Registered Competitor etc.

Class Points System:

1st in Class	10 Points
2nd in Class	8 Points
3rd in Class	7 Points
4th in Class	6 Points
5th in Class	5 Points
6th in Class	4 Points

Each competitor who does not score points in the class structure and is deemed a starter by the Clerk of the Course of the event shall receive 3 points in the Class Results.

30.2. Lady Competitor shall score points towards the Ladies Award on the same scale as the Class Structure referred to in (a).

- 1st at each event in Championship
- 2nd at each event in Championship
- 3rd at each event in Championship

30.3. For the allocation of overall championship points the cars will be divided into two distinct groups, namely:

"Saloons" – Classes 1, 2, 3 and 5.

"Specials and Buggies" – Class 4.

Points as under will be allotted based on the final results of the event for competitors in each group:

- 1st in group – 20 points
- 2nd in group – 18 points
- 3rd in group – 16 points
- 4th in group – 14 points
- 5th in group – 13 points
- 6th in group – 12 points
- 7th in group – 11 points
- 8th in group – 10 points
- 9th in group – 9 points
- 10th in group – 8 points
- 11th in group – 7 points
- 12th in group – 6 points
- 13th in group – 5 points
- 14th in group – 4 points

Each competitor who does not score points and is deemed a starter by the Clerk of the Course of the event shall receive 3 points on the Overall Results.

30.4. "Fastest time run of the day" being the fastest time for one run as declared by the Final Results signed by the C.O.C. shall attract a bonus 0.5 of a point in the Overall Results to the winning competitor.

31. TIES SHALL BE DECIDED AS UNDER.

31.1. In Counting Events; the points for a class place tied for, and the points for the next lower class place or places shall be added together and divided equally between those tying.

31.2. In the Championship Overall; ties will be decided in favour of the competitor who obtains the most FTD's in their group in the series, and if this fails to resolve the tie, the result will be determined in favour of the driver with the greatest number of firsts, seconds, etc. overall or in their class as appropriate and so on until the tie is resolved.

31.3. For the Fastest Time of the Day; there shall be a run off.

32. CHAMPIONSHIP AWARDS.

- 1st Overall Special (Barry Transport Trophy)
- 1st Overall Saloon (Kenequip Perpetual Trophy)
- 1st Overall Intermediate (Donie Lucey Trophy)
- 1st Overall Junior (M.A.G. Keegan Cup)
- 1st Overall Ladies (Pat Kerley Perpetual Trophy)

In addition to the above awards, there will be 1st, 2nd and 3rd in each class including ladies. No competitor may win more than one award in the championship. Lady competitors will be placed in the class for ladies in the championship overall in preference to their capacity class, and the next highest competitors will fill their places in the capacity classes.

33. REGISTRATION OF DRIVERS.

To be eligible to score points in the Championship, a driver must be registered. This may be done by completing the forms specified, available from the MI or directly with the registrar. A competitor may register at any time in the season but will only score points from that time. The registration form requires a driver to nominate the class in which he wishes to compete, and a competitor can only score points in his/her nominated class. Each registered competitor will be allocated a permanent competition number for the season, on payment to the registrar of €25.00. The Event Secretary must receive entry for the event on the previous Friday in order for registered competitors to be eligible for Championship points.

A competitor may use a fellow competitor's car providing:

- a) their own car is no longer servicable
- b) they have the owners consent
- c) the car is eligible for the class originally entered
- d) They are inspected by COC or Safety Officer in driving position



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34. CHAMPIONSHIP EVENTS.

The list of Championship events is:

18 March	Birr
21-22 April	Carrick on Suir
9-10 June	Imokilly
23-24 June	Castle
7-8 July	Galway
11-12 August	Midland
25-26 August	Carlow

35. ADDITIONAL POINTS.

35.1. Bonus points per round will be added to each competitors final score who compete in all championship rounds.

35.2. Deleted.

35.3. Deleted 1.1.12.



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STANDARD REGULATIONS FOR ALL NON -CHAMPIONSHIP AUTOCROSS EVENTS

(inc. Loose Surface and Grass Surface)

1. CLASSES.

At discretion of clubs ***Specials, Single Seaters, Sports Racing Cars and all motorcycle engined cars are only open to competitors 18 years and over.***

2. PENALTIES.

5 seconds for striking or passing on the wrong side of a STANDING red marker. In the case of a baulk only, penalties incurred in the original run will be carried forward to the re-run in all cases. Where markers have been knocked down during a run and not replaced competitors are still required to follow the intended course otherwise penalties as above will be applied.

3. COURSES.

3.1. Starts.

Flying starts from a dummy grid placed approximately 5 yards behind the Timing/Start line. Timing to commence when the front of the car crosses the Timing/Start line. Start and finish lines must be clearly defined and should be on a reasonable level.

3.2. The club must provide a safe cordoned off area for spectators. Forbidden ground signs must be displayed where spectators are not allowed.

3.3. Changes to a course during an event between timed runs in the interest of safety to be at the sole discretion of the Clerk of the Course acting in consultation with the event Safety Officer and MI Steward. The Clerk of the Course must then call drivers and advise them of changes to the course.

4. CARS ON COURSE.

There shall be no overtaking and cars will be started singly at such intervals as will ensure that not more than 4 cars are on the course at any one time, and that the possibility of baulking is reduced to a minimum. In dusty conditions organisers should satisfy themselves that the visibility is adequate for all competitors, and if it is not they must reduce the number of cars on the course to rectify the position. Corners should be of a flowing nature, thereby avoiding tight turns and preventing rutting and undue damage.

5. BAULKING.

Claims for baulking must be made at the end of the run in which the baulk occurred, and before the next run begins.

6. CARS AND EQUIPMENT

6.1. Also to be read in conjunction with "Scrutiny Requirements, Appendix 2.

6.2. Modifications.

All forms of tuning and lightening permitted and cars fitted with super-chargers, turbo-chargers and forced induction carburettors will have their capacity multiplied by 1.7.

6.3. Twin engines NOT permitted.

6.4. Tyres.

6.4.1. Deleted 1.1.11.

6.4.2. Use of Quad or Agricultural and Unimog tyres are not permitted. All tyres used for autocross must be highway approved and have an E mark and A R Rating.

6.5. Limited slip and locked differentials are permitted as a modification.

6.6. Cars and equipment must comply with MI Scrutiny Requirements for speed events, including the use of roll bars in all cars and laminated windscreens in saloons.

6.7. Competitor numbers should be displayed on both sides of the car. Numbers should be a minimum of 6" high, black on white background.

6.8. Crash Helmets must comply with MI specification as listed in Appendix 2.

6.9. Fire Extinguishers must be min 1.75litre AFFF or equivalent as per Appendix 2.

6.10. Vehicle logbooks are compulsory.

6.11. All cars must be fitted with mudflaps.

6.12. Protective clothing must comply with the standards set out in Appendix 2 and 3 for autocross events.

7. DRIVERS.

Driving to a maximum of 3 drivers per car is permitted. Competitors must advise COC of change of competition car and be inspected in the driving position by scrutineer or safety officer.

8. POSTPONEMENTS AND CANCELLATIONS.

Where weather conditions make the running of an event doubtful the decision for or against running shall rest with the Club concerned up to midday of the day prior to the event. In the event of a sudden deterioration on the day of the event,



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the decision shall rest with the Clerk of the Course in consultation with the Stewards of the Meeting. In these circumstances organisers must make every possible effort to notify intending competitors of the cancellation. Any Club cancelling an event on the advertised date shall be required to satisfy the Motor Sport Commission that the decision could not have been taken before midday on the previous day, and in default of such evidence shall be liable for a fine of €150. The Clerk of the Course at an event in conjunction with the Stewards of the Meeting may cancel an event if requested to do so by the land owner, or in the case of Force Majeure.

9. RECOMMENDATIONS TO ORGANISERS OF ALL AUTOCROSSES.

9.1. Drivers briefing must be held before practice begins.

9.2. The attention of Specials Drivers and Scrutineers should be drawn to MI regulations covering speed events, with particular emphasis on fireproof bulkheads and bodywork.

9.3. Awards to be given out at Clubs discretion.

9.4. Adequate signposting from the nearest town(s) to the venue should be arranged and clubs make adequate efforts to publicise the events locally especially via local radio and press.

9.5. Organisers are requested to have a properly laid out paddock for cars.

9.6. Organisers must provide an up to date results board on display throughout the event.

9.7. Timing shall be at least 1/100th of a second. The Timekeeper must be in line with the start and finish line when timing.

9.8. No alcohol should be allowed in paddock.

10. SAFETY PRECAUTIONS.

It is now mandatory for organising clubs to provide adequate fire extinguishers around the course at every event. A suitable all terrain vehicle (eg tractor or 4x4 Jeep) must be provided. A suitable telephone communications system must be provided by the club for emergency use.

11. PRACTICE.

All cars should complete a minimum of 1 lap practice. Junior Licence holders will be allowed a minimum of 2 laps practice. Practice should be for a maximum of four cars at a time with no pace car or similar control. Cars arriving after the start of timed runs may not be given a practice run.

12. DOCTORS AND AMBULANCES.

Organisers should note that one proper Ambulance must be in attendance throughout.

Clubs must provide an emergency route for the ambulance which must be kept clear at all times during the event. The organising club must provide the ambulance crew with route notes and a map to the nearest Hospital. The Safety Officer should ensure that a person with knowledge of the route to the hospital, be assigned to the ambulance crew for the duration of the event.

13. SAFETY PLAN.

A safety plan is compulsory from the 01.01.06 for all autocross events. A sample plan showing the minimum requirement is available from Motorsport Ireland.

Each club must appoint a safety officer for their event, and are requested to submit the safety plan with the permit application to Motorsport Ireland. No permit shall be granted until a satisfactory plan has been received.

14. COURSE INSPECTION.

All Courses must be inspected well in advance by arrangement with a member of the MI Autocross committee in conjunction with the Club's Safety Officer. A Course Inspection Report must be completed and returned to MI when the applying for the event organising permit.

The MI Steward and the Club Safety Officer should jointly inspect the course prior to the commencement of practice.

15. COURSE ADDITIONAL.

15.1. Paddock should not be in the centre of the track.

15.2. Paddock should be clearly defined separately from Spectator parking areas.

15.3. No spectators should be located opposite the end of the straights.

15.4. Paddock exit/entry should have "holding lanes" for competitors leaving and entering the paddock, with a marshal in charge.

15.5. A separate area away from the paddock should be allocated for parking competitors' trailers.

15.6. All standing and dangerous objects must be adequately protected by straw bales or tyres.

15.7. Where possible, organising Clubs should avail of the services of qualified personnel. Where heavy plant and machinery is utilised, only certified drivers can operate the equipment.



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15.8. All individuals involved in the preparation of a venue should be signed on.

16. SAFETY REQUIREMENTS.

Competitors not adhering to the safety requirements whilst competing will not be allocated a time for that run.

17. PRIORITY ENTRIES.

Clubs must give five priority entries to intermediate drivers, (16-18 year olds) if requested.

18. COMPETITOR CHECKS DURING EVENT.

Clubs are requested to appoint an individual(s) to check the readiness of Competitors at the Start of each timed run (goggles, helmet fastening, harness, gloves, suits, fire extinguisher etc.).

This should be carried out at random and at a reasonable distance or time before cars are allowed approach the start line so as not to interfere with the Competitors and timekeepers as they prepare to start their timed run.

Competitors should not be released to the start line if they do not comply.



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AUTOCROSS SINGLE-SEATER / SPECIALS AND BUGGIES

1. GENERAL.

No car may be driven in the event until it has been approved by an Motorsport Ireland (MI) Scrutineer of the meeting.

1.1. If after approval any car is dismantled or modified in a manner which may affect its safety or eligibility, it is involved in any incident which is likely to have a similar effect, or any aerodynamic devices is added or removed, it shall be represented for approval. The entrant shall be responsible for seeking such approval. Vehicle logbooks are compulsory.

2. NOISE.

A limit of 105 dB is imposed for all cars. The noise will be measured with a sonometer regulated at "A" and "SLOW", placed at an angle of 45° to and a distance of 50 cm from the exhaust outlet, with the car's engine running at 4500 rpm. A carpet of minimum 1.50 x 1.50 m must be placed over the relevant area of ground.

3. TOWING EYE.

Must be fitted at the front and at the rear. These eyes must not protrude beyond the perimeter of the bodywork seen from above. They should be painted a bright yellow, red or orange, and must be fitted so as to be easy to find for the rescue in case of emergency.

4. DRIVER'S SEAT.

A complete, FIA-Homologated seat is recommended. This seat may not be modified in any way. A seat which has not been Homologated by the FIA may be pierced to allow straps to be passed through it. These straps must comply with the traction angles shown in drawing 253-42. The shell of the seat must then be reinforced locally so that it at least retains its original resistance, and the trim must protect the straps from any risk of deterioration. The seat must be securely fixed; if it is mounted on rails, or if it has an adjustable back rest, it must be additionally secured so as to be absolutely immovable and rigid. The seat shall include a headrest. The dimensions of the headrest shall be such that the driver's head cannot be trapped between the rollbar and the headrest.

5. SPARE WHEEL.

Prohibited.

5.1. Wheel & Tyres.

The complete wheel (flange + rim + inflated tyre) must always fit inside a U-shaped gauge of which the extremities are 250 mm apart the measurement to be made on an unloaded part of the tyre. Twin wheels and wheels fitted with

chains are forbidden. Studded tyre's are forbidden. Tyres fitted with "knobbly" treads or rubber studs are not permitted. No gap between two blocks measured perpendicularly or parallel to the tread may exceed 15 mm. In the case of wear or tear of the corners, the measurement will be taken at the base of the block. In the case of circular or oval blocks, the measurement is taken at the tangent of the blocks; - the depth of the tread may not exceed 15 mm. These measurements do not apply over a width of 30 mm at the edge on each side of the tread, but the blocks may not extend beyond the vertical plane of the tyre walls. If wheels made from a material other than steel are used, the competitor must provide documentary evidence that these wheels have been supplied for a series production car either as original equipment or as alternative equipment. Home-made constructions are prohibited.

5.2. Use of Quad or Agricultural and Unimog tyres are not permitted. All tyres used for autocross must be highway approved and have an E mark and A R Rating.

6. THROTTLE.

There shall be a positive means of closing the throttle in the event of failure of the throttle linkage, by means of an external spring operating on each throttle spindle or slide.

6.1. Fuel tank.

If a non-original tank is filled, it shall be located at least 30 cm from the bodyshell in both lateral and longitudinal directions, outside the driver's compartment, and must be separated from this compartment by a firewall or by a container, both of which shall be flame- proof and fire-resistant.

6.2. Fuel, oil and cooling Water tanks.

They shall be isolated from the driving compartment by means of bulkheads. So that in the case of spillage, leakage or failure of a tank, no liquid will pass into the driving compartment. The same applies to the fuel tanks vis-à-vis the engine compartment and exhaust system. The fuel tank be situated behind the seat. It must be mounted in a sufficiently protected location and be firmly attached to the car. It must not be in the driver's compartment, and must be separated from it by a fire-wall. Unless the fuel tank is isolated from the engine and the exhaust by a leak-proof, non-inflammable bulkhead, this tank must be situated at least 40 cm away from the cylinder head and the exhaust system. The filler caps of this fuel tank must be leak proof and must not protrude beyond the bodywork. The capacity of the fuel tank must not exceed 20 litres.



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6.3. Fuel Lines and pumps.

Fuel lines, oil lines and brake lines must be protected externally against any risk of deterioration (stones, corrosion, mechanical breakage's, etc.), and inside the cockpit, as far as the fuel circuit is concerned, against all risks of fire. There must be no connections on the lines situated in the cockpit. Automatic fuel-flow cut-off: It is recommended that all fuel feed pipes going to the engine and return pipes from the engine be provided with automatic cut-off valves located directly on the fuel tank which automatically close all the fuel lines under pressure if one of these lines in the fuel system is fractured or leaks. The vent lines should also be fitted with a gravity activated roll-over valve. All the fuel pumps should only operate when the engine is running, or during the starting process.

7. STEERING COLUMN.

Anti-theft devices must be removed.

7.1. Steering.

Appendix 2 3.1

8. SAFETY HARNESS.

Compulsory, with at least six points conforming to the specification- of Article 253.6 of Appendix J. The two shoulder straps shall have separate anchorage points.'

9. SAFETY ROLLAGE.

Must be fitted, as defined in Appendix 2 16. with backstays and diagonal member. Recommendation that a three corner piece of steel is used to reinforce forward section of roll cage at each joint, from main hoop.

9.1. Autocross buggies must use an FIA Certified cell/cage.

10. CYLINDER CAPACITY.

The maximum corrected cylinder capacity is 3550 cm3.

11. ENGINE PROTECTION.

A protective hoop is recommended for rear-engine. The rear part of this hoop must entirely envelope the engine including the exhaust and its outlet. This hoop must be braced in its centre. This may be connected to the underneath of the vehicle or to the main rollbar. The tubes used will have a minimum wall thickness of 1.5 mm. The protective hoop for the engine may be in several detachable parts, but in this case the joined tubes must be sleeved and the assembly effected by a bolt of 6 mm minimum diameter on each end of the sleeve, positioned at 90° the one to the other, separated by at least 30 mm. The diameter of the bolts to be used is at least 6 mm.

11.1. Lateral Protection.

This will consist of composite honeycomb structures solidly attached to steel tube structures on each side of the car. These tube structures must conform to the material specifications given in article 253.8.3. of Appendix J, with the exception of the dimensions of the tubes, which must measure at least 30 x 2 mm. These structures must be fixed to the main structure of the car. The minimum thickness of the composite panels is 15 mm, and they may be mounted on either side of the tubes. The outermost part of the protection shall be situated at the level of the centre of the wheel hubs, over a minimum length of 60 % of the wheel base. This protection shall extend outwards on both sides at least as far as the vertical planes passing through the middle of the foremost part of the rear tyre's and through the middle of the rear- most part of the front tyre's, but not further than the vertical planes passing through the outside of the foremost part of the rear tyre's and through the outside of the rearmost part of the front tyre's. The space between this protection and the bodywork must be covered, to prevent wheels penetrating it.

11.2. Bodywork.

This must be impeccably finished, in no way of a makeshift nature. It must not have any sharp angles or sharp-edged or pointed parts, and angles or corners must be rounded with a radius of not less than 15 mm. At the front and at the sides there must be hard, opaque bodywork providing protection against stones. This body work must rise at least to the level of the centre of the steering wheel, and its height must not be less than 42 cm measured from the driver's seat mounting. All mechanical elements necessary for propulsion (engine, transmission) must be covered by the bodywork or mudguards. Seen from above, all parts of the engine must be covered by sturdy, hard and opaque bodywork; the sides of the engine may be left uncovered. The panels used must not be more than 10 mm thick.

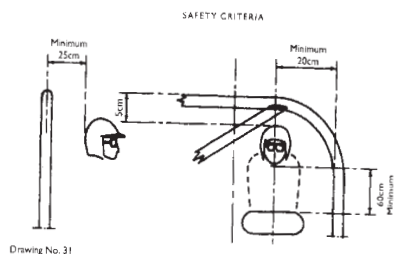
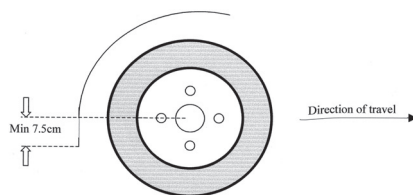
11.3. Cockpit.

The width of the cockpit, maintained over 50 cm from the most rearward point of the seat in a horizontal plane towards the front, shall not be less than 60 cm. No part of the cockpit, or situated in the cockpit, may have sharp or pointed parts. Particular care must be taken to avoid any protrusion which could injure the driver. The two safety rollbars must be high enough for a line extended from the top of the main rollbar to the top of the front rollbar to pass at least 5 cm over the top of the drivers helmet when he is seated normally in the car with his Helmet on and his safety harness fastened. A rigid roof panel above the driver is permitted. Any transmission shaft joint situated beneath the floor of the cockpit



AUTOCROSS

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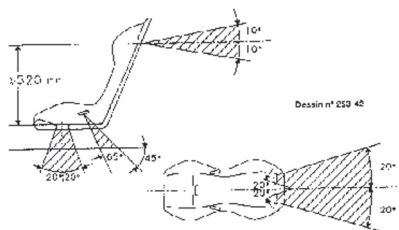
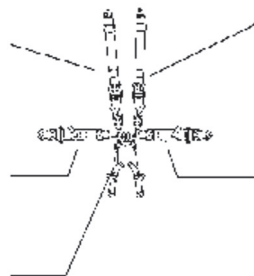


Drawing No. 31

must be enveloped by a band of mild steel at least 3 mm thick over a length of at least 25 cm, securely fixed to the chassis, in order to prevent the shaft from penetrating the cockpit or hitting the ground in case of failure of the joint. No mechanical part other than the Controls necessary for driving the vehicle may be situated in the cockpit. It is recommended that lateral protection be provided as follows for the two side openings of the cockpit: These openings must be closed completely to prevent the passage of a hand or arm. This closing must be effected: . either by netting with a maximum mesh of 6 cm x 6 cm made from cords of at least 3 mm in diameter, this netting being fixed permanently at the top and rapidly detachable at the bottom from inside or outside; - or by a wire grille with a maximum mesh of 6 cm x 6 cm, the wire being at least 2 mm in diameter, this grille being attached by two hinges at the top and having an external quick release device at the bottom, also accessible from inside the car (an opening may be made for this purpose), allowing the grille to be swung upwards to a vertical position. - or by side windows made from polycarbonate, of a minimum thickness of 5 mm. A fireproof bulkhead is mandatory between fuel tanks and occupants, and between engine and occupants.

12. WEIGHT.

The minimum weight of the vehicle, without the driver on board, must at all times during the event comply with the following scale of minimum weights according to the cylinder capacity, the number of cylinders, the type of engine and the type of transmission:



August 1996

Cylinder Capacity

< 1,300 cm3	440 kg
< 1,600 cm3	450 kg
< 2,000 cm3	500 kg
< 2,500 cm3	550 kg
< 3,500 cm3	600 kg

13. MUDGUARDS.

Mudguards must be appropriately mounted and in close proximity to all driving wheels. They must be a minimum width and provide efficient covering as defined below:

Minimum width: Covering the entire width of the tyre plus a minimum projection of 2.5cm on both sides of the tyre.

Efficient covering requirement: They must cover at least one third of the circumference of the tyre and descend towards the rear to at least 7.5cm below the centre line of the wheel at all times.

Mudguards must not have perforations or sharp edges. See diagram above.

13.1. Suspension.

The axles must be sprung. The mounting of axles directly onto the chassis is not allowed.

14. DYNAMOS, ALTERNATORS, BATTERIES.

Dynamos and alternators may be removed, but each car must have a fully charged battery. The use of any outside Source of energy to start the engine of the car on the grid or during a race is forbidden.



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15. COMPETITION NUMBER.

This must be displayed on each side of the car and on each side of a panel on the roof. The car must bear no other number likely to be confused with it. The roof number must be permanently fixed on a vertical support, 24 cm x 35 cm, with no sharp edges and must be positioned along the longitudinal axis of the car. The number must be 18 cm high and the strokes forming it must be 4 cm thick.

16. WINDSCREEN.

This must be made of laminated glass, or of a polycarbonate at least 5 mm thick. Scrutineers will not accept cars whose wind- screen shows traces of collision or cracks to such an extent that visibility is seriously impaired or that there is a likelihood of their breaking further during the event. The windscreen may be replaced, or protected, by a metal grille covering the entire surface of the windscreen opening. The mesh size shall be between 10 mm x 10 mm and 25 mm x 25 mm, and the minimum diameter of the wire of which the mesh is formed shall be 1 mm. In cars which have a laminated glass windscreen or which have the metal grille defined above and no polycarbonate windscreen, motorcycle type goggles or a visor fitted on the helmet must be worn by the driver. Apertures of a total area not exceeding 64 cm² may be made in the windscreen.

17. FUEL .

Appendix 2 28.2.

18. BATTERY.

Appendix 2 10.

19. FIRE EXTINGUISHER.

Appendix 2 6.3

20. BATTERY CUT OUT SWITCH.

Appendix 2 22. It must be operable from both inside and outside of the vehicle.

21. A DUAL CIRCUIT BRAKING SYSTEM.

Is compulsory for all cars.

22. SIDE NETTING.

Side netting is compulsory on both sides and must be securely fitted while competing. Such side netting must be FIA approved.



SPORTING TRIALS

APPENDIX 88

2011/2012 SPORTING TRIALS REGULATIONS

SPORTING REGULATIONS EFFECTIVE FROM 1.9.11

The Irish Sporting Trials Regulations and Car Specifications.

1. SPONSORSHIP.

Sponsorship is deemed to be outside the spirit of the sport.

2. SCRUTINY.

N.B. A VERY STRICT SCRUTINY WILL TAKE PLACE AT THE START OF EACH SEASON. Prior to the start of the first trial of every new season, all cars must be declared eligible by their owners before being officially scrutinised in accordance with the regulations.

3. CHAMPIONSHIPS.

It is recommended that there is no overall Annual Sporting Trials Championship.

4. PROCEDURES FOR DRIVERS AND OFFICIALS.

4.1. Standing Starts only permitted with front wheels on start line. ***A new draw should be made to decide the starting order of each driver at each event.*** Competitors must only start when observer is ready and must follow intended line of section. Greater use of "via" stakes is recommended to define intended line.

4.2. ANY REARWARD MOTION excludes further scoring in that section.

4.3. When any part of car touches a marker stake the competitor is deemed to have finished in that section and receives the value for the last pair of markers through which the centre line of the front wheels have passed. Crossing of tracks within a section is not permitted unless specifically designed into the section. When a section is laid that turns back in close proximity to itself the designer must use sufficient marker or via stakes to prevent accidental crossing of tracks. Where a section is laid through undergrowth - ensure that it is cut.

4.4. In the event of a stall, use of the starter is not permitted. An unaided forward rolling start only is allowed to qualify further progress.

4.5. A reasonable time lapse (e.g. 5 seconds) is allowed while the competitor is stationary in search of grip, after which marks will be awarded according to progress to that point. Observers to use whistles to signal end of the reasonable time.

4.6. Observers should always be in a good position to differentiate between a car touching a stake and the ground around it moving the stake.

4.7. Where a stake has been run down, care should be taken to ensure that it is replaced in exactly the same position to give equal opportunity to all drivers. Observers should use a reference point e.g. distance from tree, rock etc.

4.8. Competitors are not permitted to alter the course under any circumstances.

4.9. When a competitor has failed in a section he should immediately be advised so that the course does not get unnecessarily broken up.

4.10. Drivers, Bouncers and Passengers feet must remain on the cockpit floor within the confines of the car.

Standing not allowed. No person outside the car may assist or direct the crew during any section.

4.11. Each driver must complete his first lap in sequence. Doubling up is permitted on the 2nd lap. Where a Competitor on the first lap arrives at a section out of the sequence dictated by the draw, then he/she may be penalised 10 marks.

4.12. Clerks of the Course are responsible for vetting and approving the safety of all events, sections and section area both prior to the start of and during the trial. They are also recommended to go around the course at the end of an event to advise officials when the trial is finished. For the competitors and observers benefit the C.O.C. must be clearly identified at an event by either a hi visibility armband or vest printed with the letters C.O.C.

4.13. *Organisers are recommended to start an equal number of competitors at each of the 6 sections.*

4.14. Drivers must notify the Official at the check in control if they retire.

4.15. Drivers and Observers briefing will take place 15 minutes before the due start time.

4.16. That the width of all "Gates" in a section be a minimum of seven (7) feet. Minimum height through the section should be at least six (6) feet. Red markers must always be on the left.

4.17. The Clerk of the Course may impose a penalty of up to 10 marks on a Competitor who arrives late at the event or at the first section dictated by the draw.

4.18. An overall time limit may be imposed by an organising club and a penalty maybe imposed for infringement of this requirement.



SPORTING TRIALS

APPENDIX 88

4.19. Organisers are required to provide **and signpost** a laid out practice section for use by all competitors. This practice area will be closed upon commencement of Drivers Briefing.

4.20. 'A 10 mark "roll" penalty for each roll by a driver will be added to their final score'

4.20.1. A roll is defined as - a driven vehicle that turns over either to its side or upside down; or vertically ends up supported on its roll bar, engine or sump guard; (anywhere within the confines of the overall trial venue) and which cannot be righted by the driver, acting with no outside assistance and while within the confines of the cockpit and subject to compliance with Article 4 sub sections 4.2, 4.3, 4.4 and 4.5 herein. Competitors are required to report all rolls to the C.O.C. so decisions can be made as to what action needs to be taken to ensure the continued safety of the event.

5. MARKING SYSTEM.

5.1. Zero marks will be awarded for a clean non-stop passage through an observed section. Sections to be sub-divided and if a car stops or touches a marker pole or tape it will be deemed to have failed that sub-section. Marks will be awarded for the sub-sections completed up to the point of failure.

5.2. The 10 mark "roll" penalty should be punched against the nearest section where it occurs in addition to the mark achieved on that section by either the observer or the C.O.C.

5.3. **All ties will initially be decided in favour of the competitor with the most 0s, 1s, 2s etc. scored in the event.**

5.3.1. **Any ties that remain unresolved, excluding those in contention for the Premier Award, will be decided on the competitors ranking on the current grading list i.e. the lower position wins the tie.**

5.3.2. **Unresolvable ties for the Premier Award will be decided over an extra section, unless this is deemed impracticable by the CoC. In such a case the competitors ranking on the current grading list, as in 5.3.1. above, will be applied so a clear winner of the event is achieved.**

5.3.3. **The CoC shall determine the driving order in the extra section by a drawing of the names with the first name out driving first, second name out driving second etc.**

5.3.4. **Competitors driving the extra section will be placed in overall result according to their marks in the extra section. If the Premier Award is unresolved after the first run of the extra section, then the section should be adjusted with only the driver's tying on the lowest mark driving the altered section. This process may be repeated or, at the discretion of CoC a result declared on the basis of the lower ranked driver**

on the grading list being declared the winner as in 5.3.1. above.

5.3.5. **The results of all competitors taking part in the extra section shall determine the overall and class result for those drivers.**

5.4. The winner of the overall award will be the driver with the lowest marks. No driver may win more than one award with the exception of age related memorial trophies. "Scratch" placings take precedence over handicap and Junior awards.

6. GRADING SYSTEM.

A system to grade drivers will be employed. The Registrar will compile the grades and revise them twice yearly - at the start of the season and on 1st January of the following year. Clubs are requested to send results of trials to the registrar immediately after each event.

The Registrar is:
Mr. Hugh O'Neill
10 Parkmore Drive, Terenure, Dublin 6W.

7. JUNIOR AND 1 DAY COMPETITION LICENCE HOLDERS

7.1. Junior Licences valid only for Sporting Trials are available between the ages of 14 to 16 years old. Holders of these licences must be accompanied in the car at all times by the holder of a full competition licence, but may drive alone in a section at the discretion of the C.O.C. and/or their parent/guardian.

7.2. 1 DAY licence holders, who are deemed by the officials of the event to have inadequate sporting trials car driving experience, must similarly be accompanied in the car at all times by the holder of a full competition licence (between sections). 1 day licence holders may be permitted to drive alone in sections at the discretion of the C.O.C. (see Appendix 1 Article 12).

7.3. Both these classes of competitors are required to make use of the practice section prior to the start of the trial.

8. CREW.

8.1. Two entries will be accepted in respect of each car. No competition car may be driven on any part of the course or adjoining lands by any person other than a person who is entered for the event and who has signed the official entry form. Passengers or Bouncers may be carried in competition cars subject to the following conditions.

8.2. All Passengers or Bouncers must complete an entry form from the organisers of each event and sign the indemnity. In the case of passengers or Bouncers under 18 years of age, this must be counter signed by a Parent or Guardian.



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8.3. Competition licences are not required *for bouncers or passengers*.

8.4. *Passengers between the ages of 5 and 14 may travel between sections*, on condition that a restraining strap is worn.

8.5. Only one Passenger permitted in a car at any one time.

8.6. Any driver carrying a Passenger who has not entered the event may be excluded and be subject to such other penalty as the Stewards may decide.

8.7. The 72 hour entry rule may be applied at the discretion of the organisers of each individual event.

9. FIRST AID KIT

A basic first aid kit must be available at all trials. This kit must be located at the Start Control.



SPORTING TRIALS

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2011/2012 SPORTING TRIALS REGULATIONS

TECHNICAL & SAFETY REGULATIONS

12. CHASSIS.

12.1. Free choice of design and construction.

12.2. Minimum wheel base 60".

12.3. Minimum track front 44" and rear 48".

13. ROLL OVER BAR.

13.1. To be made from adequate strength steel.

13.2. With a minimum outside diameter of 1 1/4", a wall thickness of min. 1/8" and a 1/8" witness hole.

13.3. To be securely mounted to the chassis at a place not less than the cockpit width.

13.4. To be a minimum of 2" above the heads of the occupants but also a minimum of 40" above the base of both seats.

13.5. To have a minimum horizontal width at the top of 10".

13.6. Both sides to be braced either forwards or backwards.

13.7. To provide driver/passenger equal protection in the event of rolling over.

13.8. Bar must be padded with material no thinner than .25" (e.g. central heating pipe type insulation), and cover the complete roll bar down to within 12" of the cockpit sides.

14. ENGINE.

14.1. Free choice of type up to 2.000cc.

14.2. Free choice of location.

14.3. Any engine modification permitted.

14.4. Two throttle return springs required.

14.5. Exhaust to be effectively silenced.

14.6. Adequate guards to be fitted over exposed pulleys, fanblades, belts, etc.

14.7. The maximum noise level for Sporting Trials Cars is 95Db.

The procedure will be as follows:

- Measurements will be made at a distance of 3 metres and an angle of 45° from the exhaust outlet.
- The engine is to be held at 50% power for duration of measurement.

- Measurements are to be taken outside, with no large reflecting surfaces (e.g. walls) within 3 metres. Background noise should be 10Db below the measured level.

15. TRANSMISSION.

15.1. Rear wheel drive only permitted.

15.2. A free turning differential must be used in the rear axle. A locked, solid or welded differential is not permitted in this position. Also, any form of mechanical, centrifugal, electronic, hydraulic or automatic anti-lock or torque biasing of the drive to the rear wheels is not permitted. This includes, but not limited to, proprietary or designer variations of:

ABS - automatic braking system

LSD - limited slip differential,

ETC - electronic traction control,

ATB - automatic torque bias,

ASP - advanced stability platform.

15.3. Car must have an operable reverse gear.

15.4. All gear selection must be possible from the driver's seated position.

15.5. Adequate guards to be fitted over any exposed moving propshafts.

16. SUSPENSION.

All wheels must have an operable suspension system. The medium of suspension is free.

17. STEERING.

17.1. Front wheel steering only permitted.

17.2. Conventional type steering wheel only permitted.

17.3. *Only a steering wheel knob, similar in design to a "mushroom" topped proprietary type safety wheel knob, may be fitted.*

18. BRAKES.

18.1. Rear brakes mandatory.

18.2. Independent operation permitted.

18.3. Front brakes optional.

18.4. Handbrake levers not to be higher than top of steering wheel and rough ends to be protected.

19. WHEELS.

Free choice.

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20. TYRES.

20.1. The only rear tyre permitted is the Hakka M+S pattern car tyre, manufactured by Maxsport and selected by the Trials Committee. Tyres must be inflated to a minimum of 3 psi.

20.2. The maximum permitted rear tyre cross section (width) is 165 mm and diameter up to a maximum of 15".

20.3. Front tyre size is unlimited and may be of any standard road going car or motorcycle pattern.

20.4. Cutting or altering of the tyre tread pattern is not permitted.

20.5. Chains, studs, etc. not permitted.

20.6. Tyre to rim security bolts permitted.

21. ELECTRICAL.

21.1. Battery to be adequately secured to chassis. All battery terminals must be covered.

21.2. Battery to be sealed to prevent acid spillage e.g. substantial plastic cover.

21.3. Ignition switch to be centrally mounted on 'dashboard' and clearly marked on/off.

21.4. All wiring to be of good workmanship, properly insulated and secured to chassis/bodywork.

21.5. Electrical wiring to be routed separately to fuel lines.

22. BODYWORK AND SAFETY EQUIPMENT.

22.1. The cockpit width to be a minimum of 32" and the sides to be a minimum of 4" above the seat base.

22.2. Cockpit to accommodate two adult sized seats, side by side, which must be adequately secured to car. Right hand driving position only permitted.

22.3. Every effort should be made to keep the centre of gravity as low as possible. The maximum height of the base of BOTH seats must not be more than 21" from the ground. It is strongly recommended that the seat height be lowered below this figure especially where a new car is being constructed. The seat measurement must be made with the car on level ground without occupants, tyre pressure at 5psi and suspension at normal ride level.

22.4. Rear mudguards to provide adequate protection for driver/passenger from wheel and tyre and must cover at least half the width of the tyre. Front mudguards optional.

22.5. Fuel tanks to be secured to the chassis and be leakproof in the event of the car turning over.

22.6. A dry powder type fire extinguisher (or an alternative MI approved type) with a minimum capacity of 1kg. plus a condition gauge, to be fitted in an obvious and accessible position.

22.7. An adequate tow rope to be carried at all times, minimum length 15 feet, and to have appropriate hook/eye connections.

22.8. A solid and adequate floor to be fitted to provide protection to drivers/passengers feet and legs.

22.9. The bodywork to be a proper workmanlike construction providing a safe compartment for the driver and passenger. The bodywork also to be maintained to a standard where it will not bring the sport into disrepute.

22.10. A towing eye/hook to be fitted to the front and rear of all cars. Such eye/hook at the rear of the car should be fitted at the lowest possible point.

23. COMPETITION NUMBERS.

All cars must display a number on the rear panel. This number will be allocated at pre-season or subsequent scrutiny. The numbers must comply with the following dimensions: Height - 4 inches, width - 1.5 inches. The numbers to be black on white background.



SPORTING TRIALS

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SPORTING TRIALS STANDARD REGULATIONS

Name of Club

Sporting Trial: Date:

To be held under the International Sporting Code of the F.I.A, The General Competition Rules of Motorsport Ireland, and these Supplementary Regulations.

(This event counts towards the Club Sporting Trial Championship).

Clerk of the Course: Motorsport Ireland Steward

(name of Club) Steward: Motorsport Ireland Permit No.

Venue:

(by kind permission of

signposted from
(ensure sign posting is clear)

Start: Drivers Briefing:

Competitors are requested to check in at

The Clerk of the Course may impose a penalty of 5 marks on a competitor who arrives late at the event or at the first section dictated by the draw.

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COURSE.

The course will be of a sporting nature and will consist of observed sections only. The route may be dyed, marked or taped. Competitors must follow the intended line of each section. Competitors are requested not to damage private property and to bring home any litter. No crossing of tracks is permitted, unless specifically designed into the section. Where parking occurs on a public road warning triangles must be displayed on all approaches.

CARS & TYRES.

Must comply with the requirements of MI Regulations for Sporting Trials. See current edition of the Yearbook for details. The Club reserves the right to scrutinise cars at any time.

CREW.

Two entries will be accepted in respect of each car. Bouncers must keep their feet on the cockpit floor within the confines of the car. Standing is not allowed. No Competition car may be driven on any part of the course or adjoining lands by any person other than a person who has entered for the event and who has signed the Official Entry Form. Breach of this Regulation may merit exclusion. A Bouncer or Passenger may be carried subject to MI Regulations. See Yearbook for details. Drivers must advise the official at the check-in control if they retire. Unsporting behaviour may merit exclusion from the results. All Competitors must be in possession of a current MI Competition Licence to be produced on demand. No Competitor may receive assistance, advice or direction from any outsider whilst traversing any section except junior competitors in their first season.

JUNIOR ENTRIES.

Junior Competitors aged between 14 and 16 years may enter for the event. Such Competitors must be in possession of a junior licence and must be accompanied in the car by a holder of a full Competition Licence at all times except when competing in a section.



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STARTING ORDER.

A Draw to determine Starting Order will be held. The Result of the Draw will be displayed at Start Control. Each Driver must complete his first lap in the draw sequence. Where a Driver, on the first lap arrives at a section and the previous section has not been punched, then he will be penalised 10 marks. Doubling up is permitted on the second lap.

MARKS.

As laid down in MI regulations for Sporting Trials - See Appendix 88 Article 5.

AWARDS.

(List awards). No driver may win more than one award with the exception of age related memorial trophies.

ENTRIES.

Open immediately and must be made on the enclosed form and must reach (Name Club and Address) on or before (Time and Date). The Club reserves the right to refuse any entry without assigning a reason. INCOMPLETE, LATE or UNPAID entries will be ignored. NO TELEPHONE ENTRIES.

ENTRY FEES:

MEMBERS - €

NOVICES - € NON-MEMBERS - €

RESULTS & AWARDS.
will be announced and presented in

TIME LIMIT.

A Time Limit may be in force on the day.

SCRUTINY.

Has your car been scrutinised this season?

Yes ☐

No ☐

Any questions regarding scrutiny should be referred to

Mr Victor Sweeney. Tel: 01-4960182.
Mr Gerry Kehoe Tel: 01-4522356



4X4

APPENDIX 89

2011/2012 4 x 4 CHAMPIONSHIP AND SAFETY REGULATIONS

1. FORMAT

1.1. The **2011/2012** Championship will be run over a minimum of 8 rounds with the best 6 scores to count. If for any reason rounds are cancelled the following will apply:-

1.2. One round cancelled, two scores may be dropped.

1.3. Two or more rounds cancelled, only one score to be dropped.

However if more than 8 rounds are run then 3 scores may be dropped.

2. POINTS SYSTEM.

Overall Position- Non registered competitors will be eliminated before points are awarded as follows- 1st Overall 30 points; 2nd Overall 29 points down to 1 point for 30th Overall.

Non registered competitors will be eliminated before class points are awarded as follows - 15 points for 1st in each class, down to 1 point for 15th in class.

In case of dispute refer to GCRs Chapter 14.

3. CHAMPIONSHIP AWARDS.

1st, 2nd, and 3rd overall and 1st, 2nd, and 3rd in each class. No competitor may win more than one award. If there are less than 6 competitors in any given class, there will only be a 1st and 2nd prize awarded.

Event Awards

1st, 2nd, and 3rd overall and 1st, 2nd, and 3rd in each class. No competitor may win more than one award.

4. TIES.

4.1. Overall.

The resolution of ties in the overall championship will be decided as follows:

4.1.1. Novice driver shall take precedence (Class 4 over Class 5, 2 and 1).

4.1.2. Class 1, 2, shall take precedence over Class 5.

4.1.3. Smaller engine size - Diesel engine = 1.5 times petrol and turbo diesel engines.

4.1.4. All leaf sprung over non all leaf sprung vehicles.

4.1.5. Long wheel base over short wheel base.

4.1.6. The greater number of wins in the current Championship.

4.1.7. The greater number of 2nd, 3rd, etc. placings in the current Championship.

4.1.8. The number of competitors beaten on the 1st, 2nd, 3rd etc. event in the current Championship.

4.2. Event.

The resolution of ties for championship points and class places in an individual event will be decided as follows:

4.2.1. Novice driver shall take precedence (Class 4 over Class 2 and 1).

4.2.2. Class 1, 2, shall take precedence over Class 5.

4.2.3. Smaller engine size - Diesel engine = 1.5 times petrol and turbo diesel engines.

4.2.4. All leaf sprung over non all leaf sprung vehicles.

4.2.5. The number of 0's, 1's, 2's, etc. scored on the event

4.2.6. Long wheel base over short wheel base.

5. REGISTRATION FOR THE CHAMPIONSHIP

In order to score points in the Championship all competitors - excluding Class 4 - must fill in, sign and return the Registration Form together with a fee of €20.00 to the Event Secretary of the first qualifying round in which they compete. Points will not be awarded retrospectively.

6. 2011/2012 DATES.

Event Club	Date	Signposted from
TBA		

7. ELIGIBLE VEHICLES

7.1. Any production 4 wheel drive vehicle with 4x4 off road capabilities excluding those with LSD and/or lockable diffs. Vehicles with engine conversions (as decided by the Scrutineer /Organising Committee) are also eligible.

7.2. In the interest of economy, bumpers may be removed and air intakes may be altered.

7.3. Suspension modifications are not allowed. Haflingers, Steyrpuch , Unimogs and forward control landrovers are not allowed. 4 wheel steer and A.T.V.'s (quad/tricycle type) are also not allowed.

7.4. A roll-cage of minimum of 50 mm steel bar with wall thickness of tube to be minimum 2 mm with all 6 points properly secured to the vehicle structure generally to the shape of FIA drawing number 253.7 are mandatory for all vehicles.

7.5. Helmets and Full Harness type belts are mandatory for drivers and passengers. Helmets must comply with RTA regulations - S.I. No. 322 of 1993.

7.6. Passenger grab handles and a 2nd return spring on all throttles are mandatory for all vehicles.



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7.7. If no windscreen present crews must wear full face protection helmet or goggles.

7.8. An isolation switch capable of being operated from both inside and outside the vehicle is strongly recommended.

7.9. Battery terminals must be fully insulated. All batteries must be enclosed in a solid container.

7.10. Side impact bars are mandatory for all open sided vehicles to a minimum height of 12" from the floor. Quick Release Window webbing / mesh is required in all vehicles, where the window is non existent or to remain open. If no windscreen present on vehicle a minimum 2" mesh is required.

7.11. No loose equipment may be carried in the vehicle.

7.12. All exhausts must be protected to ensure no hot pipes are exposed.

7.13. A working horn is required on all vehicles.

7.14. A firewall must exist between driver and fuel tank.

7.15. An effective parking brake is obligatory. It must simultaneously control the two wheels of the same axle. No wheel spacers allowed.

7.16. Competitors must carry the Championship sponsors decals and door plates displaying competitor number(s), as stipulated, on all counting rounds. No points will be allocated to any competitor who fails to comply with this regulation.

8. TYRES.

8.1. Any type of production tyre may be used but must not be fitted with studs or chains or exceed 35x12.5/16. The maximum tyre diameter must not exceed 35". Wheel rim width must not exceed 8".

8.2. Tyres manufactured specifically for agricultural use or marked for use at limited speeds are not permitted. No Dumper pattern or low ground pressure "balloon" type tyres allowed. Hand cut tyres are not allowed.

8.3. Tyres with open centres or over aggressive thread patterns (as decided by the Scrutineer / organising committee) are not permitted.

9. FIRE EXTINGUISHERS.

All vehicles must carry a minimum of 2kg fire extinguisher with gauge or valid date on it, must be properly secured, cable ties not accepted.

10. TOWING EYES.

Closed towing points must be provided front and rear of the vehicle with the position of the towing eyes marked by an obvious (red or yellow) arrow situated on the bonnet and rear of the vehicle above the water/mud line. All vehicles must carry a towing strap minimum of 2 metres long with 2 shackles in a suitable place above the water/mud line, coiled and ready for use. Wire ropes must NOT be used. Snatch Rescues may not be used.

11. CLASSES.

11.1. All Suzuki 4 wheel drive vehicles including Vitara with Suzuki SJ and Samurai front axle fitted in the original design i.e. axle over spring.

11.2. All remaining 4 wheel drive vehicles excluding Class 5.

11.3. Novices - the definition of a novice shall be any competitor who has not competed in any 4x4 event previously, or who has not won any prize at any previous Championship event. Status to remain until end of season in which first award is won. Novice class is only open to Drivers of Class 1 and 2 vehicles.

11.4. Modified Class - This class is open to any eligible vehicle which may be modified subject to the following restrictions:

11.4.1. Rigid chassis only.

11.4.2. Minimum wheel base length of 2000 mm.

11.4.3. One steering axle only.

And

11.4.4. Diff locks allowed.

11.4.5. Two independent braking systems required.

11.4.6. A three point full harness and helmet for both driver and passenger is compulsory.

11.4.7. A hand operated fiddle brake system is allowed on rear axle only and must not interfere with main hydraulic system.

Competitors in this class can only compete in one class per event.

12. STARTING ORDER.

Starting Order will be announced at drivers briefing.

13. THE COURSE.

13.1. The course will be of a sporting nature and will include observed sections only. The route will be marked and the onus will be on the competitor to follow the correct course.

13.2. At least 50% of all sections must be attempted or completed. The penalty for breach of this rule is automatic exclusion.

13.3. Any tests not attempted will carry an automatic penalty of 10 points.



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13.4. All gates from number 10 to 6 will be a minimum of 3.5 metres wide with the remainder a minimum of 3 metres wide.

13.5. Where tapes is erected to define the course, competitors will be penalised for breaking it. The penalty will be that of the previous gate.

13.6. A Drivers Course Liaison Officer will be appointed for each event by the Organising Committee. He shall inspect the course in conjunction with the Assistant Clerk of the Course on the evening before the event. The role of Drivers Liaison Officer ceases once the drivers briefing is concluded.

13.7. The course may not be altered by the Drivers Liaison Officer (or any competitor) without the prior consent of the Clerk of the Course. No driver will be allowed to be Course Liaison Officer for more than two events in this Championship.

13.8. All competitors are required to sign on as marshalls in the event of their vehicle not being capable of finishing the event.

14. PRACTICE AREA.

A practice area will be designated to all novice competitors only, after they have signed on and passed scrutiny. This practice area will be closed upon commencement of Drivers Briefing.

15. SHUNTING.

15.1. All vehicles without fiddle brakes and a wheel base length in excess of 2185mm are allowed a maximum of one shunt per test.

15.2. All vehicles entitled to a shunt will be so labelled by Scrutineer.

15.3. If the shunt is not nominated to the starting marshal (or if the shunt takes place other than between the gates nominated) the shunt will not be allowed and normal penalties will be incurred for the cessation of forward motion.

15.4. Competitors must nominate to the start marshal of each test the specific gates of the test between which the shunt will be executed. If the gates are not nominated the shunt will not be allowed and normal penalties will be incurred for cessation of forward motion.

15.5. After arriving at the nominated shunting area the vehicle must be brought to a complete stop and thereafter the vehicle will be allowed to reverse one vehicle length only. Any movement in excess of this will incur the penalty of the previous gate.

15.6. While the shunt is being executed the vehicle may cross its tracks but may not touch a marker pole or break the tape.

16. SECURITY.

No vehicles may be on the course outside competition times unless with explicit permission of the Clerk of the Course. All vehicles must go to Parc Ferme when not competing.

17. MARKS AND PENALTIES.

17.1. Penalty marks from 10 to 0 will be awarded to where the vehicle ceases forward motion or touches a marker pole or breaks the tape. The complete vehicle must pass cleanly through all marker gates.

17.2. A clean non-stop passage through an observed section will gain 0 penalties.

17.3. A vehicle may not cross its track at any stage in a test.

17.4. Looping around the Start and Finish areas is strictly forbidden. Smoking is not permitted by either driver or passenger while competing on a test and seat belts must be worn by crews while competing on tests. Breach of the above rules will automatically carry the maximum penalty of 10.

17.5. The driver who starts a test must finish the test. The penalty for breach of these rules is 10 points.

17.6. The penalty for breaches of Articles 18.8., 18.9. or 18.10 is 10 marks.

17.6.1. Competitors found to be carrying an unauthorised passenger will be excluded from result and be subject to any other penalty as the Clerk of the Course may decide.

17.6.2. Competition vehicles being driven during the event by other than registered competitors may result in both competitors being excluded at the discretion of the Clerk of the Course.

17.6.3. Competitors found to be driving from test to test at excessive speed will be immediately excluded from the event.

17.6.4. Outside Assistance is prohibited while the vehicle is in the Trial Zone and the penalty shall be 10 marks for a competitor receiving such assistance.

17.7. In deciding Overall/Event results modified cars competitors only will be scored as follows:
The higher of:

17.7.1. 1.25 times actual penalties accrued OR

17.7.2. 1.25 penalties per test in event.

(i.e. 8 tests = 10 or 16 tests = 20).

18. CREWS.

18.1. All competitors must be in possession of a current licence which must be produced at sign-on.



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18.2. All competitors must have completed and signed an entry form, which is validated at sign-on. Competitors under 18 years of age must have the entry form countersigned by a Parent or Guardian.

18.3. Competitors holding a Clubman National B licence are eligible to compete in all classes.

18.4. Competitors holding a Clubman Junior Licence or a One Day Licence and is under 17 years of age are only eligible to compete in Suzuki 4 wheel drive vehicles as laid down in Article 11.1.

18.5. Any competitor holding a Clubman Junior Licence or a One Day Licence who is under 17 years of age must be accompanied at all times by a competitor of 18 years of age or over with a current Clubman National B Licence.

18.6. A maximum of 2 drivers per vehicle is allowed.

18.7. Passengers are not required to hold a competition Licence. Passengers must be 14 years of age or over and register and have signed an indemnity at sign-on. The organisers of each individual event have the discretion to waive the 72 hour entry requirement in the case of passengers only.

18.8. Only one passenger may be carried and must be using safety equipment in accordance with Article 7.

18.9. Crews must remain within passenger compartment of the vehicle. Neither member of the crew may leave the vehicle while it is on the Observed Section. See Article 17.6 for penalty.

18.10. Competitors may only drive or be a passenger in the vehicle in which they are entered. See Article 17.6. for penalty.

19. SCRUTINY.

19.1. Scrutiny will take place in a designated place and all competitors must proceed to that place and await their turn. Competitors are obliged to present their vehicles for scrutiny at least one hour before the due start time.

19.2. If any vehicle rolls onto its roof the driver must immediately present the vehicle to Scrutineer for inspection before proceeding to next test. Penalty for failure to do so is automatic exclusion.

20. BRIEFING.

20.1. Drivers briefing will take place at least 15 minutes before the due Start Time of each event.

20.2. The Championship Registrar (or his nominee) may address the drivers at every driver's briefing.

20.3. The organisers of any event may limit the number of competitors and entries will be accepted on a "first come first served" basis. Entry fee will be €50.00 (inclusive of Personal Accident Insurance) for all those who enter at least 3 days before Official Closing Date and will be €60.00 (inclusive of Personal Accident Insurance) thereafter. For competitors from other ASN's the entry fee is €30.00 (inclusive of Personal Accident Insurance).

20.4. No entries may be taken on the day.

20.5. All clubs reserve the right to refuse an entry without assigning a reason.

Registrar: The registrar of the Championship is
Mr. Stephen Geary
10 Knockaun Court
Clonmel, Co. Tipperary
Phone: 086-8273718 before 10.30 p.m.
Email: sngeary@gofree.indigo.ie
Website : www.ckmc.ie



HEWISON TROPHY

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HEWISON TROPHY NATIONAL AUTOTEST CHAMPIONSHIP REGULATIONS 2011/2012

1. The Championship shall be decided on the best performance in a series of test trials run during the winter season, between 31st August and the following 31st March. The number of events to count in each driver's final score will be as follows:

Best 12 out of 15 or 16 events

Best 11 out of 14

Best 10 out of 13

Best 9 out of 12 or fewer events

In the Novice Class, each driver's best 10 scores will count out of 13 or more events, with 9 scores counting if there are 12 or fewer events.

2. The overall winner of the Championship will receive the Hewison Memorial Trophy (to be held for one year) and replica. Awards will also be made to the 2nd and 3rd drivers overall and to the 1st, 2nd and 3rd drivers in each class, provided there are sufficient regular entries in each. Winners of overall awards will not be eligible for a class award.

3. The winner of the Novice class will receive the Europa Cars Trophy (to be held for one year) and replica. Awards will also be made to the 2nd and 3rd drivers in this class.

4. The Championship will be open to all holders of an MI Clubman National B licence and to holders of RAC MSA driver's licence of a grade not lower than National B.

5. Classes will be as follows:

5.1. All Mini saloons.

5.2. Front wheel drive Specials.

5.3. Rear wheel drive Sports Cars and Specials

5.4. All saloons, other than Minis, up to 1300 cc

5.5. All saloons, other than Minis, over 1300 cc

5.6. Novices (in the championship only).

5.7. In each qualifying event, organisers must provide for classes A-E, although they may include extra classes at their discretion. To encourage novices in all types of car, THERE MUST BE an award to the highest placed novice in each of classes A-E in each event. If not all classes have novice entries, the novice awards originally intended for those classes which do not

have any entries should be re-allocated to classes with the greatest number of novice entries. In each qualifying event, THERE MUST BE ONE award to the highest placed Semi-expert (overall).

5.8. Before the start of each season, the Autotest Committee will allocate permanent competition numbers to each driver, based on the overall finishing order in the previous season's Hewison Championship. Clubs must use these numbers in each event.

6. A Novice is defined as a driver who has not won more than eight awards in OPEN autotests before the date of the first championship round in each season, except that 2nd or 3rd place awards in a novice class or any Beginners' awards will not count towards this total of eight. 1st place awards in a novice class or any awards in a "car class" or overall placing in any test trial or autotest (whether championship or not) other than a closed event will be counted towards a driver's total. Before the start of each season, and based on the results of the previous season, the Autotest Committee will decide who is to be promoted from the Novice class.

6.1. Any driver who is eligible for the Novice class at the date of the first championship round in each season will remain eligible for the full season.

6.2. Novice drivers will score points in both their car class and the novice class, although it is normal practice to restrict each driver to one award in each event, even where they have finished "in the awards" in both their car class and the novice section.

6.3. A driver will be classified as a Semi-expert for a period of one year from the date of promotion from Novice status. This classification will run from the date of the first championship round each season.

7. QUALIFYING EVENTS.

7.1. Each qualifying event must comply with the following requirements:

7.2. The minimum number of tests which may be run is 10, with a recommendation of 12 or 14. It is recommended that AT LEAST FOUR different test sites be used in each event. Each test site must be properly prepared and manned by marshals prior to the arrival of the first competitor. No test should be of a duration of less than 40 seconds, with a recommended average of 45-50 seconds for the best time. Tests must be of



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the "open" type, designed as far as possible to equate the performance of cars of different size.

7.3. Once the first competitor has attempted a test, NO ALTERATION MAY BE MADE TO THE TEST INSTRUCTIONS. It is recommended that a large diagram of the required manoeuvres be on display at each test site and that competitors be supplied with a set of test diagrams at the check-in. The intended manoeuvres during each test must be drawn in on these diagrams. ORGANISERS MUST NOT ISSUE TEST INSTRUCTIONS IN WORDING FORM ONLY.

7.4. Pylons must be of a minimum height of 3 feet. NO LARGE MOTORWAY-TYPE CONES WITH LARGE BASES MAY BE USED.

7.5. All spots where pylons are placed should be marked on the ground, so that marshals know exactly where to replace them when they are moved. All lines used in a test should be AT LEAST 3.5 METRES IN LENGTH and all garages should be AT LEAST 5.5 METRES LONG AND 3.5 METRES WIDE.

7.6. In the interest of safety, no test may be started and finished on the same line. Two lines side-by side may be used.

8. TIMING AND PENALTIES.

8.1. Tests will be timed in SECONDS and TENTHS OF SECONDS (note that there may not be any rounding up or down). Where watches reading in hundredths of a second are used, the last digit should be IGNORED. Penalties in seconds will be added to these times and the competitor with the lowest aggregate in each event will be the winner of that event, with other competitors being placed in order of their total times. Ties will be decided by the better time (including penalties, if any) on test 1, failing that, test 2, and so on until the tie is resolved.

8.2. Each test will have a flying start, with two lines laid out approx 12" apart. Timing will begin when the car's front wheels cross the second line.

8.3. All cars must stop astride the finish line, and timing will stop WHEN THE FRONT WHEELS CROSS THE LINE. Stopping with either rear wheel past the finish line will incur a line fault.

8.4. During each test, all lines on which a car is required to stop must be crossed by a car's LEADING WHEELS ONLY - i.e. ASTRIDE ALL LINES. A line will be deemed to have been crossed when all the area of the leading tyres in contact with the ground has passed the full width of the line. Putting three or four wheels across a line will incur a line fault, as will putting only one wheel across a line.

8.5. A penalty of 5 seconds shall be incurred for a line fault or for each initial contact with a pylon WHICH IS IN THE CORRECT POSITION. (Where a pylon is touched by a car, is then replaced in its correct position and is again touched by the car later in the test, the driver should be penalised a second time, but if the pylon is out of position when touched a second time, only the first penalty can be applied).

8.6. If a driver has a re-run on a test because of a timing failure, any penalties already incurred will NOT carry forward to the re-run. If he has a re-run for any other reason, any penalty incurred during the first run shall be carried forward to the re-run.

8.7. Any driver who receives any outside physical assistance during a test shall be deemed to have incurred a fail on that test.

8.8. Maximum penalties.

8.8.1. The maximum penalty on any test, including failure, shall be the best correct performance in class on that test plus 20 seconds. To ensure a consistent interpretation of a fail on a test, the following will apply: Where a driver has made a mistake in a test, he is allowed correct himself by going back to the point at which the mistake occurred and then completing the test correctly as required by the diagram. Once he completes the entire test correctly, he will not incur a fail for adding extra manoeuvres, even if these include the crossing of one or more lines in the wrong direction, or in the wrong order.

8.8.2. If a driver who fails a test is the only competitor in his class attempting that test, he will be required to attempt the test again in order to establish a base time for calculating his fail time, which will be this base time plus 20 seconds. If a driver who is the only competitor in his class needs to have a fail/maximum time calculated under section (j) below, his immediate previous time on that test will be used as the base time.

8.8.3. For the purposes of calculating fail (maximum) times for novice drivers, they will be treated as being in a separate class from experts driving similar cars. Therefore, for example, if a novice driving a car in class A incurs a fail or maximum time on a test, his fail time will be based on the best time on that test by another novice in class A, and NOT on the best time by an expert in class A.

8.9. The only judges of fact for a particular test will be the marshals appointed for that test.

8.10. Notification of any penalties incurred by a driver on a test must be recorded on his time card immediately he finishes the test, and may not be added later.



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8.11. To be classified as a finisher a driver must complete not less than 50% of the total number of tests. He will be given a maximum penalty for any tests not attempted. In accordance with the MI General Competition Rules, once a driver has started an event in a car, he may not change to another car.

8.12. In all events, drivers will be issued with A SEPARATE time card for each lap, on which timekeepers will record his time and penalties (if any) for each test. All penalties must be entered on this card as soon as the driver has finished each test AND NO FURTHER PENALTIES MAY BE ADDED ONCE THE CARD HAS BEEN HANDED BACK BY THE TIMEKEEPER TO THE DRIVER. TO SPEED UP THE CALCULATION OF RESULTS, EACH DRIVER MUST EXCHANGE HIS CARD FOR A NEW CARD AT THE END OF EACH LAP. Results must be calculated from these time cards.

9. POINTS SYSTEM.

9.1. Twenty five points will be awarded to the overall winner of each event, with one point less going to drivers in each successive place. In addition, classes A-E will be marked separately with the class winner receiving ten class points, with one point less going to drivers in each successive place. Points awarded in one class cannot be transferred to any other class.

9.2. Points *for Novices and Semi-Experts* will be scored as for all other classes, i.e., 10 points for the class winner, with one point less going to drivers in each successive place, irrespective of the number of starters.

In the event of a tie for any place, the winner shall be the driver with the greater number of wins during the series and if a tie still persists, the greater number of second places, and so on, until the tie is resolved.

10. TEAM EVENTS.

Where selection of a team to represent the Republic of Ireland in team events is necessary, this will be done on the following basis: for the Ken Wharton event, overall points (from best 12 events) in the previous season's Hewison Trophy Championship; for other team events, overall points (from best 12 out of 16 if more than 12 events have been run) at a date two weeks before the date of the team event in question.

11. RECOMMENDATIONS TO CLUBS.

11.1. In the interests of encouraging newcomers, it is strongly recommended that a 33.3% reduction in entry fees be granted to drivers in the novice classes.

11.2. An award should be given for 1st overall in each event, as well as the usual awards in each class. To encourage novices in all types of car, THERE MUST BE an award to the highest placed novice in each of classes A-E in each event.

11.3. Clubs are advised to start all events not later than 12 noon during the period covered by summer time and not later than 11.00 am during winter time. Also, a group of drivers (preferably from the same class) should be started at each test site in ALL EVENTS, to avoid one long queue at the first test site.

12. All forms of electronic traction control are forbidden.

12.1. Tyres which are listed on List 1B (Sports / Uprated tyres suitable for Competition) of the MSA Yearbook MAY NOT BE USED ON THE DRIVEN WHEELS.

13. QUALIFYING EVENTS 2010/2011.

Total 15 events.

Best 12 to count.

Best 10 to count for Novices.

Saturday, 3 September	Cork
Sunday, 4 September	Monster
Saturday, 29 October	Connaught (Sligo)
Sunday, 30 October	Galway
Saturday, 19 November	TDC
Saturday, 26 November	Carlow
Sunday, 27 November	Carlow

2011

Sunday, 8 January	Midland
Saturday, 21 January	Carrick-on-Suir (Waterford)
Sunday, 22 January	Carrick-on-Suir (Waterford)
Sunday, 19 February	Monaghan
Sunday, 26 February	MEC
Saturday, 10 March	Kerry
Sunday, 11 March	Kerry
Sunday, 25 March	Laois

2012

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14. SAFETY (SEE APPENDIX 2) CARS.

14.1. Must be presentable and well turned out

14.2. MUST BE FITTED WITH ADEQUATE SILENCING. Noise checks may be held at each event and drivers are requested to comply with Autotest Committee guidelines, to avoid the risk of losing venues.

14.3. It is **STRONGLY RECOMMENDED** that all cars be fitted with an operable fire extinguisher.

14.4. Roll over bars are now compulsory on all open cars.



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14.5. Lap belts must be fitted to all cars and must be worn during all tests.

15. GUIDELINES FOR ISSUE OF ONE EVENT LICENCES FOR AUTOTESTS.

15.1. Tests to be kept simple, to avoid confusing and discouraging newcomers who are the most likely users of one event licences.

15.2. Each test site to be taped off, so that spectators are kept a reasonable distance from the nearest position which a car may reach during any test.

15.3. A briefing to be held before the start for all one event licence users, explaining to them the basics of autotesting, including "dos and don'ts".

15.4. A handout should be issued containing the points mentioned in item 3 above.

15.5. Both during and after the event, expert drivers should be encouraged to suggest to newcomers where they may improve their performance, and to point out anything which they are doing wrongly which might lead to problems if continued with.

16. ORGANISERS.

Great care must be taken that all spectators are kept a reasonable distance from the nearest position which a car may reach during any test. It is strongly recommended that a suitable fire extinguisher be located at each test site during each event.

Tests MAY NOT finish with a manoeuvre which involves a change of direction from reverse to forwards immediately before the finish line. The distance between circling a pylon and reversing over a line should not be less than 25 metres.

Enquiries to:

Paul Phelan,
56 Kimmage Road West, Dublin 12
Mobile: 086 - 251 4193
e-mail: kbridges@esatclear.ie



NON-HEWISON AUTOTEST

APPENDIX 91

REGULATIONS FOR NON-HEWISON CHAMPIONSHIP AUTOTESTS, MULTI VENUE AUTOTESTS AND AUTOSOLO

1. NON-HEWISON AUTOTESTS.

It is recommended that classes A, B, C, D and E are run as in Hewison Championship events, but organisers may, if they wish, choose different classes.

2. It is recommended that novice and beginners, awards be presented, as appropriate, to encourage newcomers.

3. A Novice is defined as a driver who has not won more than six awards in OPEN autotests before the date of the first championship round in each season, except that 2nd or 3rd place awards in a novice class will not count towards this total of six. 1st place awards in a novice class or any awards in a "car class" or overall placing in any autotest (whether championship or not) other than a closed event will be counted towards a driver's total. Any driver who is eligible for the Novice class at the date of the first round of a championship will remain eligible for all the events in that championship.

4. It is recommended that AT LEAST FOUR different test sites be used in each event. Each test site must be properly prepared and manned by marshals prior to the arrival of the first competitor. Tests must be of the "open" type, designed as far as possible to equate the performance of cars of different size. Once the first competitor has attempted a test, NO ALTERATION MAY BE MADE TO THE TEST INSTRUCTIONS. ORGANISERS MUST NOT ISSUE TEST INSTRUCTIONS IN WORDING FORM ONLY. PYLONS MUST BE OF A MINIMUM HEIGHT OF 3 FEET. NO LARGE MOTORWAY-TYPE CONES WITH LARGE BASES MAY BE USED. All spots where pylons are placed should be marked on the ground, so that marshals know exactly where to replace them when they are moved. All lines used in a test should be AT LEAST 10 FEET IN LENGTH and all garages should be AT LEAST 15 FEET LONG AND 10 FEET WIDE. In the interest of safety, no test may be started and finished on the same line. Two lines side-by side may be used.

5. TIMING AND PENALTIES.

Article 8 of Appendix 90 (Hewison Championship Regulations) applies to all events.

6. SAFETY.

The Safety requirements listed in Appendix 90 apply to all events.

7. ONE EVENT LICENCES.

The guidelines listed in Appendix 90 apply to all events.

8. RECOMMENDATIONS TO CLUBS.

In the interests of encouraging newcomers, it is strongly recommended that a 33.3% reduction in entry fees be granted to drivers who are novices or beginners. An award should be given for 1st overall in each event, as well as the usual awards in each class. To encourage newcomers in all types of car, THERE SHOULD BE an award to the highest placed novice and/or beginner in each of classes A-E in every event. Clubs are advised to start all events not later than 12 noon during the period covered by summer time and not later than 11.00 am during winter time. Also, a group of drivers (preferably from the same class) should be started at each test site in ALL EVENTS, to avoid one long queue at the first test site.

9. MULTI VENUE AUTOTESTS.

9.1. Description: A Multi Venue Autotest is an autotest, in compliance with GCRs 25.6, 25.7 and 25.8, which includes a lesser amount of reversing than is required in "normal" traditional type autotests. All tests must take place on private property, but different locations may be used which may involve the use of public roads between test sites. All cars must be "road legal", whether or not public roads are used between test sites.

9.2. Competitors will not be required to travel in a forward or in reverse direction for a distance of more than 200 metres on each occasion in any one driving test (GCR 25.8). At the end of each manoeuvre, i.e., at a maximum distance of 200 metres from the previous manoeuvre, drivers must be required to do any one of the following:

1. Change direction from forward to reverse, or vice versa.
2. Carry out a complete circle of a pylon.
3. Turn a corner which is sharper than a right angle (90 degrees).



NON-HEWISON AUTOTEST

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4. Stop astride a line and restart in the opposite direction (e.g., forwards on to a line and reverse off it, or vice versa).

5. Stop in a marshalled stop box and restart.

9.3. The carrying of a passenger is at the driver's option during tests except that an organising club may require that passengers be carried during a test where the start and finish lines are not adjacent to each other.

9.4. Every passenger carried in a competing car at any time during a Multi Venue Autotest must sign the indemnity wording on the entry form for that car. In the case of passengers under 18 years of age, this must be countersigned by a parent or guardian. Passengers are not required to hold competition licences. All passengers must pay a Competitors' Personal Accident insurance premium in accordance with Appendix 7.

9.5. The only form of time schedule that may be required is that an organising club may impose a time limit for all cars to arrive at the finish of the event and a time limit for cars to DEPART from the lunch halt (where provided).

9.6. One event licences are NOT available for Multi Venue Autotests.

Recommendations:

1. To be eligible to drive in a MVAT, a driver must have driven in at least two "normal" autotests or have experience as a driver in another branch of motorsport.

2. An experienced regular driver should inspect and review the intended tests for each MVAT to ensure compliance with the regulations/recommendations.

10. AUTOSOLO.

10.1. Description.

An Autosolo is an autotest type event which does not involve any reversing.

10.2. Each test will be defined by a series of numbered cones or pylons of a minimum height of 3 feet, which must carry an arrow indicating the direction of travel. Cones/pylons must be passed in the correct direction, in ascending numerical order. The maximum distance permitted between cones/pylons in a slalom is 20 metres. The course must consist of a series of gates, slaloms, and, at least every 60 metres, turns of a right angle (90 degrees) or sharper.

10.3. Cars.

Autosolos are intended to cater for standard, roadgoing cars. Autotest Specials are not permitted. All cars must be normally aspirated, with a maximum engine size of 2 litres. Any car fitted with any type of torque biasing differential will be subject to a handicap of 10% of its total time. Only standard road tyres may be used (no tyres marked "for competition use" or similar wordings).

10.4. Regulations covering timing, penalties, results, safety are as listed in Appendix 90.

10.5. The carrying of passengers is not allowed.



MIDGET CAR RACING

APPENDIX 92

MIDGET RACING CAR CLUB OF IRELAND

MIDGET CAR RACING REGULATIONS 2012

SPORTING REGULATIONS

1. BACKGROUND.

The Midget Racing Car Club of Ireland was formed in 1936 by Dudley Colley, Count Cyril McCormack, Cyril Murray (Car Rentals), George Reddy and Leo Manthorpe. They were a group of well respected gentlemen in the motor and horse trade and one of their main ideals was to keep the sport inexpensive. They laid down specifications for the building of single seater racing cars, constructed to a very high safety standard and racing under the rules and regulations as laid down by the Royal Irish Automobile Club. Their ideals certainly appealed to the man in the street and they enjoyed many seasons racing in front of large crowds on cinder surfaced tracks in Raheny, Santry, Chapelizod, Portmarnock and Shelbourne Park. With the outbreak of war all of these cars were garaged and the sport interrupted.

With the end of the war the cars were taken back to the racetracks and the sport enjoyed a boom. A son of one of the founder members returned from the USA and brought with him what was known as a Bantam car. This was quickly adopted for Midget Car Racing and with a group of already organised competitors from County Wicklow the basics were laid for the sport as we know it today. In fact the famous Bantam car silhouette is still displayed on club membership cards. Also with the end of the war came rapid industrial development which signified the end of the cinder tracks. The club was now faced with a very serious situation. A decision was taken to race the cars on grass. This was to prove a huge success, for it was not confined to stadiums but could now be held anywhere around the country and this is exactly how the sport survives today.

2. TRACK.

The track shall consist of a closed circuit on level ground and shall be marked by suitable boundaries and a suitable safety fence. The surface on which the racing takes place shall not be less than 30 feet wide at any point and may be banked to a max. incline of 1:2. The surface shall be of any material that the Council may consider suitable. There shall be clearly marked starting and finishing points. Both sides of the track must be at least 60 feet from any solid object.

3. RACES.

3.1. All races and heats of races shall be run over one or more laps of the track. They shall be so arranged that the first car to cross the finishing line, except in the case of the proper lodging of a protest or misconduct, shall be the winner of such races or heats.

3.2. Races shall be either Standing Start Races or Flying Start Races, as laid down in the supplementary regulations for such events. Races shall be controlled by a system of flags.

3.3. The Handicapper in consultation with the Clerk of the Course may change some drivers in the heats, in the event of unequal numbers in each heat.

3.4. Cars to be changed by drawing numbers of Relevant Cards to get the amount needed.

4. CLOTHING.

4.1. All competitors must wear fire proof suits and underwear to MI specification. The minimum standard is two layers and no nylon clothing is to be worn. Helmets must also be to MI specification as listed in the appendices.

4.2. The specs as laid by M.R.C.C.I. is to be used for Midget Car Racing only, but in the event of Special Exhibition for any activity outside the Club, permission to do so will be at the discretion of the Committee.

5. ENTRIES.

A driver and double may only drive the car they have entered to race in. The driver only to go in the class race (eg. 850cc – 1000cc – 1300cc) a double driver may not go in the class race.

6. POINTS.

6.1. Points are awarded on the day as follows:

- 1st – 7 points
- 2nd – 5 points
- 3rd – 4 points
- 4th – 3 points
- 5th – 2 points
- 6th – 1 point

6.2. With the exception, if a driver wins a heat for the Schwarzopf and the same driver wins the final on the same day, he is awarded 8 points, for the final.

6.3. Where only one car is ready for starter's orders in any race, the driver is automatically awarded half points.

6.4. Two cars ready for starter's orders will make a race and are awarded FULL points. Gellar points are 1st – 7 points to 1 point for 10th place.

6.5. Points scored in Junior or Ladies Championship not to be counted in any other Championship.



MIDGET CAR RACING

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7. SCRUTINY.

A driver must attend scrutiny with his car and other necessary equipment.

8. FINES.

Late arrivals and not attending to the fence may incur a €60 fine on the day.

9. PAYMENT AT GATE.

A driver and one helper or a driver and their spouse will be admitted free. Officials on the day will also be admitted free. Everyone else is obliged to pay at the gate.

10. NON-MEMBERS.

Non-Members may race in an OPEN Midget Car Race Meeting, but they must pay a dearer entry fee and may gain no points in the M.R.C.C.I. Championships.

11. REGISTRATION.

Existing drivers must register their intention to race before January 31st of each year.

12. EQUIPMENT.

With regard to stickers, badges, labels etc., on helmets and overalls. It is solely the responsibility of the driver to ensure that they remain intact, and if they are not legible, they may not pass scrutiny. So as not to bring the sport into disrepute, cars, overalls, helmets etc. must always be clean and in good order at the start of a meeting.

13. INTERMEDIATE JUNIOR CHAMPIONSHIP.

Junior Championship to be run over nine rounds, each of six laps, all rounds to count. A Junior may in the event of the car entered in breaking down before Junior race be allowed to drive another car in Junior Race only with the consent of the owner and Clerk of Course.

Intermediate Junior Drivers 16 – 18 years old, with a competition licence may race in open competitions all day and also compete in the Junior Race. Entry Fees €29 plus Insurance, €9 for one race only. €71 plus Insurance for all races.

14. JUNIOR CLASS.

Junior Competitors aged between 14 and 16 years may enter for this class. Such competitors must be in possession of a Junior Licence and accompanied by a parent or guardian at the events. Junior Class to be run over two heats and a final at each event. Junior's may not compete in other races on the day. Junior entries €35 plus Insurance.

15. QUALIFYING.

Only the first four cars to cross the finish line in the Schwarzkopf final. Where two drivers qualify in the one car, only one may compete and no

other car may take the remaining place. Only the first eight cars in the Dublin and the Wicklow qualify for the Gellar final. Numbers on cars may not be changed or altered in any way.

16. JUMP START.

In the event of a jump start, the race will be restarted with the offenders at the back of the grid.

17. ROLL OVERS.

A roll over of a car is an automatic racestop situation – this being when the car is on or off the track. Officials will deal with any such events.

18. BREAKDOWNS.

If a car breaks down during a race, it must be removed from the track as soon as it is safe to do so. If a car is undergoing a verifiable repair and is not ready for the scratch race it is entered in, the driver may change to a different race subject to approval of Clerk of Course.

If a car is undergoing a repair and not ready for early heats, the driver may change car subject to approval of Clerk of Course. The Clerk of the Course may ask another driver to change to these heats to keep the balance of numbers of cars in each heat.

19. RE-RUNS OF A RACE.

In the event of a race being re-run; this being if less than two thirds of the race has been completed when it is stopped, the race must be re-run. The restart is to have the cars in their original positions on the grid. Only the cars that were in the race at the time it was stopped are to go in the re-run and the race is to be re-run over the full amount of laps.

20. CLASSES.

The Council have the power to equalise the number of cars in each class. Existing drivers registered in January have first preference in the class they raced in the previous year. No more than four existing drivers are allowed change class each year. All new drivers are to check with the Committee.

21. LADIES RACE.

Ladies with a competitive licence may double drive and also drive in the Ladies Championship. Ladies may also change the car they race in from the one they entered, this being in the event of the car they entered, not being ready for starter's orders and the fact that they only get one race per meeting.

Ladies must pay full amount as per MI List.

Entry: €29 + €9 Insurance per race.

22. SPONSORSHIP.

Drivers may not accept individual sponsorship, either in goods or monies. All sponsorship for



MIDGET CAR RACING

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cars advertisements equipment etc. must go to the Club. Breach of this rule will warrant expulsion.

23. MEMBERSHIP.

Membership of the Club shall consist of those who have subscribed the fee fixed at the A.G.M. for the current year and who have been proposed and seconded by members and elected by the Council. The Council reserves the right to refuse to accept a proposed member without stating a reason. Honorary Members may be elected by the Council. They shall not have the power to vote at General Meetings of the Club.

24. EXPULSION.

24.1. Any member wilfully infringing or violating the rules of the Club or whose conduct in or out of the Club shall be deemed derogatory or injurious to the interests of the Club, shall be subject to expulsion by a Special General Meeting of the Club, which it shall be the duty of the Council to convene, either at their own discretion or on a requisition signed by a least 25 members of the Club. The opinion of such meeting shall be taken by ballot when, if two-thirds of the members voting shall decide that such member has merited expulsion, he shall cease to be a member of the Club. The Council shall have the power to suspend a member. Members are responsible for the people in their company or that they bring with them.

24.2. There are three technical officers which are appointed to advise on construction of new cars and reconstruction of old cars. The intention being to avoid anyone doing work that won't pass scrutiny. Members are advised to seek their advice a least four times.

They are: John Sheppard Tel: 046 54435
John Molloy Tel: 01-4932482
Fred Cross Tel: 087-2360661

25. QUERIES.

Complaints and queries about racing are to be given to the Clerk of the Course on the day, these to be dealt with at the next Committee Meeting. Complaints in general to be given, in writing, to the Secretary or any Committee Member, these also to be dealt with at the next Committee Meeting.



MIDGET CAR RACING

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MIDGET RACING CAR CLUB OF IRELAND

MIDGET CAR RACING REGULATIONS 2011

TECHNICAL REGULATIONS

30. MIDGET CAR RACING.

The term Midget Car Racing shall mean the competition by racing of motor cars of smaller dimensions than normal motor cars, and constructed to certain maximum and minimum specifications as laid down in these racing rules.

31. POWER UNIT.

31.1. The power unit shall be one internal combustion engine, actuated by one or more pistons, and operating at atmospheric pressure. All engines must be fitted with one single choke carburetor with 1 1/4 (inch) or 32mm, maximum throttle outlet. There shall be two classes of cars. Front engine rear wheel drive, and rear engine rear wheel drive. Rear engine: shall be limited to 850cc Mini components as standard.

31.2. 1000cc – 1200cc Front engine rear wheel drive. The engine shall be placed forward of the driver and shall be connected to the rear driving wheels only by a friction clutch or other device in such a manner that a standing start may be made without outside assistance. The drive shaft must run down the centre line of the car. Chain drive must not be used anywhere in the transmission. Maximum capacity (i.e. swept volume) 1200 cc. Minimum capacity 490cc.

31.3. Rear engine, rear wheel drive, shall be limited to maximum 850cc B.M.C. engine, gearbox and subframe. Engine modifications are permitted similar to front engine cars. In both cases over boring is only permitted up to the manufacturer's recommended oversizes, and must not exceed 60 thou or one m/m in any case. Stroke to remain as standard. All other modifications allowed for 850 class.

31.4. 1000cc rear engine, rear wheel drive shall be limited to 1000cc B.M.C. A series engine gearbox and subframe. Engine to remain standard. In all cases, overboring is only allowed up to manufacturer's recommended sizes. Must not exceed 60 thou or one mm in any case. Stroke to remain as standard.

31.5. Up to 1300cc cars can be built using rear engine, rear wheel drive, only using any manufacturer's engine. But using the present general specifications of MRCCCI. (Excludes the use of BMC A Series engine box subframe.)

32. FUEL.

Only pump fuel to be used.

33. WHEELS.

Four road wheels, tyres shall be pneumatic and shall not have metal studs or other hard projections. All tyres shall be clearly stamped. Rear tyres shall have a maximum size of 165 by 13. Front tyres shall have a maximum size of 165 x 13 M&S. You may use radial or crossply tyres, standard road or track grip or any size less in diameter and or width as approved by the committee. Remoulds are permitted. Tyres cannot be altered. Hub centre shall not project more than 2 inches beyond the outer plane of the tyre. 155 x 12 Fr 1565 x 13 rear recommended.

34. GENERAL DIMENSIONS.

Wheel Base (i.e. distance from centre point of contact of ground of other front or rear wheel)

Maximum 78 inches

Minimum 60 inches

35. TRACK.

(i.e. distance from centre point of contact with the ground of front or rear wheel to centre point of front or rear wheel to centre point of contact of ground of other front or rear wheel.)

Maximum 55 inches

Minimum 44 inches

Overall Length

Maximum 108 inches

36. ENGINE COMPARTMENT.

(i.e. portion of body including radiator, cowl, back to but not including the scuttle.) Maximum height from the ground 36 inches.

Maximum 38 inches

Maximum height from ground 45 inches

Minimum height from ground 30 inches

37. TAIL.

(i.e. the portion of body behind the driver)

Maximum height from ground 45 inches.

Minimum height from ground 30 inches.

38. NOSE.

Minimum height from ground 18 inches and must have a FLAT front at least 6 inches deep.

39. BODY.

(i.e. Metal shell surrounding engine, rear wheel drive, 850cc cars.)



MIDGET CAR RACING

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40. WHEEL BASE.

(i.e. Distance front wheel centre to rear wheel centre.)

Maximum 78 inches

Minimum 60 inches

41. BODY.

(i.e. metal shell surrounding seat, pedals, tanks, etc. back to but not including engine compartment.)

Maximum width 30 inches

Minimum width 36 inches

42. PEDALS.

Must be fitted behind centre line of front axle.

43. GENERAL SPECIFICATIONS.

43.1. Midget cars shall be soundly constructed on accepted engineering principles. They shall be well finished and must satisfy the Technical Officer of the Club and the M.I. Scrutineer that they are safe and suitable for racing. The steering wheel and seta shall be approx. on the centre line of the car. No steering system which relies solely on welding will be allowed. All cars to be fitted with rear mud flaps ground clearance 2 inches max.

43.2. BODY WORK.

The body shall be constructed of sheet metal, shall be finished in such a manner that there are no sharp projections or unprotected edges. It shall have its engine, bonnet and other hinged or detachable parts of its structure provided with such fastening as shall, in the opinion of the Scrutineers that it will not bring the sport into disrepute.

43.3. BRAKES.

Efficient brakes shall be fitted to all four wheels.

43.4. EXHAUST PIPES.

Shall be so constructed that they shall carry the exhaust gases clear of the bodywork. Normally they shall be constructed in such a way that they bring the exhaust gases to the back of the car. The end of such exhaust pipes shall not be cut off square. All exhausts outside the body to be covered with a heat shield.

44. COLOUR.

44.1. Midget cars are to be painted in bright high gloss colour or colours. Bare sheet metal does not constitute a colour. The entrant has the choice of colour, but if too many cars are painted the same, then the Council of the Club shall demand the colours be changed. This will apply to the most recently constructed cars.

44.2. A rubber protection bumper must be fitted to the front and the rear of the cars, it must be the width of the car, 3 inches high and no more than two inches thick. It must be bolted on and painted. It is optional.

44.3. Roll Bars.

Cars shall be equipped with a roll bar to protect the driver in case a car turns over. It shall be formally attached to the chassis structure and designed to withstand compression forces from the weight of the car, as well as fore and aft loads from horizontal forces. It shall be placed behind the driver and must come to a minimum level of 6 inches above the drivers helmet when seated. It must be constructed of tubing of not less than 40mm diameter if 3ml gauge tubing (a small hole must be bored for inspection purposes). In order to withstand fore and aft loads, the roll bar will have some form of inclined strut or diagonal bracing. The main roll hoop must be one continuous piece.

44.4. Safety.

Cut out switch to be fitted on or under the protection of the roll bar. The on/off position must be clearly marked.

44.5. Belts.

A full safety harness must be fitted and properly secured. Helmets must be up to current M.I. specifications. Appendix 2 Art. 17.

A drip tray to be fitted underneath the carburetor this to include a drainage pipe. All cars to have a seal battery securely fitted. A red light must be fitted at least 43cms from ground level. It must be in working order with a separate switch in the cockpit and be fitted with a 21 watt bulb. Where the battery is situated beside the petrol tank, they must be delivered by a metal plate. The battery must be sealed.

45. NUMBER.

The competition number allotted to the car must be painted in unobscured positions on both sides of the body work in such a way that they are visible from the oblique front on either side. They shall be in figures at least 10 inches high and 1.5 inches in width throughout (on a plain circular background preferably) and generally displayed so as to be easily legible under all racing conditions. Numbers at least 8 inches high and 1 inches wide may be displayed on the front of the car. 850cc cars to have red numbers on a white background; 1000cc cars have to have black numbers on white background.

Neither paper or water soluble colours shall be used for competition numbers. The duty of providing the car with the allotted numbers and of doing so in every respect to the satisfaction of the Council shall rest solely with the competitor concerned. The number allotted shall be based on the points gained in the previous season (if the car is still in possession of the winner of such points) and shall be normally carried throughout the season. Newly constructed cars and cars which have changed ownership shall be allotted numbers in rotation.



MIDGET CAR RACING

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46. ALTERATIONS TO A SPECIFICATIONS OF MIDGET CARS.

Any alteration to the spec as laid down in these rules shall be made only at a General Meeting of the Club. Such alterations shall come into force at the start of the following Racing Season or later if the club decides. All members shall be notified immediately of such alterations.



OFFICIALS' LICENCES

APPENDIX 100

REQUIREMENTS FOR THE ISSUE OF OFFICIALS' LICENCES

All Rally; Race; Karting, Hillclimb/Sprint and Rallycross Officials, as detailed in GCR 123, must have a current MI Officials Licence to officiate at MI permitted events.

Applicants for all Official Licence grades must apply by completing the OL1 form.

All Official applicants must be a minimum of 18 years.

TRAINING AND APPROVAL REQUIREMENTS
(Please see page 2 of application form OL1)

1. STEWARDS AND SAFETY OFFICERS (RALLIES).

1.1. Licence for Steward or Safety Officer (Motorsport Ireland appointment).

In order to obtain a Stewards or Safety Officers Licence to act as an MI Official the applicant must attend an MI course for **Rally** Senior Officials every 3 years.

1.2. Licence for Steward or Safety Officer (Club appointment only):

To act as Steward or Safety Officer appointed by the Club the Official must be nominated by his / her Affiliated Club to act as Steward / Safety Officer.

It is preferable that Club appointed Stewards and Safety Officers attend the MI **Rally** Course.

2. STEWARDS AND SAFETY OFFICERS (HILLCLIMB/SPRINT).

2.1. In order to obtain a Stewards or Safety Officers Licence to act as an MI Official the applicant must attend an MI **Race, Rally or Hillclimb / Sprint** course for Senior Officials every 3 years.

2.2. To act as Club Steward or Safety Officer the Official must be nominated by his / her Affiliated Club to act as Steward / Safety Officer.

It is recommended that all Club Stewards and Safety Officers attend **MI Race, Rally or Hillclimb / Sprint** Senior Official Training. Club appointed Stewards and Safety Officers must be nominated by their Club to obtain their licence.

3. STEWARDS (RACING, RALLYCROSS AND KARTING).

It is mandatory for all MI Stewards to attend Senior Official Training for their discipline. Club Stewards must be nominated by their Club to obtain their licence. It is recommended that they attend training. **MI Race or Rally Senior Official Training is also acceptable for Kart Stewards.**

4. CLERK OF THE COURSE (ALL DISCIPLINES).

4.1. First Time: Officials wishing to obtain their first Clerk of the Course licence must attend the MI course for Senior Officials and be nominated by his / her Affiliated Club, for Rally Clerks of the Course he / she must have acted as deputy / assistant clerk of the course once.

4.2. Renewal.

All Clerks of the Course wishing to renew their licence must attend the appropriate training course once every 3 years.

4.3. Rally Senior Officials training is acceptable for Hillclimb CoC's. Race Senior Officials training is acceptable for Hillclimb and Kart CoC's.

5. DEPUTY CLERK OF THE COURSE (ALL DISCIPLINES).

Must be nominated by their Affiliated Club. It is recommended that Deputy Clerks of the Course attend the MI Senior Officials training.

6. STAGE COMMANDERS (RALLY ONLY).

In order to obtain (and maintain) a full Stage Commanders licence you must attend a Stage Commanders Seminar once every 3 years.

First time stage commanders will be issued a deputy licence and must act as deputy / assistant at two events before they will be issued a full stage commanders licence

7. SCRUTINEERS (ALL DISCIPLINES).

All first time Scrutineer applications to be approved by the Technical Committee. It is recommended that all Scrutineers attend a seminar each year. All Scrutineers must attend a minimum of 1 seminar in 3 years.

8. NOTES.

8.1. All MI licenced Officials must agree to abide by the MI regulations and not to do anything that may damage the reputation of motorsport in Ireland.

8.2. Officials should not exercise the privileges of their licence if they are aware of any medical or other reason that renders them unfit to do so.

8.3. MI may take disciplinary action against any Official who does anything to damage the reputation of motorsport in Ireland.

8.4. No individual who is named as an Official for an event may compete in that event.



OFFICIALS' LICENCES

APPENDIX 100

8.5. No individual is allowed hold more than one Official position at an event.

8.6. Licences are valid from the 1st of January to the 31st of December.

8.7. A passport photograph must accompany all applications.

8.8. Licences without passport photograph are not valid.

8.9. Clubs will be advised of dates and venues for all training courses; they will also be advertised on the Motorsport Ireland website. (www.motorsportireland.com).

Sample Licence Application form on following page



OFFICIALS' LICENCES

APPENDIX 100



1.
MOTORSPORT IRELAND (MI)
34, DAWSON STREET,
DUBLIN 2
Tel: 01-677 5628 Fax: 01-671 0793

DATE
RECEIVED

OL1

APPLICATION FOR MI OFFICIALS' LICENCE (MUST BE COMPLETED IN FULL)

FIRST TIME ☐ RENEWAL ☐ LICENCE NO.

SURNAME

FIRST NAME

ADDRESS

TOWN

COUNTY

TELEPHONE - HOME

MOBILE PHONE

EMAIL

☐ Please tick if you do not wish to be added to MI mailing list.

DATE OF BIRTH MOTOR CLUB (If applicable)

Please ensure that you read page 2 for all training requirements.

Tick ☒ box/s for licence/s you require.

RALLY OFFICIAL

- ☐ 1. Steward ☐ Club
☐ 2. Clerk of the Course
☐ 3. Deputy Clerk of the Course
☐ 3. Safety Officer ☐ Club
☐ 4. Stage Commander
☐ 6. Deputy Stage Commander

RACE / RALLYCROSS OFFICIAL

- ☐ 1. Steward ☐ Club
☐ 2. Clerk of the Course
☐ 3. Deputy Clerk of the Course

KART OFFICIAL

- ☐ 1. Steward ☐ Club
☐ 2. Clerk of the Course
☐ 3. Deputy Clerk of the Course

HILLCLIMB / SPRINT OFFICIAL

- ☐ 1. Steward ☐ Club
☐ 2. Clerk of the Course
☐ 3. Deputy Clerk of the Course
☐ 4. Safety Officer ☐ Club

SCRUTINEER

- ☐ 1. Open
☐ 2. Rally
☐ 3. Race
☐ 4. Kart
☐ 5. Other – (Please specify)

**FIRST TIME APPLICANTS
AND THOSE WHO HAVE NOT
RENEWED THEIR LICENCE
SINCE 2006 MUST INCLUDE
A PASSPORT PHOTOGRAPH**

PLEASE LIST ALL SEMINARS ATTENDED IN LAST 3 YEARS:

RALLY UPGRADES:
LIST EVENTS FOR UPGRADE

DECLARATION:

- I hereby apply for Registration on the Officials' Register of MI and I undertake, if registered to submit to and be bound by the International Sporting Code of the FIA, the General Competition Rules of MI, the MI Code of Conduct for children's sport, Appendix 100 of the current MI yearbook and any regulations supplementary thereto as may be imposed from time to time by MI.
- I can confirm that I have complied with the maintenance criteria set out within the MI rules and regulations to enable me to apply for my licence.
- I will not exercise the privileges of this licence if I am aware of any medical or other reason that renders me unfit to do so.

Signed:

Date:

100



OFFICIALS' LICENCES

APPENDIX 100

2.

OFFICIAL TRAINING AND APPROVAL REQUIREMENTS

RALLY OFFICIALS

Official Grade	Training Mandatory	Training Recommended	Event Experience (see note 8 & 9)	Club Nomination
Steward – MI appointed	✓			
Steward – Club appointed only		✓		✓
Safety Officer – MI appointed	✓			
Safety Officer – Club appointed only		✓		✓
Deputy Stage Commander		✓		
Stage Commander – First Time	✓		✓	
Stage Commander – Renewal	✓			
Clerk of the Course – First Time	✓		✓	✓
Clerk of the Course – Renewal	✓			
Deputy Clerk of Course		✓		✓

RACE / RALLYCROSS OFFICIALS

Official Grade	Training Mandatory	Training Recommended	Club Nomination
Steward – MI	✓		
Steward - Club Appointed		✓	✓
Clerk of the Course – First Time	✓		✓
Clerk of the Course – Renewal	✓		
Deputy Clerk of the Course		✓	✓

KART OFFICIALS

Official Grade	Training Mandatory	Training Recommended	Club Nomination
Steward – MI	✓		
Steward – Club appointed		✓	✓
Clerk of the Course – First Time	✓		✓
Clerk of the Course – Renewal	✓		
Deputy Clerk of the Course		✓	✓

HILLCLIMB OFFICIALS

Official Grade	Training Mandatory	Training Recommended	Club Nomination
Steward – MI	✓		
Steward - Club Appointed		✓	✓
Safety Officer – MI appointed	✓		
Safety Officer – Club appointed		✓	✓
Clerk of the Course – First Time	✓		✓
Clerk of the Course – Renewal	✓		
Deputy Clerk of the Course		✓	✓

SCRutineers

	Seminar Attendance Mandatory	MI Technical Committee Approval
First Time	✓	✓
Renewal	✓	Upgrades

Notes:

- All MI licenced Officials must agree to abide by the MI regulations, in particular, Chapter 9 of the GCRs and Appendix 100 and must not do anything that may damage the reputation of motor-sport in Ireland.
- MI may take disciplinary action against any Official who does anything to damage the reputation of motorsport in Ireland.
- No individual who is named as an Official for an event may compete in that event.
- No individual is allowed hold more than one Official position at an event.
- For trained Officials licences can be issued for up to a 3-year period, 1-year licences will be issued for all other Officials.
- First time applicants and those who have not renewed their licence since 2006 must include a passport photograph.
- All Officials for which training is mandatory must attend refresher training every 3 years.
- Stage Commander Rally Upgrade – Event Experience: Two events as deputy or one event as Stage Commander within a 5-year period prior to application for upgrade.
- Clerk of the Course Rally Upgrade – Event Experience: One event as assistant / deputy or one event as Clerk of the Course within a 5-year period prior to application for upgrade.



COMMITTEE STRUCTURES

APPENDIX 101

COMMITTEE STRUCTURES

1. The Chairman and Safety Representative for each Committee will be nominated by the Motor Sport Commission (MSC).

2. The MSC will vary these nominations.

3. The remaining MSC nominations will not necessarily be members of the MSC.

4. In the interests of improved continuity in carrying out the work of the committee, nominations from eligible clubs in each discipline will be sought on a rolling basis. The accompanying tables for the committees concerned indicate how the system will work.

5. Clubs whose turn it is to make a nomination for a particular discipline must be active at the time of nomination in running an event in the discipline concerned.

6. Clubs should liaise with those other clubs that are entitled to make nominations for the same discipline at the same time.

7. The person being nominated need not necessarily be a member of the club but preferably be the "best person for the job". In addition, the person should

confirm that they are willing to serve on the committee in question, to work for the overall benefit of the discipline/sport and be able to attend meetings as required per the meeting date schedule which is issued with the nomination paper.

8. Vacancies occasioned by the ineligibility or failure of a club to nominate will be filled by the Motorsport Commission. In the case of sub-committees, these vacancies will be filled by the main committee e.g. Rallies.

9. A member of a committee nominated by a club will serve a 2 year term. A person appointed as Committee Chairman may serve 4 years in that position, even though they may have been on relevant Committee.

10. No nominated person may serve on the same committee for a continuous period in excess of 4 years, with the exception of Advisory Group nominees.

11. The Competitions Advisory Committee may visit different venues from time to time.

12. Terms of Reference for the committees may be found in Appendix 102.

RALLIES SUB-COMMITTEES

Year	National Stages	National Forestry	National Navigation
2012	Connacht Monaghan	Limerick/MSC Carrick-on-Suir	Cork Donegal
2013	Monaghan Mayo	Carrick-on-Suir Donegal	Donegal Midland
2014	Mayo Carlow	Donegal Tipperary	Midland TDC
2015	Carlow Carrick-on-Suir	Tipperary Birr	TDC Cavan
2016	Carrick-on-Suir Cavan	Birr Cork	Cavan Skibbereen
2017	Cavan Galway	Cork Munster	Skibbereen Monaghan
2018	Galway Skibbereen	Munster Limerick	Monaghan IMRC
2019	Skibbereen Tipperary	Limerick Carrick-on-Suir	IMRC Cork
2020	Tipperary Birr	Carrick-on-Suir Donegal	Cork Donegal

Rallies Sub Committees

5 Members

3 Rallies Committee Nominations (Chairman, championship registrar, plus one)

2 Club Nominations



COMMITTEE STRUCTURES

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Rallies Committee

12 Members
5 MSC Nominations
6 Club Nominations
1 TROA

2012	Wexford Skibereen Tipperary	2013
	ALMC Cavan Mayo	
2014	Carlow Kerry Carrick-on-Suir	2015
	North-East GSMC Connacht	
2016	Birr Clare Killarney	2017
	Cork Imokilly Laois	
2018	MEC Donegal Castle	
	Galway Limerick Munster	

Trials Committee (incl. Sporting & 4x4 Trials)

6 Members
1 MSC Nomination
4 Club Nominations – 2 from each discipline.

	Sporting	4x4
2012	GSMC, Leinster	Limerick, Laois
2013	Leinster, MEC	Laois, Munster
2014	MEC, GSMC	Munster, Tipperary
2015	GSMC, Leinster	Tipperary, Cork
2016	Leinster, MEC	Cork, Limerick
2017	MEC, GSMC	Limerick, Laois

Autocross Committee

6 Members
2 MSC Nomination
4 Club Nominations

2012 - Mayo, Carlow, Skibereen, Clare
2013 - Skibereen, Clare, Tipperary, Munster
2014 - Tipperary, Munster, Carrick, Birr
2015 - Carrick, Birr, Castle, North-East
2016 - Castle, North-East, Cork, Cavan
2017 - Cork, Cavan, Connacht, Imokilly
2018 - Connacht, Imokilly, Monaghan, Donegal
2019 - Monaghan, Donegal, Midland, Kilarney

Autotest Committee

6 Members
2 MSC Nomination
4 Club Nominations

2012 - Cork, Connacht, Galway, Laois
2013 - Galway, Laois, Munster, Skibereen
2014 - Munster, Skibereen, Wexford, Clare
2015 - Wexford, Clare, Imokilly, Carlow
2016 - Imokilly, Carlow, Midland, Carrick
2017 - Midland, Carrick, Monaghan, MEC
2018 - Monaghan, MEC, TDC, Kerry

Hillclimb & Sprint Committee

6 Members
3 MSC Nomination
4 Club Nominations

2012 - Galway, MEC, Carrick, Donegal
2013 - Carrick, Donegal, Tipperary, Castle
2014 - Tipperary, Castle, Imokilly, Carlow
2015 - Imokilly, Carlow, Limerick, Clare
2016 - Limerick, Clare, Wexford, Galway

Race Committee

10 Members
5 MSC Nominations
5 Club Nominations

IMRC
Kildare
Leinster
MEC
Carlow



COMMITTEE STRUCTURES

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OVERALL COMMITTEE STRUCTURE

The membership of the MSC shall not exceed twenty and shall comprise the following:

President / Vice President

The President and Vice President of the MSC shall be elected by the MSC members from its own membership in accordance with the rules published annually in the Motorsport Ireland Yearbook.

Members

- (a) Three persons nominated from time to time by the IAC.
- (b) The immediate past President of the MSC.
- (c) The Secretary of IMF
- (d) Regional Representatives which are for the time being: Eastern, Northern, Southern.
- (e) The Chairman of the Karting, Race, and Rally Specialist Committees (SC).
- (f) The Representative of each of the Clubman and Speed Groupings which are for the time being; (i) Autotest, Midget Car Racing, Trials and (ii) Autocross, Hillclimb/Sprint, Rallycross respectively. The Representative for each Grouping to be agreed by the Chairmen of the Specialist Committees in each Grouping.
- (g) The Convener (or their representative) of each of IMF's Advisory Groups (AG), which are for the time being Safety, Technical & Rules.
- (h) Such other person(s) as the Board of Directors of IMF from time to time shall nominate.

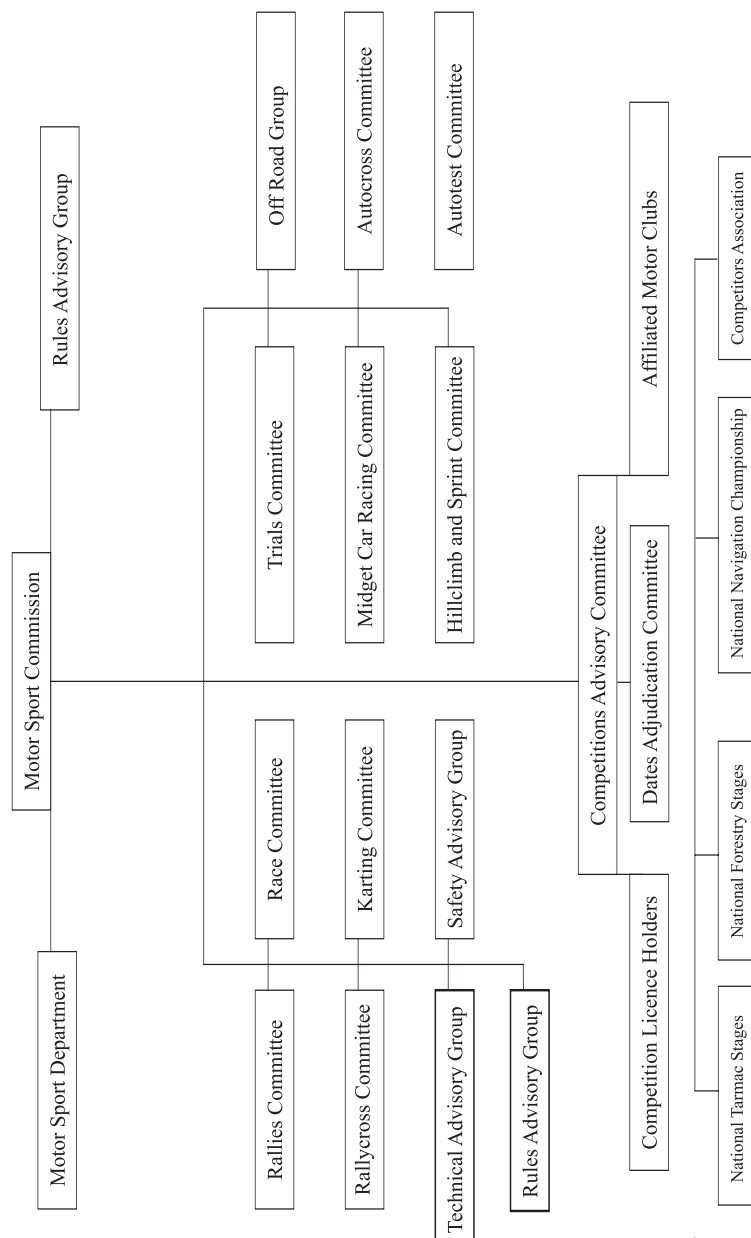
Rallies Committee (including Rallysprints) 12 members 5 MSC Nominations 6 Club Nominations 1 TROA	Race Committee TBA	Trials Committee (including Sporting & 4x4 Trials) 6 members 2 MSC Nomination 2 Nominations from each discipline	Autocross Committee 6 members 2 MSC Nominations 4 Club Nominations
Autotest Committee 6 members 2 MSC Nominations 4 Club Nominations	Karting Committee TBA	Midget Car Racing Committee 6 members 3 MSC Nominations 3 Club Nominations	Rallycross Committee 6 members 4 MSC Nominations includes 1 Circuit 2 Club Nominations (Carlow and CKMC)
Technical Advisory Group 6 members 6 MSC Nominations Representative of a range of disciplines	Safety Advisory Group Up to 14 members all MSC Nominations	Off Road Group 4 members Chairperson of Trials, Autocross, Autotest & Midget Car Racing Committee Meet twice per year - common interest matters	Hillclimb & Sprint Committee TBA
	Rules Advisory Group 5 MSC Nominations	Championship Stewards 3 MSC Nominations	



COMMITTEE STRUCTURES

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ORGANISATIONAL STRUCTURE





TERMS OF REFERENCE

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TERMS OF REFERENCE FOR COMMITTEES AND ADVISORY GROUPS

1. ALL COMMITTEES AND ADVISORY GROUPS.

- 1.1. Associated committees are responsible for
- Approving calendars for the events under their control.
 - Arranging for the organisation of national championships and the approval of permits for same.
 - Drafting the regulations for inclusion in the Yearbook.
 - Nominating Stewards and Safety Officers (as appropriate) for events/championships from panel of licensed officials.
 - When appointing Stewards and Safety Officers the committee chairman should not normally be appointed for events in his/her own discipline. Appointments of chairmen from other disciplines is permitted.
 - Arranging proper end of season championship awards presentations.
 - Addressing the future of the sport.
 - Dealing with business that does not require MSC input.
 - Reviewing Stewards' reports as a source of information on standards of organisation and on-event problems.
 - Ensuring the receipt of all Stewards' reports within a defined period set by committee.
 - Submitting an annual report on the prescribed form to the Motorsport Department, together with a Statement of Account should income be generated.
 - Bring to the attention of the Motorsport Department any matter which may need to be considered.
 - To ratify final championship results as presented by the Championship Registrar.

The President and Chair of the relevant Associated Committee, in consultation with the CEO, will make decisions concerning minor issues and they will be accountable to the MSC for these decisions.

1.2. The minutes of all meetings must be submitted to the Motorsport Department for circulation to the members of the MSC. In addition, corresponding resume minutes must be submitted simultaneously for circulation at the CAC.

1.3. The associated committees are permitted to apply to the MSC for approval to set up working groups provided the reason for setting them up is given.

Such working groups:

- To address specific tasks and within a specific period.

- To have a membership of not more than five persons whose names must be submitted to the MSC for approval.
- To meet independently and not become part of the associated committee.
- To submit all reports in writing to the associated committee for approval.

1.4. Committees and Advisory Groups need not necessarily meet monthly. It may be more effective to have less frequent formal meetings but to make them properly structured and business like. To help this, all meetings should have an agenda.

1.5. To facilitate members who travel long distances all meetings should end no later than 22.30 hrs.

1.6. It is important that committee members keep in close contact with clubs who do not have a nomination in any particular year and vice versa. Wide soundings to find out general views should be part of the committees' normal practice.

2. TECHNICAL ADVISORY GROUP.

2.1. Equipment.

2.1.1. The present equipment list is to be kept updated together with a record of the whereabouts of each item.

2.1.2. A replacement policy must be formulated.

2.1.3. The equipment is to be calibrated as required and the original certificates lodged with the Motorsport Department.

2.2. Eligibility.

Create a clear and concise guide to the procedures to be carried out by scrutineers where components are to be sealed for eligibility inspection.

2.3. Appointments.

2.3.1. The issue of a licence to a scrutineer is the responsibility of the Technical Advisory Group subject to approved training.

2.3.2. The criteria for the Licensing of scrutineers to be set down in writing. It may be appropriate to identify separately safety and eligibility approval.

2.3.3. In support of licensees, training seminars, to cover subjects such as documentation and safety in conjunction with other appropriate committees and the Safety and Training Executive.

2.3.4. Scrutineers for events to be appointed by the Technical Advisory Group on behalf of Motorsport Ireland.

2.4. Communication.

2.4.1. The in-charge scrutineer must make a report to the Technical Committee on each event.



TERMS OF REFERENCE

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2.4.2. The Technical Advisory Group will nominate a specific member to act as the official contact for each major discipline (Race, Rally, Karting, Autocross, Hillclimb/Sprint etc.)

2.4.3. The Technical Advisory Group will advise the Motorsport Department of any proposed changes to National and International technical regulations so that these can be considered by the MSC and published in the Motorsport Bulletin if approved.

2.4.4. A list of appointments to be circulated to all relevant committees.

3. SAFETY ADVISORY GROUP.

3.1. Members of the Safety Advisory Group are appointed by the Motorsport Commission. The membership will normally comprise:

- the convenor
- the safety and training executive
- the member from each discipline charged with safety matters within the discipline
- a representative from marshalling
- a representative from rescue.

3.2. The Safety Advisory Group will meet on a quarterly basis.

3.3. Its key note will be to assess on a continuing basis the general state of safety and to introduce improvements as appropriate.

3.4. It should interpret issues/changes emanating from the FIA or other appropriate safety orientated sources.

3.5. It should monitor safety standards and practices of other ASN's.

3.6. It should consider reports from Safety Officers and Associated Committees of serious breaches of specified rules arising from events and determine whether they should be passed to MI (GCR170(a)).

4. MONITORING BY THE MOTOR SPORT COMMISSION.

4.1. The Motor Sport Commission (MSC) will monitor the operation of the various committees to ensure that they are meeting the objectives for which they were formed.

4.2. Attendance of individual members at committee meetings is to be recorded in the Minutes. Any member who is unable or unwilling to attend meetings on a reasonably regular basis may have his/her appointment to a committee or advisory group reviewed by the MSC.

4.3. All matters pertaining to the organisation and control of Motorsport within the jurisdiction of MI are at all times subject to the approval of the MSC, within the terms of the constitution of Irish Motorsport Federation Ltd.

5. RULES ADVISORY GROUP (RAG).

Remit of RAG.

5.1. All proposed changes to the GCRs must be submitted to the RAG.

5.2. Such proposals may be made only by

- 5.2.1. the MSC;
- 5.2.2. the Specialist Committees;
- 5.2.3. the Motor Sport Department;
- 5.2.4. the RAG itself.

5.3. Alterations to the GCRs when approved by the RAG must be submitted for approval to the MSC.

5.4. The RAG must be made aware of any changes to the Appendices proposed by the relevant Specialist Committees or Advisory Groups.



STANDING ORDERS

APPENDIX 103

STANDING ORDERS ADOPTED BY THE MOTORSPORT COMMISSION

These standing Orders govern meetings of

- The Motorsport Commission
- The Competitions Advisory Committee
- The Associated Committees and other groups authorised by the Motorsport Commission.

2. Of the above bodies, those which meet regularly should do so on the same day each month unless circumstances necessitate a temporary change in this arrangement. The remaining bodies should meet as required but in all cases, unless in the case of an emergency the date for the following meeting should be agreed at the previous one.

3. Only persons properly authorised by the Motorsport Commission may attend meetings governed by these Orders, the Chair being empowered to ensure that those not so entitled leave before business commences.

4. An agenda, supported by any additional documentation considered appropriate, should be provided. Items for inclusion on an Agenda should be in writing and be with the person charged with preparing the agenda at least seventy two hours prior to the meeting.

5. All members of the above bodies will, at meetings, conduct themselves in what is generally acknowledged to be a relevant constructive manner. The Chairman is empowered to deal with any person(s) who may act otherwise. In this respect such powers entitle the Chair to:

- Rule the offender out of order and/or call upon him/her to tender an apology.
- Expel the offender from the meeting, but only if a majority of the entitled members present agree with such action.
- Adjourn / suspend a meeting at which disruptive behaviour by a member or members has occurred.

6. All questions of order and relevance shall be decided by the Chair.

7. The Chair shall regulate the proceedings of the meeting in all matters not provided for in these Orders by analogy to parliamentary practice.

8. Items of an urgent nature may be introduced by means of a motion agreed by the meeting to "Suspend Standing Orders".

9. The quorum necessary for the transaction of the business of Committees is five. The quorum for Advisory Groups is three.

10. If at any meeting the Chairman, President or his appointed deputy is not present within fifteen minutes after the time appointed for holding of same, the members of the Committee may choose one of their number to be Chairman of the Meeting

11. Election of President of the Motorsport Commission (MSC)

11.1. The President shall be elected every year at the January meeting of the MSC.

11.2. The election shall be by secret ballot and the following procedures shall apply:

11.2.1. Voting papers shall be preprinted forms supplied by the Motorsport Department (MSD).

11.2.2. A candidate who receives more than 50% of the vote cast shall be declared elected.

11.2.3. If no candidate receives more than 50% of the votes cast, the candidate with the lowest number of votes shall be eliminated and another secret ballot shall be held, and so on, until one candidate receives more than 50% of the votes cast.

11.2.4. In the event of a tie, another ballot shall be held, after which the President of the Royal Irish Automobile Club (RIAC) will have the casting vote.

11.3. Those entitled to vote shall all be members of the MSC for the year of the election.

11.4. The Returning Officer shall be a member of the MSD, selected by the Motorsport CEO.

11.5. The maximum number of consecutive terms of office a person may serve as President is six. A term is defined as a calendar year, i.e. January to December.

11.6. A sitting President who wishes to retire before the end of their sixth consecutive term should preferably give the Commission at least three months notice.

11.7. When a sitting President is retiring at the end of their sixth consecutive term, prospective candidates for the position, who comply with 11.10, should preferably announce their intentions at least three months before the end of the term and be entitled to make submissions to the MSC, after which an agreed candidate may emerge.

11.8. There shall be no limit to the maximum number of non-consecutive terms a member may serve as President.



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11.9. The outgoing President shall always be eligible for election provided they have not served six consecutive terms and provided they are nominated as in 11.11 and 11.12 below.

11.10. A candidate for the position of President must be a member of the MSC in the year of their election and for at least two in the previous five years prior to the year of their nomination for election.

11.11. A candidate for the position of President must be nominated, that is proposed and seconded, by at least two current members of the MSC for the year of the election.

11.12. Nominations for the post of President must be made on a preprinted form, received from the MSD, signed by the proposer and seconder and lodged with the Motorsport Chief Executive not later than 17.00 hours on the day which is 14 days before the date of the MSC Meeting at which the election is to take place.

11.13. All members of the MSC who are entitled to vote shall receive the names of the candidates together with the names of the proposer and seconder, at least 7 days before the date of the MSC meeting at which the election is to take place.

11.14. Candidates shall be entitled to provide members with written submissions which must be circulated as set out in 11.13 above and shall be entitled to address the MSC for no longer than 5 minutes each before the election, if they so wish.



OFFICIALS EXPENSES

APPENDIX 104

GUIDELINES FOR THE REINBURSEMENT OF OFFICIALS EXPENSES BY EVENT ORGANISERS

The following officials: Stewards, Safety Officers and Scrutineers, who have been appointed by Motorsport Ireland, shall be reimbursed for their expenses by the event organisers.

When appropriate, overnight accommodation is to be provided by the event organisers with B+B the minimum requirement.

What can be claimed for:

Mileage:

Travel will be reimbursed at the rate of €0.20 per Km and will be calculated by reference to the current edition of the AA Members Handbook.

Officials may only claim for mileage incurred at their own expense e.g. if a vehicle has been provided by the organisers for use by the official then this mileage may not be claimed for.

Stewards and Safety Officers may claim for the distance from their home to the nearest town immediately at or beyond the event HQ and where appropriate, in the case of a Stage Rally or Navigation Trial, the total road mileage of the event as specified in the 'Roadbook' or 'Supplementary Regulations'.

Scrutineers may claim for the distance from their home to the nearest town immediately at or beyond the event HQ and where appropriate, mileage incurred in the course of their duties.

Meals.

If one meal per day has not already been provided by the club, €19.00 for each day, or part of day official is on duty, may be claimed.

Where the position is one of Safety Officer the mileage for the initial inspection of the stages plus meals may also be claimed. Also, where a note taker is required, this person's meals may be claimed for.

How to Claim.

Each official may apply for reimbursement of expenses by completing Form R1. This form has two parts, a 'Club Copy' and 'Official's Copy'.

The completed form must be given to the event Secretary before departure from the event.

The Secretary must sign in the 'Received by' box and retain the 'Club Copy'. The 'Official's Copy' is to be retained by the official.

The organising club must reimburse the official within 30 days of the date of the event.

If payment is not received within the specified time, the official concerned should inform the Motorsport Department.



PERMANENT CIRCUITS

APPENDIX 105

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BASIC SAFETY REQUIREMENTS FOR PERMANENT MOTOR RACING CIRCUITS

The following basic safety requirements are mandatory for all meetings catering for Saloon and Racing cars held on the approved Mondello Park Circuit. These requirements will be under constant review and may be amended or added to as required. They do not relate to Rallycross or Rally Sprint events, which are catered for separately.

1. CIRCUIT - GENERAL REQUIREMENTS.

The racing surface to be maintained in a consistent smooth condition with a near uniform level of adhesion. Concrete edging on outside of bends to have a marked "rumble" element, painted to delineate it clearly. Specific irregularities such as surface drainage gullies also to be painted.

Infield grass to be cut regularly so that it will not obstruct drivers views across a corner.

Sand traps to be raked regularly and maintained in loose condition and weed free.

2. SURROUND.

Fences acting as spectator barriers around the circuit and the paddock to be maintained in good condition and so constructed that spectators cannot sit on top of the fences with their legs on the inside.

Perimeter protection banks to be faced regularly to maintain a near vertical surface.

Entrance gates to the track from main entrance and from paddock to be permanently manned during a meeting and kept closed when practice is taking place.

3. CIRCUIT APPROVAL.

The permit for each event must indicate which Track will be used. Only one Track may be utilised per event.

The circuit is inspected annually for clockwise racing. Any other formats will require additional specific approval before they are used, as will any alterations effecting circuit layout or changes impinging on safety considerations for Competitors or Spectators.

4. GRID SIZES.

Refer to track licence for details of permitted number of starters.

5. PITS.

A siren is to be installed and permanently manned during racing to give audible warning of cars entering the pit lane.

Both Entrance and Exit gates from the Paddock must be permanently manned during practice and racing.

Marshals must be provided to control movement throughout the pit area and around the base of the Tower.

The Pits are a No Smoking area. Children are not permitted in the pit area. Bottles/Glasses are not permitted in this area.

Each competitor will be provided with two pit passes. Only persons with valid passes are allowed on the pit road or pit wall, competent marshals must be assigned to control this area. A notice to this effect must be placed in the Final Instructions.

A single yellow line should extend from the end of the pit lane armco. This line should run parallel with the inside of the track at a distance of 150cm approx.

6. FLAG MARSHALS.

The location and protection of Flag Marshal posts needs to be reviewed annually to ensure that signals can be clearly seen by drivers on all occasions. The use of light signals may be necessary in some positions where it would be unsafe to locate Marshals. The number of any other non-marshal personnel required to be present in the vicinity of Marshals Posts must be strictly limited. They will only be allowed if specially authorised by the Promoting Club for specific purposes. They must sign-on the Club Insurance in advance and must not remain near the Marshals post longer than necessary.

7. FIRE EXTINGUISHERS.

It is a requirement that 72 extinguishers be available, located as required around the circuit. The existing cartridge type tend to be less reliable and should be replaced as necessary by the retained pressure type.

8. RESCUE UNIT.

A Rescue Unit with equipment and staff similar to that outlined in Appendix 108 of the MI Yearbook must be present during all Race Meetings. In the absence of the the normal



PERMANENT CIRCUITS

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M.S.R.S. unit, a suitable alternative with duplicate equipment, as available from M.S.R.S., must be provided.

9. MEDICAL.

9.1. While it is desirable to have two ambulances in attendance for all race meetings, it is mandatory to have one ambulance at the circuit at all times of practice and racing. Ambulance crews must be familiar with a designated route to the nearest Hospital. The departure route from the Circuit must be kept clear at all times.

Two or more Doctors are also desirable, but one Doctor present during racing is mandatory.

A First Aid Centre must be kept in clean and tidy condition and contain the following minimum equipment.

9.2. Race Medical Centre.

9.2.1. A Resuscitation kit which should comprise a hand operated bag type ventilator together with a full set of equipment for endotracheal intubation. 1 hand or foot operated suction unit able to obtain 300mm mercury suction and with a container of not less than 350ml or overspill system and a full selection of catheters including a Yankaur type.

Intravenous Fluids:

8 x 500ml Haemaccel or equivalent
6 x 500ml Hartmann's Solution
2 x 500ml 5% Dextrose
8 Intravenous giving sets and Cannulae (e.g. 14, 16, 18)

Airways: 8

2 x Laryngoscope and cuffed endotracheal tubes
(3) and syringe to inflate Portable 'Entronox' set with a supply of 2 cylinders is strongly recommended

Powerful hand torch.

A means of collecting blood for cross matchings
1 dozen large field dressings or mine dressings.
1 sphygmomanometer
1 stethoscope
Burn dressings

9.2.2. Drugs which are necessary for resuscitation and for analgesia are not listed as these must only be carried by the individual doctor concerned because of local Drug regulations. It is essential however that the doctor equip himself/herself with those drugs which he/she feels may be necessary in such rescue work, e.g. Adrenaline, Lignocaine, and Atrophine for cardiac arrest, and Nailbuphine and Ketamine for parental analgesia.

9.2.3. The Minimum equipment which must be provided at the Medical Centre is:

- 9.2.3.1. Needle holders
- 9.2.3.2. Dressing forceps
- 9.2.3.3. Artery forceps
- 9.2.3.4. Large and small scissors
- 9.2.3.5. Scalpels and blades
- 9.2.3.6. Suture materials
- 9.2.3.7. Adequate supply of bandages and splints
- 9.2.3.8. Small Autoclave recommended
- 9.2.3.9. Cervical Collar
- 9.2.3.10. Defibrillator and ECG monitor (desirable)
- 9.2.3.11. Scoop stretcher
- 9.2.3.12. Oxygen cylinder (size F-1360 litres, 48 cu.ft) and spare, together with necessary pressure reducing valve and equipment.
- 9.2.3.13. Chest drainage kit
- 9.2.3.14. Facilities for cricethotomy
- 9.2.3.15. Casualty immobiliser (vacuum mattress) recommended
- 9.2.3.16. Stretcher for Emergency Ambulance [18.1.7]
- 9.2.3.17. At International Race Meetings - a Pulse Oximeter. (NB This recommended for other race meetings.)

Prior to every Race Meeting the above facilities must be checked and the relevant Hospital put on alert.

10. COMMUNICATIONS.

The use of effective Radio units is essential. A Control unit and 10 field units are required for Marshals posts and in addition up to 6 units may be required for Club use.

It is important that after any serious incident on the track reliable and accurate information should be relayed to the Public Address to allay concern and anxiety amongst competitors and spectators.

11. OBSERVERS.

Incident Officers may act as Observers but it is desirable to have a trained selected group of Observers available to act in conjunction with the I.O.'s.

12. SCRUTINY.

Scrutiny should be operated on a suitable timetable and for meetings having a large entry, two streams should be in use. Drivers should be able to avoid undue time pressure prior to their emergence for practice or racing.

13. CLUB/CIRCUIT MANAGEMENT LIMITED LIAISON.

About one week prior to every race meeting the Clerk of the Course must contact Circuit Management to discuss the general and safety conditions prevailing and any special requirements or temporary alterations that the Promoting Club seeks.



TEMPORARY VENUES

APPENDIX 106

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SAFETY CRITERIA FOR TEMPORARY MOTORSPORT VENUES

1. OBJECT.

1.1. These criteria define the conditions that should be satisfied for a speed event held at a temporary venue on roads or tracks closed to the public, before being accepted by the Motorsport Ireland Safety Advisory Group. Alternative solutions or exceptions might be accepted only as the result of a study by the Safety Advisory Group of each case individually, in consideration of past experience gained in the case of an existing course, or other special circumstances in the case of a new course.

1.2. A course is seen as a road or track in open country, closed to the public for the duration of an event. Built up areas are not considered as suitable for speed events and if requested would be subject to the full safety precautions needed for circuit racing. Built up areas are defined as roads bounded by kerbs or footpaths backed by either private houses or commercial buildings that have direct access on the footpath. They may also include obstacles such as lampposts, telegraph poles, traffic signs etc.

2. APPROVAL.

Approval is granted following a Safety Inspection arranged through Motorsport Ireland. For new venues, six months notice must be given.

3. COURSE LAYOUT.

3.1. The shape of the course, both in plan and profile, is not in general subject to restrictions in these regulations, as it is dictated by certain variable factors: the character of the terrain, consideration of the economics, aesthetics, tradition, the type and speed potential of vehicles envisaged, etc. Areas presenting any special risk should be referred to MI by the Safety Officer.

3.2. Surface.

The surface may be one of the metalled types commonly used for public roads and must be well maintained where possible. Changes of surface should not occur in curves or areas of hard braking or acceleration.

3.3. Track Edges and Verges.

In principle, both sides of the track should have verges at least 2m wide. This is not applicable to hillclimbs/sprints.

3.4. Visibility.

From any point on the course, the driver of the fastest car should have unobstructed forward vision for a distance along the course equal to the car's braking distance. Should this not be possible a signalling system should be installed to give warning.

3.5. Protection to the Public.

The public will only be admitted to those areas that can be fully controlled and where they are not exposed to danger, taking into account the distance from and the height above the course, also the speeds attained there. The public should at all times be behind suitable protection, be it natural or installed.

3.6. Prohibited Areas.

Prohibited areas must be fully defined by the Safety Officer. Under no circumstances should spectators, or unauthorised personnel, be allowed in these areas whilst events are in progress. Prohibited area signs must be displayed.

4. COURSE INSTALLATIONS.

4.1. Paddock.

The Paddock Area is subject to proposals and study by the Safety Officer.

4.2. Observation/Marshals Posts.

The posts are intended to provide Observers and Marshals with the necessary facilities to perform their duties during events. These posts, set adjacent to the track, should provide sufficient stabilised area, protected from competing cars.

4.3. Number and Location.

These are determined for each course by studying the circuit characteristics and vision. Visual contact must be maintained between preceding and following posts. Each post must be numbered in sequence from the Start to Finish.

Locations will be proposed by the Clerk of the Course and agreed with the Safety Officer.

4.4. Protection.

The posts must be situated so that only in case of an incident the staff would be forced to operate unprotected. Hillclimb and sprints marshals are generally encouraged not to vacate their posts, but to wait for assistance. Ideally they should have the same protection as at Race Circuits. Where this is not possible /practical, the Safety Officer, in conjunction with the Clerk of the Course, will study each post situation.

4.5. Course Control.

The Clerk of the Course must be based close to the track and have direct access to the competition area. The Clerk of the Course and his team need the facilities necessary to perform their duties in satisfactory working conditions.

Course control must have access to an outside network telephone connection.



TEMPORARY VENUES

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4.6. Equipment.

For race only the Clerk of the Course must be linked by either radio or telephone with the main posts on the course and those that are not linked to Course Control must be in direct contact with the main posts.

4.7. Return Routes.

Return routes, other than the track, shall be controlled at all times during events and marshalled at both ends. Strict control must be maintained upon the movement of vehicles and a system of signalling when the route is in operation.

4.8. Course Maintenance.

Proper maintenance of the course, its installations and its facilities, is a condition of the licence. Courses should be checked on a routine basis also directly after events to make sure that repair and refurbishment programmes are initiated without delay.

5. SERVICES.

5.1. A Doctor, two ambulances (one for hillclimbs/sprints), a rescue vehicle and a recovery vehicle must be provided at each event. In addition, an evacuation route must be planned and kept open during the meeting. The nearest suitable hospital must be notified.

5.2. Fire extinguishers must be provided at the start and finish and all other points as agreed with the Safety Officer and personnel must be competent to operate them.

5.3. Toilet Facilities.

These must be provided at all temporary venues.

6. An Inspection Fee of €100.00 will apply to all venues inspected both permanent and temporary, (see Appendix 15, and the GCR's).

For all new venues the promoting club must notify the MI Motor Sport Department 6 (six) months prior to the event and include plans and a description of the venue, the Safety Plan, together with details of classes proposed. A Safety Inspection will be carried out as soon as possible.



COACHING VENUES

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MINIMUM CRITERIA FOR MOTORSPORT IRELAND LICENSED PERMANENT COMPETITOR COACHING VENUES

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To obtain an MI Licence for a Permanent Competitor Coaching Venue, the following minimum criteria must be met:

Application form C1 must be completed and submitted to MI accompanied by the relevant fee/s. This form is available from the MI office and on the MI website: www.motorsportireland.com.

1. INSURANCE.

1.1. Public Liability Insurance must be in place to cover participants on the course and any guests they may bring with them.

1.2. Employee Liability Insurance must be in place to cover the Coaches.

2. INDOOR TRAINING FACILITIES.

2.1. An adequate size classroom must be available including the necessary Audio Visual equipment and coaching materials. A list of recommended equipment is available from MI.

2.2. The room must be adequately lit, with ventilation and heating as necessary.

2.3. Adequate seating and tables must be provided.

2.4. In the case of karts and single-seaters, suitable covered facilities must be provided for vehicle familiarisation prior to going on track.

3. FOOD PREPARATION AND EATING AREAS.

3.1. If food prepared at the venue is available, the food preparation and hygiene standards must comply with current Government Regulations.

3.2. A suitable seated area, preferably with tables, must be provided for eating. This area must be kept clean and hygienic.

4. MALE/FEMALE TOILETS & CHANGING AREAS.

4.1. Suitable clean and hygienic male and female toilet facilities must be available.

4.2. Male and female changing rooms must be provided and these should be of suitable size to allow competitors to change into protective clothing.

5. OFFICE FACILITIES.

5.1. The venue must have a suitable system of administration in place in order to organise each

coaching course, record participation and liaise with MI.

5.2. A contact telephone number for normal office hours (i.e. 9 am-5 pm Monday to Friday) must be provided.

6. PROTECTIVE CLOTHING & EQUIPMENT.

6.1. Suitable clean protective clothing must be available for all participants in a full range of sizes.

6.2. A good range of helmets of a suitable standard and in a full range of sizes must be provided. Minimum standard BS6658-85 Type A or Snell SA95.

6.3. At least one full set of MI Approved standard protective clothing and helmet must be provided for demonstration purposes.

7. FIRE PREVENTION FACILITIES.

7.1. All vehicles, with the exception of karts, must be fitted with suitable fire extinguishers with current valid in-service certificates attached.

7.2. The venue must have an adequate number of visible fire stop points around the circuit.

7.3. The venue must have suitable fire prevention, control and escape measures in place for all relevant buildings and structures.

8. MEDICAL FACILITIES.

8.1. All staff and coaches should ideally have First-Aid Training.

8.2. A written emergency procedure must be in place and available at all times.

8.3. A well-stocked suitable First-Aid kit must be available.

9. VEHICLES.

9.1. An up-to-date maintenance record must be kept for each vehicle.

9.2. All vehicles must be prepared to a standard to pass MI competition scrutiny (with the exception of fire extinguishers which may comply with 7.1 above).

9.3. Replacement vehicles must be available in case of mechanical failure.



COACHING VENUES

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10. COURSE / VENUE.

10.1. The configuration of the course, both in terms of surface material and layout, must be of a type compatible with the vehicles being used for coaching and the speeds being attained.

10.2. Each course/venue will be subject to inspection and licensing by MI on a bi-annual basis.

10.3. The responsibility for arranging timely inspections will rest with the owner/occupier of the course/venue.

11. TRACK INSPECTION & LICENSING FEES.

11.1. Track Inspection and licensing fees will be in accordance with MI General Competition Rules (GCRs) Schedule of fees, Article 4.



RESCUE UNIT

APPENDIX 108

MINIMUM EQUIPMENT SPECIFICATION FOR RESCUE UNIT

Definition of a rescue unit: A set of resources comprising of no less than 2 crew, a vehicle and equipment complying with the minimum equipment list as defined by Motorsport Ireland.		
Role: To provide rapid medical, fire and extrication intervention at rally, race and such speed events as are required by MI Regulations. To isolate and control scene hazards so as to provide a safe working environment to access and treat injured parties. To resuscitate, stabilise and extricate injured parties in conjunction with the doctor and ambulance service. To conduct incident cleanup procedures so as to ensure that the environment is returned to a safe condition and that those hazards that still exist are minimised to the satisfaction of the stewards. To provide full cooperation where local authority emergency services are in attendance.		
Vehicle	Notes	
Type of vehicle	Expt must be carried in no more than 1 vehicle or vehicle / trailer combination. The vehicle must be suitable for the weight being carried.	Free
No of crew	Proposed 3 crew min for 2008	2
Light bar colour	Blue only if vehicle is registered ambulance/rescue with local authority	Red or Amber
Rally Freq radio Fitted	Vehicle to be clearly marked "Rescue"	1
Markings		1 per side
Crew Personal Kit	All personnel must be clearly identifiable as Rescue Personnel on scene.	
Eye protection per crew members		1
Leather / fire gloves per crew members		1 pair
Latex / medical gloves per crew members		3 pairs
Overalls / turnout gear per crew member		1 pair
Steel toe capped boots per crew member		1 pair
Hi Vis jacket / waistcoat per crew member		1 pair
Medical Kits		
Equipment carried must meet or exceed the standard medical kit list from MI		
Fire Fighting Expt		
9 Kg Dry Powder stores pressure extinguisher	Fire suppression systems based on tanks and hoses may also comply each system will be considered on its effectiveness.	2
9 Litre AFFF (Foam) stored pressure extinguisher	All fire fighting expt must have valid annual test certificates	2
Fire Resistant Blanket 1m x 1m		1
Hydraulic cutting gear	The choice of tools is open to the operator however each operator must ensure that their choice of tools will allow them to deal with type of accidents encountered in the motorsport environment.	
Hydraulic tools for spreading open doors, cutting and removing A, B, C posts and roll cages must be carried. Tools carried must be suitable for use on modern cars using the latest materials and construction techniques.	All cutting equipment must have annual certification from the manufacturers designated service agent.	1 cutter 1 spreader or 1 combitool
At minimum a cutter and spreader must be carried. A cutter and spreader may be combined into a combi-tool.		1 power ram Optional
Where a powered system is carried fuel must be carried in an approved safety container. Spare spark plugs, engine oil and hydraulic fluid must also be carried.		
* Where a powered ram is carried the 10 ton ram from the manual hydraulics incl extensions may be omitted.		
For 2007 tools may be powered by hand/foot pump. For 2008 a motor powered system will be required.		

RESCUE UNIT

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Manual powered Hydraulics 10 Ton Hydraulic Body Kit to include ram, extensions, wedge and pump Pedal Cutter (Min 3cm opening) Trolley jack (2000kg capacity min) Bottle jack (2000kg capacity min)	1		Note where a powered ram is carried the 10 ton ram may be omitted from this section.
	1		
	1		
	1		
Air powered kit (Optional) Twin cylinder frame carrying 2x 1980 PSI cylinders with regulator to 100 PSI. (3,000 PSI cylinders also approved)	1		This section is optional Where air cylinders are carried, valid cylinder test certificates must be available for inspection.
Air chisel and 3 spare bits.	1		
Airline min 5 m long	2		
Reciprocating saw Recip saw with min 10 spare blades either air, electrical or battery powered For battery operated units at least 1 spare battery to be carried.	1		
Lighting Vehicle or generator powered lights	2		At least 1 metre long.
Torches	2		
Warning triangles	1		
Hand tools Crowbars 1 metre	1		
Hand Axe	1		Note: "selection" implies sizes / types suitable for automotive disassembly.
Bushmans saw	1		
Screwdrivers Selection of cross head and flat head	1		
Allen keys Selection of metric and imperial sizes	1		
Sockets Selection of metric and imperial sizes including ratchets & extensions.	1		To include 2 spare blades
Spanners Adjustable 1 large and 1 small	1		
Spanner	1 each		
Hammers 1 Engineers and 1 2lb club	1 each		
Centre Punch	1		Including 10 spare blades Including 10 spare blades
Glass saw	1		
Pilers Selection	1		
Bolt cutters	1		
Hacksaws	2		(suitable for supporting weights in excess of 2 ton.) (suitable for supporting weights in excess of 2 ton.)
Knife (Stanley type)	2		
Vehicle Stabilisation			
Wedges	4		
Step Chocks	4		
Assorted blocking	Yes		
Ropes (1 rope to be min 10m long)	2		
Ratchet straps (1000kg capacity min)	2		
Patient Protection			
Hard protective shields	1		
Heavy duty plastic sheeting	2		
Materials for sharp edge protection	Yes		
Miscellaneous			
Camera and 1 spare roll of film.	1		
Motorsport Ireland Accident report forms	10		



PUBLICITY

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MOTOR SPORT PUBLICITY

If you are unhappy or indeed happy with the coverage given to Irish Motor Sport then please inform the relevant person listed below. It could make all the difference. We need your help so perhaps you should keep this by your phone!

Publication	Sports Editor	Phone	E-mail
Irish Times	Malachy Logan	01 6758000	mlogan@irish-times.ie
Irish Independent	Dave Courtney	01 7055333	dcourtney@independent.ie
Sunday Independent	John Greene	01 7055333	jgreene@independent.ie
Evening Herald	Pat Keane	01 7055333	pkeane@independent.ie
Sunday Times	Nick Greenslade	01 4792424	nick.greenslade@sunday-times.ie
The Examiner	Tony Leen	021 2300500	sport@examiner.ie
Evening Echo	John McHale	021 4802228	john.mchale@eecho.ie
Irish Mail on Sunday	Jack White	01 6375854	jack.white@mailonsunday.ie
Irish Daily Mail	Joe Callaghan	01 6375854	joe.calaghan@dailymail.ie
The Irish Mirror	Neil Fullerton	048 90-568000	irishsport@mirror.co.uk
Irish Daily Star	Eoin Brannigan	01 4901228	eoin.brannigan@thestar.ie
Sunday World	Eamon Gibson	01 8848952	eamon.gibson@sundayworld.com
Irish Sun	Geoff Thompson	01 4792579	geoff.thompson@the-sun.ie
RTE TV	Ryle Nugent	01 2082217	ryle.nugent@rte.ie
RTE Radio	Roy Willoughby	01 2082426	roy.willoughby@rte.ie
TV3	Kieran Holden	01 4193358	kieran.holden@tv3.ie
TG4	Ronan O'Coisdealbha	091 505050	ronan.o.coisdealbha@tg4.ie
Setanta Ireland	Tony Whelan	01 4748000	tony.whelan@setanta.com
Newstalk 106-108FM	Ger Gilroy	01 6445100	ger@newstalk.ie

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PRESS LIST

The following members of the Motoring Press who will gladly receive information have agreed to the publication of their telephone and fax numbers where appropriate:

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	Office	Home		Office	Home	
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Where two e-mail addresses are shown, please send all information to both



GENERAL CODE OF CONDUCT

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MOTORSPORT IRELAND GENERAL CODE OF CONDUCT

1. GENERAL CODE OF CONDUCT

Motorsport Ireland expects you, and your associates, at all times to:

- Abide by Motorsports Ireland's regulations
- Respect the decisions of event officials
- Treat all competitors, marshals and officials equally with respect
- Maintain the highest standards of driving behaviour
- Conduct yourself in a proper manner at all times and always behave in the best interest of Irish motor sport
- Make every effort to minimise the impact of your activities on the environment around you

Abuse

Motorsport Ireland will not tolerate any form of abuse or aggression towards officials, marshals, spectators and competitors.

Motorsport Ireland and the National Tribunal will impose the most stringent sanction against any transgression.

Safety

You are reminded that participation in motor sport carries an inherent risk and the ultimate responsibility for safety lies with the individual. Motorsport Ireland urges you to make the maximum possible effort to ensure your own safety at all times.

2. CODE OF CONDUCT OUTSIDE OF COMPETITION.

Behaviours expected of all involved in Motorsport, its governance and development.

The guiding principles for the Code are trust, respect, fair competition and co-operation, and the pursuit of disciplined and responsible enjoyment and fun in the sport for all current and future participants.

The overriding principal for officeholders is the sustainable development of Motorsport in Ireland. This involves a whole of sport view, a balance between the short and longer term and an approach to action that is co-operative and altruistic.

The reputation of the sport and its ability to govern itself are crucial to its future success. All behaviours of those involved must be consistent with maintenance of the good reputation of the sport and its national governing body, Motorsport Ireland.

The sport is competitive and inherently dangerous if not conducted with safety in mind. The conduct of people within the sport can also result in hurt, even if this is not physical hurt. The principals of respect and trust should be evident in the behaviours of all involved in the sport. Though not tangible, poor behaviours can and should be identified and challenged through the procedures listed in below. Examples of these are given for illustrative purposes and additions may be made to this list by putting a proposal, supported by two-thirds (66%) of registered clubs, to the Motorsport Commission for consideration:

Side deals or agreements at informal meetings before the main meeting are not a valid way of conducting business as they are outside the regulated channels. Such deals are null and void unless brought to the relevant decision-making body for discussion and decision.

The use of the media as a method of exerting additional pressure towards an outcome is not a behaviour that is consistent with this Code.

Support to an individual who is in a difficult spot, at an individual level if not for the position taken, is a positive behaviour. This could include a phone call providing moral or emotional support or a quiet word of support at a human level on the margins of the meeting.

The outcome of decisions should be communicated as agreed. When it is agreed that a topic, or the points raised in discussion on a topic-, are confidential, members of the Commission/club/committee will not divulge that information to non-members of that governing structure.

DISPUTE RESOLUTION

The following procedures must be conducted in keeping with the Code of Conduct.

1. In the first instance, those directly involved will identify the problem and address it.
2. If 1) does not happen, the relevant committee will speak with those directly involved to determine the issue, the views and seek to mediate a resolution.
3. If 2) does not resolve the issue, the case will be referred to the President of the Motor Sport Commission (MSC) who will attempt to mediate a solution or rule on the matter.



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4. If 3) is not successful, the issue will be referred to Just Sport Ireland (an independent arbitrator) who specialises in sport disputes, who will rule on the matter and their decision will be binding. The notes and submissions from the various steps in the resolution process will be made available to the arbitrator and they may call parties to participate in the arbitration process.

5. Where the behaviour of an individual or a group of individuals is in breach of the Code of Conduct, the individuals can be called to account, using the above procedures, through a formal complaint made by two-thirds (66%) of the members of the Commission or two-thirds (66%) of the organising committees of the disciplines in the sport. Note the two-thirds refers in all cases to the total membership of the group, not just those in attendance.

6. The President of the Motorsport Commission, to allow for mediation discussions to take place, may declare a cooling off period of up to 4 weeks. This period will be respected and no action will be taken (during the cooling off period) by any sides to the dispute, other than to participate in the resolution process if requested.

7. If the matter is referred to Just Sport Ireland a binding arbitration process will conclude the matter under discussion



CODE OF CONDUCT FOR CHILDREN'S SPORT

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CODE OF CONDUCT FOR CHILDREN'S SPORT

1. MOTORSPORT IRELAND'S POLICY STATEMENT

Motorsport Ireland is fully committed to safeguarding the well being of its members. Every individual in the organisation should at all times, show respect and understanding for members rights, safety and welfare and conduct themselves in a way that reflects the principles of the organisation and the guidelines contained in the Code of Ethics and Good Practice for Children's Sport. The purpose of this code is to ensure that children can strive to achieve in competition and still be protected from the vulnerabilities of childhood.

2. GUIDING PRINCIPLES

For the purposes of this code, a child/young person is defined by the Irish Sports Council as any person under 18 years of age. The guidance given in this code of conduct is based on the principles set out in the following publications:

- "Code of Ethics and Good Practice for Children's Sport", Irish Sports Council.
- "Children First" National Guidelines for the Protection and Welfare of Children.
- "Our Duty to Care" Dept. of Health & Children, April 2002.

The aim of this code is to promote best practice by everyone involved in Motorsport and to provide a safe, healthy, enjoyable environment for young members.

3. MOTORSPORT IRELAND'S CORE VALUES IN YOUNG PEOPLE'S SPORT

INTEGRITY IN RELATIONSHIPS:

Adults interacting with children in sport should do so with integrity and respect for the child. There is a danger that sporting contexts can be used to exploit or undermine children. All adult actions in sport should be guided by what is best for the child and in the context of quality, open working relationships. Verbal, physical, neglect, emotional or sexual abuse of any kind is unacceptable within sport.

FAIR PLAY:

All children's sport should be conducted in an atmosphere of fair play. Ireland has contributed and is committed to the European Code of Sports Ethics, which defines fair play as: "much more than playing within the rules".

It incorporates the concepts of friendship, respect for others and always playing with the right spirit. Fair play is defined as a way of thinking, not just behaving. It incorporates issues concerned with the elimination of opportunities, excessive commercialisation and corruption". (European Sports Charter and Code of Ethics, Council of Europe, 1993).

QUALITY ATMOSPHERE AND ETHOS:

Children's sport should be conducted in a safe, positive and encouraging atmosphere. A Child centred ethos will help to ensure that competition and specialisation are kept in their appropriate place. Too often competitive demands are placed on children too early and results in excessive levels of pressure on them and as a consequence, high levels of dropout from sport.

EQUALITY:

All children should be treated in an equitable and fair manner regardless of age, ability, sex, religion, social and ethnic background or political persuasion. Children with disability should be involved in sports activities in an integrated way, thus allowing them to participate to their potential alongside other children.

Leaders in children's sport should strive to create a positive environment for the children in their care. They have an overall responsibility to take the steps necessary to ensure that positive and healthy experiences are provided.

4. MOTORSPORT IRELAND GUIDELINES FOR YOUNG COMPETITORS:

Motorsport Ireland wishes to provide the best possible environment for all young people involved in the sport. Young competitors deserve to be given enjoyable, safe sporting opportunities, free of abuse of any kind. These competitors have rights, which must be respected, and responsibilities, which they must accept.

YOUNG COMPETITORS ARE ENTITLED TO:

- Be treated with dignity and respect.
- Be safe and to feel safe.
- To get help against bullies.
- To be listened to.
- To be believed.
- To protect their own bodies.
- To be free from inappropriate touches.
- To be happy, have fun and enjoy sport.
- To experience competition and the desire to win as a positive and healthy outcome of striving for best performance.

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CODE OF CONDUCT FOR CHILDREN'S SPORT

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YOUNG COMPETITORS SHOULD ALWAYS:

- Abide by the rules of the sport.
- Treat all coaches, team managers/owners, selectors, club and event organisers and officials with respect.
- Avoid behaviour, conduct or language that may bring the sport into disrepute.
- Abide by the rules set down by team managers/owners when travelling to away events.
- Take responsibility for their own safety and the safety of others while driving a competition vehicle.

YOUNG COMPETITORS SHOULD NEVER:

- Use unfair or bullying tactics to gain advantage on or off the track.
- Use bullying tactics to isolate another competitor.
- Pass on gossip about another competitor or adult.
- Make false allegations against other competitors or adults.
- Use drugs, alcohol or tobacco at Motorsport Ireland activities.

5. BULLYING

"Bullying is repeated aggression (verbal, psychological or physical) conducted by an individual or group against others."

It thrives in an atmosphere of uncertainty and secrecy, where the victim feels a sense of hopelessness against the power of the bully.

Some Types of Bullying:

- Physical aggression
- Damage to property
- "Slagging" involving personal remarks
- Extortion
- Intimidation
- Abusive phone calls, texts or internet messages.
- Isolation
- Name-calling
- Sarcasm
- Humiliation
- Threatening behaviour

Bullying of any kind is unacceptable within motorsport.

6. MOTORSPORT IRELAND GUIDELINES FOR PARENTS/GUARDIANS:

Motorsport Ireland is committed to providing a safe and fair environment for all juvenile competitors. MI's duty of care and jurisdiction only extends from 8.00am on the day of the event for which we have issued a Permit, until the end of the event when the competition has finished. Parents/guardians must understand that they

have the ultimate responsibility for the safety of their children. Travel and overnight accommodation does not come under the jurisdiction of the Motorsport Ireland Permit and so parents/guardians must be careful and have confidence in those they charge with the minding of their children. Our first priority is the welfare of young competitors and we are committed to providing an environment, which will allow participants to perform to their best ability, free from bullying and intimidation. Motorsport Ireland believes that parents/guardians should....

- Be a role model for your child and maintain the highest standards of conduct when interacting with children, other parents, with officials and organisers.
- Encourage your child to abide by the rules of the sport.
- Always behave responsibly on the sidelines and do not seek to unfairly affect the competition or a competitor in the event.
- Take care not to expose any junior competitor, intentionally or unintentionally, to embarrassment or disparagement by the use of flippant, sarcastic or abusive remarks.
- Always recognise the value and importance of the volunteers who provide sporting /recreational opportunities for your child.
- Respect stewards, scrutineers, organisers, coaches and other competitors.
- Do not publicly question the judgement or honesty of stewards, scrutineers, organisers or coaches.
- Teach your child that honest endeavour is as important as winning and do all you can to encourage good sportsmanship.
- Set a good example by applauding good competition by all competitors.
- Encourage mutual respect for team-mates and opponents. Parents should support all efforts to remove abusive and bullying behaviour in all its forms:

CHILD TO CHILD – includes physical aggression, verbal bullying, intimidation, isolation, abusive phone, text or internet messages.

ADULT TO CHILD – includes the use of repeated gestures or expressions of a threatening or intimidatory nature, or any comment intended to degrade the child.

ADULT TO ADULT – includes verbal aggression towards other adults in order to achieve a beneficial outcome for own self or own child.

CHILD TO ADULT - includes repeated gestures or expressions of a threatening or intimidatory nature by an individual child or a group of children.



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7. PARENTS'/GUARDIANS' DECLARATION FORM:

1. I will respect the rules and procedures set down in Motorsport Ireland's (MI) Code of Conduct for Children in Sport.
2. I will respect my child's fellow competitors, their parents, team managers/owners and coaches.
3. I will give encouragement and applaud only positive accomplishments whether from my child, his/her fellow competitors or the officials.
4. I will respect my child's coach and support his/her efforts.
5. I will respect MI and club officials and their authority during events.
6. I will never demonstrate threatening or abusive behaviour or use foul language.
7. I will encourage my child to treat other participants, coaches, selectors, team managers/owners, MI officials, club officials and event organisers with respect.
8. I have read and accept the guidelines for travelling with children contained in MI's Code of Conduct for Children's Sport.

Parent/s and/or guardian/s must sign this form as appropriate.

Name:
PLEASE PRINT IN BLOCK CAPITALS

Signature:

Name:
PLEASE PRINT IN BLOCK CAPITALS

Signature:

Name of competitor:
PLEASE PRINT IN BLOCK CAPITALS

Licence No.

Date:

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8. MOTORSPORT IRELAND'S POLICY-ENTRANTS /COACHES/TEAM MANAGERS/TEAM OWNERS:

Motorsport Ireland recognises the key role entrants, coaches and team managers/owners play in the lives of children in sport. Motorsport Ireland has adopted the principles of Irish Sports Council's "Code of Ethics and Good Practice for Children's sport".

All entrants/coaches/team managers/owners should have as their first priority the children's safety and enjoyment of the sport and should adhere to the guidelines and regulations set out in Motorsport Ireland's Code of Practice.

All entrants/coaches/team managers/owners must respect the rights, dignity and worth of every child and must treat everyone equally, regardless of sex, ethnic origin, religion or ability.

There must be a "sign-up" procedure, whereby the appointed/reappointed entrant/coach/team manager/owner, agrees to abide by the "Code of Ethics and Good Practice for Children in Sport" and to the policies and code of Motorsport Ireland.

When travel/overnight travel is involved, the parents of children are advised to abide by the guidelines as set out in this Code of Conduct.

9. GUIDELINES FOR COACHES:

A coach of juvenile competitors has a duty of care, which is more onerous than that of a coach to an adult competitor.

A coach must act as a role model and promote the positive aspects of sport and of Motorsport Ireland and maintain the highest standards of personal conduct.

The use of drugs, alcohol and tobacco must be actively discouraged as being incompatible with a healthy approach to sporting activity.

Remember your behaviour to competitors, other officials, and opponents will have an effect on the competitors in your care.

Be generous with praise and never ridicule or shout at competitors for making mistakes or for losing a competition. All young competitors are entitled to respect.

Be careful to avoid the "star system". Each child deserves equal time and attention.

Care must be taken not to expose a child intentionally or unintentionally to embarrassment

or disparagement by use of sarcastic, flippant or abusive remarks about the child or his/her family.

Physical punishment or physical force must never be used. Never punish a mistake - by verbal means, physical means, or isolation.

Insist that competitors in your care respect the rules of the sport. Insist on fair play and ensure that your competitors know that you will not tolerate cheating or bullying behaviour.

Remember that young competitors participate for fun and enjoyment and that skill and competing for fun have priority over highly structured competition.

Never make winning the only objective.

Encourage the development of respect for fellow competitors, MI and club officials.

It is important to realise that certain situations or friendly actions could be misinterpreted by the competitor or by outsiders.

Motorsport Ireland coaches are responsible for setting and monitoring the boundaries between a working relationship and friendship with competitors. It is inadvisable for coaches to involve young competitors in their personal life i.e. visits to coaches home or overnight stays.

When approached to take on a new competitor, ensure that any previous coach-student relationship has been ended by the student /others in a professional manner.

When young competitors are invited into adult groups/teams, it is advisable to get agreement from a parent/carer. Boundaries of behaviour in adult groups are normally different from the boundaries that apply to junior groups/teams.

Motorsport Ireland coaches who become aware of a conflict between their obligation to their competitors and their obligation to their governing body must make explicit the nature of the conflict and the loyalties and responsibilities involved, to all parties concerned.

Coaches should communicate and co-operate with medical and ancillary practitioners in the diagnosis, treatment and management of their competitors' medical and psychological problems.

Set realistic goals and do not push young competitors. Create a safe and enjoyable environment. Do not criticise other officials, coaches, and selectors. You are the role model for the children in your care.



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Avoid giving advice of a personal or medical nature – if you are not qualified to do so. Any information of a personal or medical nature must be kept strictly confidential unless the welfare of the child requires the passing on of this information.

It is advisable to:

- Record attendance at coaching sessions.
- Keep a brief record of injury(s) and action taken.
- If behavioural problems arise and a young competitor has to be disciplined, keep a brief record of problem/action/outcomes.

The nature of the relationship between coach and competitor can often mean that a coach will learn confidential information about a competitor or competitor's family. This information must be regarded as confidential and except where abuse is suspected, must not be divulged to a third party without the express permission of the competitor/family.

Coaches must refrain from drinking alcohol in the presence of young people and be particularly careful about their use of alcohol before coaching, during events and on trips with young competitors.

Coaches should familiarise themselves with the Code of Ethics and Good Practice for Children's Sport and with Motorsport Ireland's Code of Conduct and follow the procedures if they suspect or receive complaints of abuse of any sort.

10. GUIDELINES FOR ADULT COMPETITORS (18 YEARS AND OVER):

- Adult competitors should act as role models promoting the positive aspects of sport and Motorsport Ireland while maintaining the highest standards of personal conduct.
- The use of drugs and alcohol should be actively discouraged as being incompatible with Motorsport.
- Remember your behaviour to competitors, other officials, and opponents may have an effect on young competitors.
- Care must be taken not to expose a child intentionally or unintentionally to embarrassment or disparagement by use of sarcastic, flippant or abusive remarks about the child or his/her family.
- Physical punishment or physical force must never be used. Never punish a mistake - by verbal means, physical means, or isolation.
- It is important to realise that certain situations or friendly actions could be misinterpreted by a competitor or by outsiders.

- Adult competitors should familiarise themselves with this Code of Conduct and follow the procedures at the Articles 18 to 22 if they suspect or receive complaints of abuse of any sort.

11. GUIDELINES FOR ENTRANTS/TEAM MANAGERS/TEAM OWNERS:

An Entrant/Team Manager/Team Owner must act as a role model and promote the positive aspects of sport and of Motorsport Ireland and maintain the highest standards of personal conduct.

The use of drugs, alcohol and tobacco must be actively discouraged as being incompatible with a healthy approach to sporting activity.

Remember your behaviour to competitors, other officials, and opponents will have an effect on the competitors in your care.

Be generous with praise and never ridicule or shout at competitors for making mistakes or for losing a competition. All young competitors are entitled to respect.

Be careful to avoid the "star system". Each child deserves equal time and attention.

Care must be taken not to expose a child intentionally or unintentionally to embarrassment or disparagement by use of sarcastic, flippant or abusive remarks about the child or his/her family.

Physical punishment or physical force must never be used. Never punish a mistake - by verbal means, physical means, or isolation.

Insist that competitors in your care respect the rules of the sport. Insist on fair play and ensure that your competitors know that you will not tolerate cheating or bullying behaviour.

Remember that young competitors participate for fun and enjoyment and that skill and competing for fun have priority over highly structured competition.

Never make winning the only objective.

Encourage the development of respect for fellow competitors, MI and club officials.

It is important to realise that certain situations or friendly actions could be misinterpreted by the competitor or by outsiders.

Set realistic goals and do not push young competitors.

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Create a safe and enjoyable environment.

Do not criticise other officials, coaches, team managers/ owners. You are the role model for the children in your care.

Avoid giving advice of a personal or medical nature – if you are not qualified to do so. Any information of a personal or medical nature must be kept strictly confidential unless the welfare of the child requires the passing on of this information.

It is advisable to:

- Record attendance at practice or testing sessions.
- Keep a brief record of injury(s) and action taken.
- If behavioural problems arise and a young competitor has to be disciplined, keep a brief record of problem /action/outcomes.

Entrants/Team Managers/Team Owners should refrain from drinking alcohol in the presence of young people and should be particularly careful about their use of alcohol before and during events and on trips with young competitors.

Entrants/Team Managers/Team Owners should familiarise themselves with the Code of Ethics and Good Practice for Children's Sport and with Motorsport Ireland's Code of Conduct and follow the procedures if they suspect or receive complaints of abuse of any sort.

12. GUIDELINES FOR CLUB AND MOTORSPORT IRELAND OFFICIALS AT ALL MI EVENTS:

- Young competitors must not be interviewed unless in the presence of a Children's Officer and/or the child's Entrant /Parent.
- Young competitors must be treated with dignity and respect.
- No matter how serious an allegation may be, the young competitor concerned must not feel bullied or intimidated.
- Officials must listen carefully to the explanation given by a young competitor.
- Avoid behaviour or conduct that could be deemed as being offensive.
- Understand that young competitors will naturally be nervous in the presence of officials.
- Officials must explain to the young competitor why their presence is required and explain to them as clearly as possible what regulation, if any, has been breached.
- Officials should act as role models promoting the positive aspects of sport and Motorsport Ireland while maintaining the highest standards of personal conduct.

- The use of drugs and alcohol should be actively discouraged as being incompatible with Motorsport.
- Remember your behaviour to competitors, other officials, and opponents may have an effect on young competitors.
- Care must be taken not to expose a child intentionally or unintentionally to embarrassment or disparagement by use of sarcastic, flippant or abusive remarks about the child or his/her family.
- Physical punishment or physical force must never be used. Never punish a mistake - by verbal means, physical means, or isolation.
- It is important to realise that certain situations or friendly actions could be misinterpreted by a competitor or by outsiders.
- Officials should familiarise themselves with this Code of Conduct and follow the procedures at the Articles 18 to 22 if they suspect or receive complaints of abuse of any sort.



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13. ASSESSMENT / APPLICATION QUESTIONS FOR MI COACHES

Position applied for:

Name:

Any surname previously known by:

Date of Birth:

Present address:

Current occupation:.....

Name of organisation:.....

Previous experience of working with young children in a voluntary or professional capacity:

.....

Sporting/ Motorsport Ireland Qualifications:

Reason for Applying:

Have you ever been asked to leave a sporting organisation:

Yes ☐ No ☐

If you have answered yes, we will contact you in confidence.

Name and address of two people who know you (but who are not related to you) and who would have experience of you working with children, whom we can contact and who would provide you with a reference.

Name and Address of Referee 1:

.....

Name and Address of Referee 2:

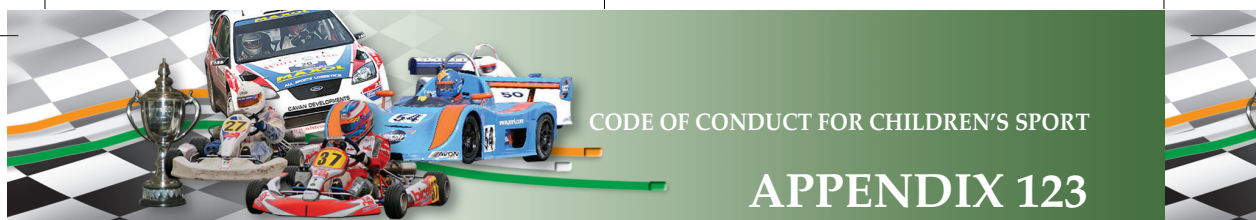
.....

I agree to abide by the Code of Ethics and Good Practice for Children's Sport and Motorsport Ireland's Code of Conduct.

Signed

Date:

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14. SAMPLE LETTER TO PERSON GIVING A REFERENCE

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Name & Address of Referee

Re: Name & Address of Coach

Dear

Motorsport Ireland is the National Governing Body of four-wheeled motor sport in Ireland. Under the umbrella of the Irish Sports Council and the National Coaching and Training Centre (NCTC), we are involved in training Motorsport Coaches. We have a "Code of Conduct for Children's Sport" and in line with this and the Irish Sports Council's "Code of Ethics and Good Practice for Children's Sport", we are required to seek references for all of our registered coaches.

(Name of Coach) has trained as a Level 1 Motorsport Coach and has given us your name as a referee. If you are satisfied that this person is suitable to work with children in a sporting capacity, we would be very much obliged if you would please complete the bottom of this page and return this letter to us at your earliest convenience.

If you have any queries about this, please do not hesitate to contact me.

With many thanks,

Yours faithfully,
Motorsport Ireland

How long have you known this person?

In what capacity?

Are you satisfied that (Name of Coach) is suitable to work with children in a sporting capacity?

Yes ☐

No ☐

Signed:

Date:

Please Note: This is a standard request issued in line with Motorsport Ireland's "Code of Conduct for Children's Sport" and the Irish Sports Council's "Code of Ethics and Good Practice for Children's Sport".

Thank you.



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15. MOTORSPORT IRELAND POLICY – TRAVELLING WITH CHILDREN. GUIDELINES FOR PARENTS / GUARDIANS / COACHES / ENTRANTS / TEAM MANAGERS/ TEAM OWNERS.

Motorsport Ireland recognises the extra responsibilities taken on by coaches / entrants / team managers and team owners when they travel with children to events. This responsibility is even more onerous when overnight stays are involved.

Parents / guardians must understand that they have the ultimate responsibility for the safety of their children. Travel and overnight accommodation does not come under the jurisdiction of the Motorsport Ireland Permit and so parents / guardians must be careful and have confidence in those they charge with the minding of their children.

Cars used to transport competitors / teams: Those who use their own vehicles to transport competitors must ensure that they have adequate insurance cover and be careful not to carry more than the permitted number of passengers.

- Avoid being alone with one competitor (e.g. travelling to a venue or consulting / advising in closed off room – If necessary leave the door open and be in earshot of others).
- Avoid unnecessary physical contact outside the normal coaching, technical area. Certain types of coaching may require a "hands on approach" i.e. it may be necessary to support a child in order to physically demonstrate a particular technique. This should only occur when necessary and in an open and appropriate way with the knowledge, permission and full understanding of the child concerned and his/her parents / guardians.
- Remember casual comment can be open to misconstruction.

At Hotels, Guesthouses, Campsites etc. the following rules should apply:

- Adults should not enter a competitor's room without first knocking.
- All group socialisation should take place in communal areas (i.e. no boys in girls' rooms and vice versa).
- Alcoholic drink, smoking or other illegal substances are forbidden to competitors.
- The behaviour of the group should at all times be such that the good name of Motorsport Ireland is upheld.

- There must be at least one authorised adult of each gender with a mixed party.
- Lights out times should be enforced.
- Young competitors should be under reasonable supervision at all times and should never be allowed to leave the accommodation or event venue without prior permission.

Injury(s): Make a brief record of injury and action taken.

Behavioural Problems: Make a brief record of the problem / action / outcome. Submit report to parents / guardians on return home.

16. MOTORSPORT IRELAND GUIDELINES ON USE OF PHOTOGRAPHIC AND FILMING EQUIPMENT AT COMPETITIONS & PRACTICE SESSIONS

Amateur photographers / film / video operators wishing to record an event or practice session should seek accreditation with the event organiser.

Amateur photographs / film / video should be restricted to dummy grid, on-track, parc ferme, podium and the confines of that competitor's work place in the paddock.

Videoing as a coaching aid: Video equipment can be used as a legitimate coaching aid. However, permission should first be obtained from the competitor and the competitor's parents / guardians.

17. RESPONDING TO DISCLOSURE, SUSPICIONS & ALLEGATIONS

Motorsport Ireland accepts that organisations, which include children as members, are vulnerable to the occurrence of child abuse and therefore there is a need for policies and procedures to deal with this issue. The safety of young people is everyone's responsibility. If a young person says or indicates that he / she is being abused, or information is obtained which gives cause for concern that a young person is being abused, you should react immediately. False allegations of abuse can occur but are very rare.

The Protection for Persons Reporting Child Abuse Act, 1998 provides immunity from civil liability to persons who report child abuse "reasonably and in good faith" to the Health Board or the Gardaí. The act also covers the offence of "false reporting". This Act came into operation on 23rd January 1999. The main provisions of the Act are:

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1. The provision of immunity from civil liability to any person who reports child abuse "reasonably and in good faith" to designated officers of Health Boards or any member of An Garda Síochána;
2. The provision of significant protections for employees who report child abuse. These protections cover all employees and all forms of discrimination up to and including, dismissal;
3. The creation of a new offence of false reporting of child abuse where a person makes a report of child abuse to the appropriate authorities "knowing that statement to be false". This is a new criminal offence designed to protect innocent persons from malicious reports.

This Law does not exist in Northern Ireland where any person wrongly accused can seek recourse under the laws of slander, libel or malicious prosecution.

Each Motor Club affiliated to Motorsport Ireland should have clear procedures for responding to reports or concerns relating to the welfare and safety of children. All involved adults, children and parents/guardians should be aware of how to report, and to whom concerns should be reported, within the club/organisation. Copies of the Statutory Authority guidelines should be available in each club and copies are held at Motorsport Ireland's Office. Everyone involved in child protection matters should be aware of his/her responsibility to work in co-operation with the statutory child protection authorities.

The term "Sports Leader" means all adults, in a paid or voluntary capacity e.g. club helpers, event officials, MI officials, coaches, entrants, team managers, team owners.

18. REPORTING OF SUSPECTED OR ACTUAL CHILD ABUSE

If a Sports Leader or a parent/guardian is uneasy or suspicious about a child's safety or welfare the following response should be made:

19. RESPONSE TO A CHILD REPORTING ANY FORM OF ABUSE

The following actions should be taken:

- React calmly.
- Listen compassionately and carefully – keep the questions to an absolute minimum, facilitating the child to tell about the problem.
- Take what the child is saying seriously.
- Remember the child has decided to tell about something very important and has taken a risk to do so.
- Reassure the child.

- If the nature of what the child is saying is unclear, use open, non-specific questions such as "Can you explain to me what you mean by that".
- The child should be given some indication of what happens next - informing parents /guardians, health board or social services. It should be kept in mind that the child may have been threatened and may feel vulnerable at this stage.
- Carefully record the details.

The following actions should be avoided:

- Do not make promises of confidentiality – it may not be possible to keep that promise.
- Do not make a judgement or make negative comments about the alleged abuser
- Do not speculate or make assumptions
- Do not interview the child – do not probe for more information than is offered
- Do not approach the alleged abuser
- Do not allow your shock or distaste to show.

20. REPORTING CHILD ABUSE

It is not the responsibility of anyone working under the auspices of Motorsport Ireland, in a paid or voluntary capacity, or those working in affiliated organisations, to take responsibility or decide whether or not child abuse is taking place.

However, there is a responsibility to protect children in order that appropriate agencies can then make enquiries and take any necessary action to protect the young person.

Actions to Take:

- Observe and note dates, times, locations and contexts in which the incident occurred or suspicion was aroused, together with any other relevant information;
- Report the matter as soon as possible to the person designated for reporting abuse (the National Children's Officer (NCO), the COC or the Club Chairperson). If the NCO, COC or Club Chairperson has reasonable grounds for believing that the child has been abused or is at risk of abuse, s/he will make a report to the health board/social services who have statutory responsibility to investigate and assess suspected or actual child abuse.
- In cases of emergency, where a child appears to be at immediate and serious risk and the designated person is unable to contact a duty social worker, the police authorities should be contacted. Under no circumstances should a child be left in a dangerous situation pending intervention by the Statutory Authorities. Make a record of the report.
- If the designated person is unsure whether reasonable grounds for concern exist or not, s/he should informally consult with the local health board/social services. S/he will be advised whether or not the matter requires a formal report;



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- A designated person reporting suspected or actual child abuse to the Statutory Authorities should first inform the family of their intention to make such a report, unless doing so would endanger the child or undermine an investigation;
- A report should be given by the designated person to the Statutory Authorities in person or by phone, and in writing;
- It is best to report child abuse concerns by making personal contact with the relevant personnel in the Statutory Authorities.

21. ALLEGATIONS OF ABUSE AGAINST SPORTS LEADERS

Each club affiliated to Motorsport Ireland should have agreed procedures to be followed in cases of alleged child abuse against Sports Leaders. If such an allegation is made, two procedures should be followed:

- The reporting procedure as outlined in "Reporting Child Abuse".
- The procedure for dealing with the Sports Leader (below)

The following points should be considered:

The safety of the child making the allegation and any others who are/may be at risk should be ensured and this should take precedence over any other consideration. In this regard, the club should take any necessary steps that may be immediately necessary to protect children.

If a Sports Leader is the subject of the concern s/he should be treated with respect and fairness.

Steps to be taken within the Sports Organisation:

Where reasonable grounds for concern exist the following steps should be taken by the club:

Advice should be sought from the local health board/social services with regard to any action by the club deemed necessary to protect the child/children who may be at risk

The matter should be reported to the local health board/social services following the standard reporting procedure outlined above

In the event that the concern is connected to the actions of a Sports Leader in the club, the Sports Leader involved in the concern should be asked to stand aside pending the outcome of any investigation by the Statutory Authorities.

It is advisable that this task be undertaken by an appointed committee member other than the Chairperson if they take the responsibility for reporting.

- The Sports Leader should be informed, in private,
- That an allegation has been made against him/her,
 - The nature of the allegation,

S/he should be afforded an opportunity to respond. His/her response should be noted and passed on to the health board/social services personnel.

All persons involved in a child protection process (the child, his/her parents/guardians, the alleged offender, his/her family, Sports Leaders) should be afforded appropriate respect, fairness, support and confidentiality at all stages of the procedure.

22. CONFIDENTIALITY

Every effort should be made to ensure that confidentiality is maintained for all concerned.

Information should be handled and disseminated on a need to know basis only.

Information should be stored in a secure place, with limited access to designated people.

The requirements of the Data Protection laws should be adhered to.

Breach of confidentiality is a serious matter.

Anonymous Complaints

Anonymous complaints can be difficult to deal with but should not be ignored. In all cases the safety and welfare of the child/children is paramount. Any such complaints relating to inappropriate behaviour should be brought to the attention of the Chairperson. The information should be checked out and handled in a confidential manner.

Rumours

Rumours should not be allowed to hang in the air. Any rumours relating to inappropriate behaviour should be brought to the attention of the Chairperson and checked out without delay.

23. SAMPLE DECLARATIONS

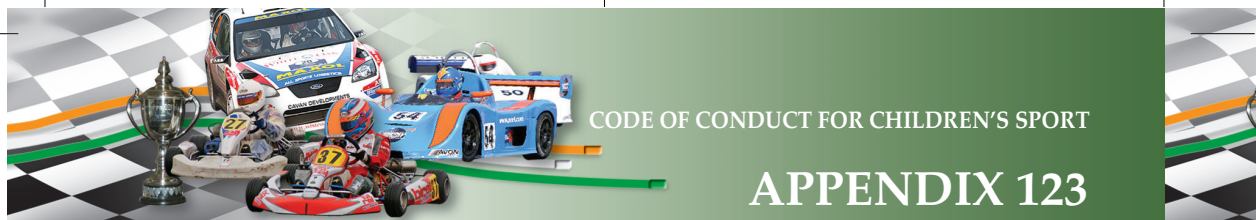
Entrants/Coaches/Team Owners/Team Managers

I hereby agree to abide by the guidelines and regulations contained in Motorsport Ireland's Code of Conduct for Children's Sport.

Name: _____

Role: _____ Date: _____

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Authorised Officials

I hereby agree to abide by the guidelines and regulations contained in Motorsport Ireland's Code of Conduct for Children's Sport.

Name: _____

Role: _____ Date: _____

All Competitors

I hereby agree to abide by the guidelines and regulations contained in Motorsport Ireland's Code of Conduct for Children's Sport.

Name: _____

Licence No: _____ Date: _____

Motorsport Ireland Affiliated & Recognised Clubs

We hereby agree to abide by the guidelines and regulations contained in Motorsport Ireland's Code of Conduct for Children's Sport.

Name of

Club: _____

Date: _____

Signed: _____

CHAIRPERSON



EVENT INSURANCE AND PREMIUMS

APPENDIX 130

2011 EVENT INSURANCE PREMIUMS AND CHARGES

2011 PREMIUMS Event Type	Status	Public Liability Insurance	Officials' Personal Accident Insurance	Permit Fee	Legal Fees	Total payable with permit application	Competitors' Personal Accident Insurance (per person)
							Adults (aged 18 and over)
							Juniors (aged 17 and under)
Circuit Race	N	1907	164	356	86	2513	15
Rallycross	N	1166	137	321	86	1710	15
Rallyprint	N	1166	137	321	86	1624	15
V&V Events	N	200	74	Waiver		274	N/A
Navigation/Retropective Trials	N	200	74	*197		471 (+ 50 levy = 521)	15
4x4 Production Vehicle Trials	N	200	74	167		441	5
Sporting Trials	N	200	74	167		441	5
Autotests	N	200	74	*167		441 (+ 50 levy = 491)	5
Autocross (LS)	N	631	110	156		897	15
Autocross (GS)	N	503	110	*156	86	855 (+ 50 levy = 905)	15
Midget Car Races	N	315	68.50	63		446.50	15
Sprint (private ground)	N	878	137	*167		1182 (+ 50 levy = 1232)	15
* Add €50 levy if your event is a National Championship round							

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2011 PREMIUMS Event Type	Status	Public Liability Insurance DEPOSIT	Officials' Personal Accident Insurance	Permit Fee	Legal Fees	Billy Coleman Award	Total payable with permit application	Competitors' Personal Accident Insurance (per person)
								Adults (aged 18 and over)
								Juniors (aged 17 and under)
Special Stage Rallies	N	11400	245	356	86	500	12587	25
Special Stage Rallies	I	15000	397	499	86	**500	16482	25
Forestry Rallies	N	3060	196	356	86		3698	25
2 Day Special Stage Rallies	N	13050	330	428	86	500	14394	25
Mini Stage Rallies (2 locations)	N	7950	220	356	86	500	9112	25
Mini Stage Rallies (3 locations)	N	9450	220	356	86	500	10612	25
Road Race	N	8250	363	356	86		9055	15
Kart Races - Circuit	N	910	110	*154	86		1260 (+ 50 levy = 1310)	15
Hillclimb & Sprint (closed road)	N	2250	137	*197	86		2670 (+ 50 levy = 2720)	15
* Add €50 levy if your event is a National Championship round								8
** Billy Coleman Award contribution applies if any part of event is run under a National permit								8

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EVENT INSURANCE AND PREMIUMS

APPENDIX 130

2011 DEPOSIT PREMIUMS		Status		Club Deposit	Maximum No. of starters for Deposit Premium	Extra premium per extra starter
Event Type						
Special Stage Rallies	N			11400	100	120 (J50%)*
Special Stage Rallies	I			15000	120	170 (J50%)*
Forestry Rallies	N			3060	50	68
2 Day Special Stage Rallies	N			13050	120	150
Mini Stage Rallies (2 locations)	N			7950	100	105
Mini Stage Rallies (3 locations)	N			9450	100	105
Road Race	N			8250	100	38
Kart Races - Circuit	N			910	65	18
Hillclimb & Sprint (closed road)	N			2250	50	43

* A JUNIOR OR HISTORIC PREMIUM (50% of full amount) IS PAYABLE WHERE THAT PORTION OF THE EVENT DOES NOT EXCEED 60% OF THE NUMBER OF STAGES.

IRDS Premium	20
--------------	----

Competitors' Personal Accident Insurance (per person)			Adults (aged 18 and over)	Juniors (aged 17 and under)
	Category 1		25	13
	Category 2		15	8
	Category 3		5	3

AMENDMENTS FOR 2011:

Mini Stage Rallies - Deposit premium for events using 2 locations reduced by €1500

- Introduction of option to use 3 stage locations without increase in Deposit premium
- Increase in number covered by Deposit Premium from 80 to 100 starters

One day National Rallies and Internationals - increase from 50% to 60% of total number of stages that may be covered by Juniors and Historics while still qualifying for reduced premium



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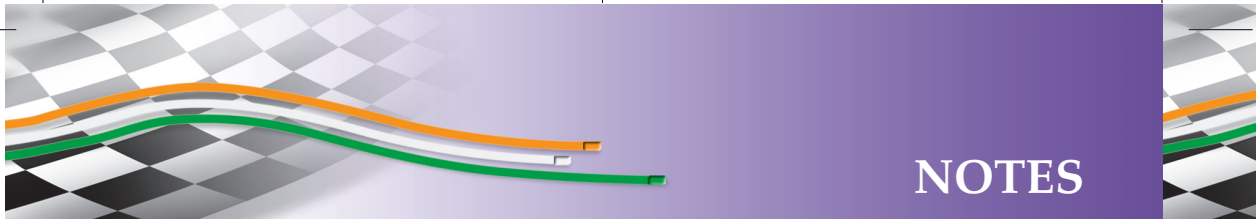
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